

**WELCOME**  
To a Regular Meeting of the  
**Coeur d'Alene City Council**  
Held in the Library Community Room at **6:00 P.M.**  
**AGENDA**

**VISION STATEMENT**

Our vision of Coeur d'Alene is of a beautiful, safe city that promotes a high quality of life and sound economy through excellence in government.

The purpose of the Agenda is to assist the Council and interested citizens in the conduct of the public meeting. Careful review of the Agenda is encouraged. Testimony from the public will be solicited for any item or issue listed under the category of Public Hearings. Any individual who wishes to address the Council on any other subject should plan to speak when **Item F - Public Comments** is identified by the Mayor. The Mayor and Council will not normally allow audience participation at any other time.

**June 4, 2024**

**A. CALL TO ORDER/ROLL CALL**

**B. INVOCATION:** Erik Curtis: The Church of Jesus Christ of Latter-day Saints-Hayden Lake Idaho Stake

**C. PLEDGE OF ALLEGIANCE**

**D. AMENDMENTS TO THE AGENDA:** Any items added less than forty-eight (48) hours prior to the meeting are added by Council motion at this time. Action Item.

**E. OTHER BUSINESS: ACTION ITEM**

1. (Quasi-judicial) Appeal Hearing for an appeal made by Joan Woodard of DR-1-24AA; CDA Hotel, LLC (Mariott Hotel) located at 602 & 612 E. Sherman Avenue.

**Staff Report by: Randy Adams, City Attorney**

**F. PUBLIC COMMENTS:** (Each speaker will be allowed a maximum of 3 minutes to address the City Council on matters that relate to City government business. Please be advised that the City Council can only take official action for those items listed on the agenda.)

**\*\*\*ITEMS BELOW ARE CONSIDERED TO BE ACTION ITEMS**

**G. ANNOUNCEMENTS**

1. City Council
2. Mayor

**H. CONSENT CALENDAR:** Being considered routine by the City Council, these items will be enacted by one motion unless requested by a Councilmember that one or more items be removed for later discussion.

1. Approval of Council Minutes for the May 21, 2024 Council Meeting.
2. Approval of Minutes from the May 28, 2024 General Services/Public Works Committee Meeting.
3. Setting of General Services/Public Works Committee Meeting for Monday, June 10, 2024, at 12:00 noon.
4. Approval of a cemetery lot transfer from Leslie Bening to Curtis Gerald Kilian; Section B, Block 40, Lot 11 of Forest Cemetery, in the amount of \$40.00
5. Approval of outdoor eating encroachment for Ten/6, LLC., Taylor Taylor, 1118 N. 2<sup>nd</sup> Street (12 seats)
6. Approval of 8 firework stand permits for 2024

**As Recommended by the City Clerk**

7. **Resolution No. 24-044**

- a. Approval of S-5-14 - The Trails 6th Addition: Final Plat Approval, Acceptance of Improvements, Maintenance/Warranty Agreement and Security

**As recommended by the City Engineer**

**I. OTHER BUSINESS:**

2. **Resolution No. 24-045** - Approval of a Contract with Alpine Northwest. for installation of a new 16” transmission main in Thomas Lane in the amount of \$2,369,358.00.

**Staff Report by: Kyle Marine, Water Department Director**

3. **Resolution No. 24-046** - Approval to reallocate part-time employee budgeted funds to purchase materials from Consolidated Supply Co. for upsizing the water main in Cda Place 38th Addition in the amount of \$85,222.93.

**Staff Report by: Glen Poelstra, Water Department Assistant Director**

**I. ADJOURNMENT**

**This meeting is aired live on CDA TV Spectrum Cable Channel 1301, TDS Channel 5,  
and on Facebook live through the City’s Facebook page.**





# Coeur d'Alene CITY COUNCIL MEETING

*June 4, 2024*

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**MEMBERS OF THE CITY COUNCIL:**

**Jim Hammond, Mayor**

**Council Members McEvers, English, Evans, Gookin, Miller, Wood**

OTHER BUSINESS

**CITY COUNCIL  
STAFF REPORT**

**DATE:** June 04, 2024  
**FROM:** Randy Adams, City Attorney  
**SUBJECT:** Appeal from DRC Approval of Marriott Hotel application

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**DECISION POINT:** Should Council affirm or reverse the approval by the DRC of the Marriott AC Hotel project on E. Sherman Avenue and S. 6<sup>th</sup> Street, or remand the matter to the DRC for further action or clarification?

**HISTORY:** On December 1, 2023, a Design Review Application was submitted for a Marriott AC Hotel project located on the southeast corner of E. Sherman Avenue and S. 6<sup>th</sup> Street. Because the project is in the Downtown Core zoning district south of midblock Lakeside/Coeur d’Alene, design review was required. The purpose of the design review process is to verify compliance with the design guidelines established by Council by Resolution. In addition, the Design Review Commission (“DRC”) is allowed to impose reasonable fact-based conditions to ensure better or more effective compliance with those guidelines, and may exercise its discretion to reconcile the adopted guidelines with site specific conditions in order to meet the intent of the Zoning Code. During the design review process, the DRC is authorized to give direction to an applicant to modify aspects of the project design for the purpose of ensuring compliance with the guidelines. Public comments are allowed at any hearing before the DRC.

The design review process starts with a Project Review, which is a meeting between the applicant and all involved City Departments. At this meeting, Department representatives provide the applicant with information about any regulatory or physical constraints pertaining to the project, services required under the City Code, applicable cap fees and meter fees, and similar matters. Then the applicant participates in an Initial Meeting with Planning Staff. The purpose of the Initial Meeting is to perform a preliminary review of the project’s compliance with the applicable guidelines and development standards. This helps the applicant to present a project to DRC without serious defects, and gives the applicant an opportunity to consider if departures from the guidelines are needed. These two meetings between staff and the applicant are not open to the public and, therefore, no public notice is provided. The law does not require public participation in staff meetings, and the planning process would be unworkable if the public was invited to staff meetings.

The meeting or meetings with the DRC are open to the public. The City must publish notice of the First Meeting in the official newspaper at least fifteen (15) days prior to the meeting, mail notice to the owners of property within three hundred feet (300') of the property, and post notice on the subject property not less than one week prior to the meeting. The required notice was provided in this case and, therefore, the DRC approval cannot be reversed on the basis of lack of notice. The DRC met on January 25, 2024. Public testimony was taken. Therefore, the DRC approval cannot be reversed on the basis of lack of public participation. The DRC is only concerned with design guidelines applicable to the DC zoning district. The DRC cannot consider

matters which it cannot modify, such as the development itself, zoning, basic zoning requirements, FAR, building height, density, intensity, parking, traffic, or use. The DRC granted approval of the application, finding that the proposal was in conformance with design guidelines for the DC zoning district and that the one requested design departure for weather protection satisfied the criteria for approving a design departure.

**Issues on Appeal:**

Joan Woodard filed a timely Administrative Appeal Application. She essentially raised five issues: (1) Lack of Public Notice and Opportunity to be Heard; (2) Inadequate or Missing Information in the Application with respect to photos of view corridors and evidence of neighborhood context; (3) Project Approval Decisions Were Made Based on Incomplete Information with respect to a traffic study; (4) 4. Design Guidelines – Ground Level Details – Have Not Been Met with respect to 6<sup>th</sup> Street; and (5) 5. Design Guidelines – Unique Historic Features – Have Not Been Met with respect to street trees and a streetlight. As noted, proper notice was given and traffic is not an issue that is within the purview of the DRC. Therefore, Issues 1 and 3 cannot be considered in this appeal.

**Appeal Hearing Procedure:**

Council considers an appeal on the record established before the DRC. No new evidence or testimony can be received by Council at the appeal hearing. The appellant and the applicant, and their respective representatives, and City Staff may address Council during the appeal hearing. In addition, members of the public will have up to three minutes each to address Council on matters within the purview of the DRC and based on the record established before the DRC. The appellant bears the burden of proving, by a preponderance of evidence (more likely than not), that the DRC made an error of fact or that the DRC ignored or incorrectly applied design guidelines. The appellant must also show that she was prejudiced by the DRC's error. Factual findings by the Commission must be accepted by Council as established if they are supported by substantial evidence.

**PERFORMANCE ANALYSIS:** Council may affirm or reverse the DRC's decision, or refer the project back to the DRC for further action or clarification. Council does not have the option, as requested by the appellant, to refer the matter back to Staff to address her concerns.

**DECISION POINT/RECOMMENDATION:** Council should affirm or reverse the DRC's decision, or refer the project back to the DRC for further action or clarification.

# DRC APPEAL

APPLICANT - CDA HOTEL LLC – MARRIOTT AC HOTEL  
602 & 612 E. SHERMAN AVE.

APPELLANT - JOAN C. WOODARD

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## Appeal Process

Staff will provide an overview of the appeal hearing procedure.

**10 Minutes:** The appellant will have an opportunity to tell the City Council that an error was made in the decision or that design standards or guidelines were ignored or incorrectly applied, and that the appellant was prejudiced thereby. The appellant may not call witnesses to give testimony or present new evidence.

**10 Minutes:** The applicant will be able to address the City Council on matters raised by the appeal. The applicant may not call witnesses to give testimony or present new evidence.

**3 Minutes:** Members of the public will be permitted up to 3 minutes to present argument on matters under the purview of the Design Review Commission based on the record presented to the Commission.

**3-5 Minutes:** The appellant will be provided an opportunity for rebuttal to address anything that came up during the City's or the applicant's presentations.

Following all of the testimony, the hearing will be closed by the Mayor and the City Council will deliberate and render a decision.

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Design Guidelines – Downtown Core  
M.C. § 17.05.705; Resolution 08-035

<ul style="list-style-type: none"> <li>Location of Parking</li> <li>Screening of Parking Lots</li> <li>Parking Lot Landscape</li> <li>Sidewalk Uses</li> <li>Width and Spacing of Curb Cuts</li> <li>Screening of Trash/Service Areas</li> <li>Lighting Intensity</li> <li>Gateways</li> <li>Maximum Setback</li> <li>Orientation to the Street</li> <li>Entrances</li> </ul>	<ul style="list-style-type: none"> <li>Massing</li> <li>Ground Level Details</li> <li>Ground Floor Windows</li> <li>Weather Protection</li> <li>Treatment of Blank Walls</li> <li>Screening Parking Structures</li> <li>Roof Edge</li> <li>Screening of Rooftop Mechanical Equipment</li> <li>Unique Historic Features</li> <li>Integration of Signs with Architecture</li> <li>Creativity/Individuality of Signs</li> </ul>
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### Design Review Commission

#### Municipal Code § 17.09.315:

B. Public Comment: \* \* \* Any public comment on a proposed project shall only be on matters related to the adopted design standards and guidelines. **No comment shall be taken on matters which cannot be modified by the Commission, including, but not limited to, basic zoning requirements, FAR, building height, density, or use.** \* \* \*

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#### Municipal Code § 17.09.330:

The applicant has the obligation to prove that the project complies with the adopted design standards and guidelines. The Commission may not substitute criteria of its own choosing for the adopted standards and guidelines nor base its decision on an individual commissioner's personal opinions about the project and its merits. **The Commission shall apply the collective judgment of its members to determine how well a project comports with the adopted standards and guidelines,** and it may impose reasonable fact-based conditions to ensure better or more effective compliance with those standards and guidelines. The Commission may also exercise discretion to reconcile the adopted standards and guidelines with site specific conditions in order to meet the intent of the Zoning Code. During the design review process, the Commission is authorized to give direction to an applicant to modify aspects of the project design for the purpose of assuring compliance with the standards and guidelines.

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B. Appeal on The Record: The Council's review of the decision of the Commission shall be based on the record developed before the Commission. No new evidence or materials shall be allowed by any party in the appeals proceedings.

M.C. § 17.09.340

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C. Hearing: Only the applicant, City staff, the appellant, and their representatives may participate in the appeals hearing. Although the hearing is open to the public, no general public testimony will be taken. Any participant in the appeal may provide comments and argument, based on the established record, concerning the decision of the Commission.

M.C. § 17.09.340

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D. Burden Of Proof: **The appellant must establish by a preponderance of evidence** that an error was made in the decision or that design standards or guidelines were ignored or incorrectly applied, and that the appellant was prejudiced thereby. **Objections to the development, its height, intensity, parking, or traffic impacts are not grounds for redress on appeal because they are not design review criteria.** Basic zoning standards and allowances embodied within the code shall be presumed to be correct and are not subject to the appeal. Factual findings by the Commission will be accepted by the Council if they are supported by substantial evidence.

M.C. § 17.09.340

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#### 1. Lack of Public Notice and Opportunity to be Heard

M.C. § 17.09.315(A) provides: "Public Notice: Not less than fifteen (15) days prior to the date of the Initial Meeting, notice shall be published once in the official newspaper of the City and mailed, first class postage prepaid, to the owners of property within three hundred feet (300') of the external boundaries of the property which is the site of the project ("subject property"). Notice shall also be posted on the subject property not less than one week prior to the meeting."

Proper notice was provided for the Initial (First) Meeting with the DRC.

Public notice is not required by Code or Law for the Initial Meeting with Staff or Project Reviews. Neither of these is open to the public. Staff makes no decisions at the Initial Meeting or the Project Review, but merely provides information to the applicant about City requirements.

The public was allowed to testify at the First Meeting with the DRC prior to the DRC rendering its decision.

#### Applicable DRC Findings Challenged by Appellant:

- A4 - The applicant has completed a project review meeting on August 1, 2023 as required by M.C. § 17.09.325(B).
- A5 - The applicant has completed an initial meeting with staff on October 21, 2023 as required by M.C. § 17.325(D).

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## 2. Inadequate or Missing Information in the Application

1. Photos of View Corridors
2. Evidence of Neighborhood Context

Applicable DRC Finding Challenged by Appellant:

**A3** - The applicant has submitted all required materials for design review as provided by M.C. § 17.09.325(D) and (E).

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## 3. Project Approval Decisions Were Made Based on Incomplete Information

Traffic Study – Condition # 4 to the DRC Approval, upon recommendation by City Engineer.

Traffic, however, is not part of the Design Guidelines for the Downtown Core Zoning District. Further, objections to traffic impact are specifically disallowed by M.C. § 17.09.340(D).

Therefore, the DRC's Approval could not be conditioned on the completion of a Traffic Study.

Applicable DRC Finding Challenged by Appellant:

**A46** - The City Engineer has provided recommended conditions of approval for consideration by the DRC to ensure compliance with City Codes related to pedestrian safety, as noted below.

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#### 4. Design Guidelines – Ground Level Details – Have Not Been Met 6<sup>th</sup> Street Context and Condition

Applicable DRC Finding Challenged by Appellant:

**A22** - The existing sidewalk on Sherman Avenue from the back of curb to the property line is 14.8'. The distance from the new 5'x5' tree planting areas to the property line is approximately 8'-6". A 7'-0" wide clear pedestrian travel area will be maintained. (SIDEWALK USES – CLEAR WALKWAY)

**A23** - An 18" wide area between the property line and the pedestrian travel area will be used for planting containers along Sherman Avenue. (SIDEWALK USES – STOREFRONT AREA)

**A29** - The DC zoning district has a 0' front and side yard setback, unless providing usable public space, forecourts or vegetative screening of parking structures. Buildings may be set back from the sidewalk a maximum of 20' for public space or entries, or a maximum of 10' for vegetative screening. Setting façades close to the street may be accomplished through base structures that extend out to the sidewalk, not necessarily the full height of the building. The building meets this requirement. The street level façade along the Pedestrian-Oriented 6th Street is set up to the back of the sidewalk along the property line. A portion of the project on the corner of Sherman and 6th Street has a dining patio for the use of hotel guests but it has a base structure that extends out to the sidewalk. (MAXIMUM SETBACK)

**A30** - The proposed building is oriented to Sherman Avenue. The building façade along Sherman incorporates numerous windows as well as an entrance canopy and signage. The façade along 6th Street incorporates windows. The primary building entrance faces Sherman and is centered in the building façade. An outdoor patio at the ground level activates the street corner at Sherman Avenue and 6th Street. (ORIENTATION TO THE STREET)

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#### 4. Design Guidelines – Ground Level Details – Have Not Been Met, Continued 6<sup>th</sup> Street Context and Condition

Applicable DRC Finding Challenged by Appellant:

**A33** - The ground floor of the building has the most amount of character to provide visual interest to pedestrians, including: Pedestrian-scaled signs to identify the building entry; seasonal planting in multiple planters against the building along Sherman Avenue; metal canopies above the ground floor storefront windows; accent wall sconces on either side of the main entrance; and a decorative concrete plinth to ground the building. The ground level also features an elevated patio at the corner of Sherman and 6th Street to add a further level of detail in this area. (GROUND LEVEL DETAILS)

**A36** - The proposed design is in compliance with the treatment of blank walls. The streetfacing walls of the building are mostly broken up by windows and doors, but there are additional architectural features that break up the impact of the walls, including: a concrete plinth that varies in height depending on the grade change (from 1-2 up to 6'-0"); a change in brick materials above the ground floor level, acting as a "belt course" for the building; recess the façade at least 2'-0" in depth; and roof overhangs/canopies at the ground floor level and upper roof level that vary from 3'-0" to 5'-0" in depth. Additional features at the pedestrian level include contrasting wall material and vegetated planter boxes (TREATMENT OF BLANK WALLS)

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## 5. Design Guidelines – Unique Historic Features – Have Not Been Met

Street Trees  
Street Lights

Applicable DRC Findings Challenged by Appellant:

**A21** - The four existing street trees will be replaced with street trees per City standards and will include new 5'x5' tree planting areas around the trees.

**A28** - There is one existing single-arm tall streetlight at the corner of Sherman and 6th Street that will remain. There are two existing post streetlights along Sherman Avenue. One light will remain in its current location, and the other will be shifted to allow for the new curb cut into the parking structure. There are no existing streetlights along 6th Street. (LIGHTING INTENSITY – STREET LIGHTING)

**A40** - The DC design guidelines require new projects to relate to the context of the Downtown's historical features. The existing site is a vacant property with lawn, so the project doesn't include any renovation or redevelopment. As a new construction project, the proposed building relates to the surrounding context through: the use of brick as a predominant exterior finish; the massing of the building with a base, middle, and top; the scale of the building as a steppingstone between the smaller buildings along Sherman Avenue and the high-rise residential Parkside Building. The design of the building is a contemporary structure that relates to the primarily modern surrounding architecture. (UNIQUE HISTORIC FEATURES)

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## DECISION POINT

### Municipal Code § 17.09.340

E. Council Action: **The Council may affirm or reverse the Commission decision, or refer the project back to the Commission for further action or clarification.** The Council shall issue its decision within fifteen (15) days of the appeal hearing. If the project is referred back to the Commission, the Commission shall hold a public meeting to consider the referral and shall render a report to the Council within forty (40) days of such referral. The City Council shall then reconvene the appeal hearing to consider the report and render a final decision as prescribed in this section.

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# ADMINISTRATIVE APPEAL APPLICATION

**STAFF USE ONLY**  
Date Submitted: 2-9-24 Received by: TC Fee paid: 500.00 Project # DR-1-24 AA

## REQUIRED SUBMITTALS

Design Review Appeal: \$500.00  
Planning Commission/Administrative: \$700.00

A **COMPLETE APPLICATION**, as determined by the Planning Department, is required at time of submittal. Application forms can be obtained at <http://cdaid.org/1105/departments/planning/application-forms>.

## DEADLINE FOR SUBMITTALS

The completed form must be submitted to the Planning Department not later than ten (10) days following the date of the decision, administrative action, or interpretation to be appealed.

- Completed application form
- Information that may be required to facilitate review
- Fee \$700.00

## NATURE OF APPEAL:

PROJECT NAME: <u>CDA Hotel, LLC (Marriott Hotel)</u>
ADDRESS: <u>602 &amp; 612 E. Sherman Avenue</u>
DATE OF DECISION OR OTHER ACTION: <u>Jan. 25, 2024</u>
PROJECT NUMBER:

- Appeal of Design Review Commission's decision, administrative action, or interpretation (Action Appeal)
- Appeal of Planning Commission's decision, administrative action, or interpretation (Action Appeal)

## APPELLANT INFORMATION:

*\*Please attest that you have standing to appeal the project. This means that you are: 1) a resident of the City of Coeur d'Alene; 2) a person having an interest in real property in the City of Coeur d'Alene; and/or 3) a person with an interest in real property located within three hundred feet (300') of the external boundaries of the land subject to the decision or other action.*

NAME: <u>Joan Woodard</u>		
MAILING ADDRESS: <u>609 E. Sherman Ave. #401</u>		
CITY: <u>Coeur d'Alene</u>	STATE: <u>ID</u>	ZIP: <u>83814</u>
PHONE: <u>707-479-5090</u>	FAX: <u>---</u>	EMAIL: <u>jeanwoodard@outlook.com</u>

**RECEIVED**  
FEB 09 2024  
BY: IRAC, CLARK

**APPLICATION INFORMATION:**

PROPERTY OWNER: CDA Hotel, LLC		
PROPERTY ADDRESS: 602 & 612 E. Sherman Avenue		
CITY: Coeur d'Alene	STATE: ID	ZIP: 83814
PHONE: 406-595-4560	FAX: —	EMAIL: plange@providencedevco.com

**APPEAL JUSTIFICATION:**

State specifically the nature of the appeal.

You must state specifically your objection(s) to the decision or other action, stating whether you believe there was an abuse of discretion and/or whether you believe the decision or other action was not supported by the evidence. You must include any information that supports your contentions in order to facilitate review. Please fill out all boxes below.

1. State the basis of your appeal (abuse of discretion, lack of evidence, etc.): (Required)

Omission of facts, abuse or lack of discretion, lack of evidence, not following guidelines & goals of the Comprehensive Plan, selective interpretation, violations of the intent of City Code

2. Identify the decision or action you believe was in error: (Required)

Design Review Commission found "findings of fact" in error upon which they based their unanimous approval. Approval should have been denied and sent back to the Applicant for the facts and information needed.

3. State the information that supports your appeal (e.g., evidence of record that does not support the decision, findings, etc.):

Please see the attached 15 page summary

I Joan C. Woodard certify that I have standing to appeal the decision.  
(Name of Appellant)

Dated this 9<sup>th</sup> day of Feb., 2024.

[Handwritten Signature]  
Signature

Subscribed and sworn to before me this 9<sup>th</sup> day of Feb., 2024

[Handwritten Signature]

Notary Public for State of Idaho

Residing at: 2900 N Gov'tway 83815

My commission expires: 8/4/2029





**Information to facilitate the review of this appeal of the Design Review Committee's approval of the Application by CDA Hotel, LLC (Mariott Hotel) on January 25, 2024**

1. The Basis of the Appeal includes lack of community notice and input, omission of facts, abuse of discretion in light of specific site conditions, and lack of supporting evidence.
  - a. Lack of Public Notice and Opportunity to be Heard. 17.09.325 A. of the City Code states that "A development applicant shall participate in the design review process as required by this Article *before* substantive design decisions are fixed and difficult or expensive to alter. The City will work with the applicant in a collaborative fashion so that the goals of both the City and the applicant can be met to the greatest degree possible, *and to address the concerns of neighbors and the community*". Yet, in this case, the community had no notice, and no opportunity to be heard or incorporate the concerns of neighbors and the community, until everything except for items within the limited scope of DRC review had already been decided and approved by the staff. To wit: (1) The public was not given notice of the 8/1/23 meeting between the applicant and the City; (2) The public was not given notice of the 10/21/23 meeting between the applicant and the City; and (3) No notice to the public was given regarding this project until notice of the Design Review Commission hearing to be held 1/25/24 meeting was published on 1/6/24; and (4) All of the City's decisions regarding this project made prior to the Design Review Commission hearing on 1/25/24 were made without notice and without regard to concerns of the community. Section 17.09.325 A contemplates a process of public meetings with affected neighbors and community members, along with City staff and the applicant that would occur before the Applicant finalizes the application for submittal to Design Review. This did not occur. *No input from neighbors or the community was sought by either the city staff or the applicant.* The City's Comprehensive Plan also establishes that concerns of neighbors and the community should be included. Goal CI 1 states that "Coeur d'Alene citizens are well informed, responsive, and involved in community discussions. There was no involvement opportunity provided for this project before a limited, narrow review by the Design Review Commission with city staff urging approval. City staff were also not forthcoming with information about the project before the issuance of the Design Review information, again restricting the opportunity for community understanding or involvement.
  - b. Inadequate or missing information in the Application. 17.09.325 D 4. Mandates that the written narrative portion of the application will include "a description and photos detailing proximity to major roads, view corridors, and neighborhood context". There was *no evidence* of material containing view corridors and *insufficient evidence* of neighborhood context for ascertaining the degree to which the project meets the downtown core design guidelines. If view corridors had been analyzed, the city would have been made aware that the massing of this proposed building will violate one of the core principles of the City's Comprehensive Plan as it relates to downtown..."Preserve

## Information to facilitate the review of this appeal of the Design Review Committee's approval of the Application by CDA Hotel, LLC (Mariott Hotel) on January 25, 2024

views of Tubbs Hill and other distant landforms". This building, as presently designed, will eliminate all views of Lake Coeur d'Alene for anyone on foot or in an automobile heading west until they get beyond the Coeur d'Alene Resort. Suddenly, the beautiful impact of being a very special "lake town" will be replaced by a downtown vision that could be any town in the country. This can't have been what was contemplated when the design guidelines were put in place. The required information on the view corridors, including photos of existing conditions and images of resulting conditions based on the submitted design should be required and discussed to determine if the project will meet the guidelines specific to views.

- c. Project approval decisions were made based on incomplete information. Section 17.09.330 states, "The Commission shall apply the collective judgment of its members to determine how well a project comports with the adopted standards and guidelines, and it may impose reasonable fact-based conditions to ensure better or more effective compliance with those standards and guidelines. The Commission may also exercise discretion to reconcile the adopted standards and guidelines with site-specific conditions in order to meet the intent of the Zoning Code." One of the conditions added to the approval of this application was that the applicant shall provide a traffic study. The City Engineer stated in an email to the appellant subsequent to the decision by the Design Review Commission "It is worth mentioning that approval of their building permit is not contingent on the traffic study as the current zoning allows for the proposed use up to a maximum of 220 feet in height." Had this statement been provided to the Commission during their deliberations, I believe after discussion they would have used the collective judgment to require this study to be completed and to come back to Design Review with the results of this traffic study and not have approved this application without that. There are a number of site-specific conditions with this property and the proposed use that make traffic a very significant issue and might require changes to the project as presented to mitigate those issues. As proposed the 131 hotel rooms, with over 200 seats for restaurant and lounge activities, present the opportunity to bring well over 300 cars to downtown each day that are not already accounted for. This would indicate that serious consideration needs to be given to all the impacts that automobiles introduce. When a traffic study and information on view corridors are made available as part of the Design Review Commission hearing, the commission is permitted and should be expected to apply their collective judgment to these new facts to ensure compliance with both the standards and guidelines articulated in the Comprehensive Plan, Downtown Design Guidelines and City Code.
- d. The Downtown Design Guidelines have not been met. In particular, the Guideline concerning "Ground Level Details" was not addressed for 6<sup>th</sup> Street. Only one (out of a requirement for five) of the elements on the list of thirteen elements was included in the submittal, however, other elements may be added beyond the thirteen if they meet the intent. 6<sup>th</sup> Street is an important "Pedestrian-Oriented Street". It is heavily utilized as a direct pedestrian corridor to McKuen Park, Tubbs Hill, City Parking, and many other

**Information to facilitate the review of this appeal of the Design Review Committee's approval of the Application by CDA Hotel, LLC (Mariott Hotel) on January 25, 2024**

amenities enjoyed by residents and visitors alike. It is too important as a visual and for safety reasons to leave this requirement of "encouraging the greatest amount of visual interest along the ground level of buildings facing downtown streets" to the applicant's discretion. The Commission should have required specific information on how the 6<sup>th</sup> street façade would meet the requirements of the "Ground Level Details" section of the Design Guidelines.

- e. The Downtown Design Guidelines have also not been met concerning "Unique Historic Features". Specifically, "Relating New Construction to Context". There is also a violation of Comprehensive Plan Goal C1 2.2 "Support programs that preserve historical collections, *key community features*, *cultural heritage*, and *traditions*." A key community feature of downtown Coeur d'Alene is the attention to and importance of street trees and historic street lights. Both are key community features during the holidays with lights on both trees and street lights. During the summer, the shade provided by the tree canopies is important for pedestrians and ground floor retail. The historic street lights provide an added sense of safety in mid-block locations where the large intersection lights are not providing illumination. A feature of the street lights and the trees is that they follow a cadence, alternating street lights with street trees. They also should match from one side of Sherman to the other. This project has not addressed the cadence at all and the proposed revised locations for the street trees and lights on Sherman will violate that rhythm. The Applicant should be required to add street trees and lights to mimic the established cadence.
- f. The "Unique Historic Features" and key community features have also not been addressed for the 6<sup>th</sup> Street side of the building. Current conditions on 6<sup>th</sup> Street were not disclosed to the Design Review Commission, and they should have been as they are pertinent to this topic. Current conditions on the east side of 6<sup>th</sup> Street include a sub-optimal sidewalk between Sherman and the alley that ranges from 7 feet to slightly less than 7 feet. Between the alley and Front Street (Parkside Condominium), the sidewalks are 12 feet. On the West side of 6<sup>th</sup> street, the sidewalks are between 10 and 12 feet. Against the historic Masonic Building, the sidewalks are greater than 12 feet. This is the context in which the commission should be exercising discretion based on facts to impose reasonable fact-based conditions to ensure better or more effective compliance with those standards and guidelines. The 7-foot width of the sidewalk is inadequate at and should be required to have at least a 10 foot width. This would reconcile the adopted standards and guidelines with site-specific conditions in order to meet the intent of the Zoning Code. It would also be a respectful reflection of the grandeur of the Masonic Building to have a more stately sidewalk experience for both sides of the street. It would "relate new construction to context", as required. In addition to the inadequate width of the sidewalk, the existing conditions concerning street trees and historic light fixtures were not disclosed and should have been. Both sides of 6<sup>th</sup> Street have street trees and lights except for the subject property. Not requiring a consistent approach to these elements for the length of the block based on the factual existing conditions is an oversight that needs to be addressed. Based on concern I expressed this week to the

**Information to facilitate the review of this appeal of the Design Review Committee’s approval of the Application by CDA Hotel, LLC (Mariott Hotel) on January 25, 2024**

City Engineer on this subject, I’m informed that he has since measured the sidewalk distances and street width and confirmed that a 10 foot sidewalk can be implemented by removing the current jog in the road and still maintain the existing street parking, and the city will require the developer to re-make this sidewalk, along with street trees and lights. This is good news for the width of the sidewalk, however, it doesn’t address the other shortcomings that are somewhat related to the sidewalk and specifically related to the massing of the building that eliminates the view corridor and the need for “ground level details” on 6<sup>th</sup> street. This project should be designed with both a wider sidewalk to allow for planter boxes or planter insets, as occur at the edge of the Parkside condominium building. A sidewalk greater than 12 feet could also present the beginning of a solution to the view corridors problem. Input from the traffic study, specifically with regard to 6<sup>th</sup> street, also needs to be part of the equation as removing the jog and still having street parking (which we can’t afford to lose) could make the traffic problem at the intersection of 6<sup>th</sup> and Sherman untenable. Crossing Sherman on 6<sup>th</sup> is already dangerous. Adequate turn lanes will likely be recommended (they don’t currently exist).

2. The above omission of facts, abuse of discretion in light of specific site conditions, and lack of supporting evidence suggest that the following “Findings of Fact” contained in the Design Review Commission Findings and Order are not true or not completely factual, and are therefore erroneous:

A3

A4

A5

A21

A22 (facts related to 6<sup>th</sup> Street were omitted)

A23 (facts related to 6<sup>th</sup> Street were omitted)

A28

A29 (omits context)

A30 (facts related to 6<sup>th</sup> Street were omitted)

A33 (facts related to 6<sup>th</sup> Street were omitted)

A36 (facts related to 6<sup>th</sup> Street were omitted)

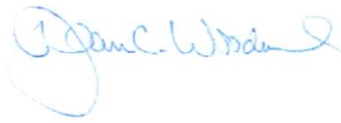
A40 (facts related to context and to 6<sup>th</sup> Street were omitted)

A46 a traffic study after the fact is meaningless to good planning

**Information to facilitate the review of this appeal of the Design Review Committee's approval of the Application by CDA Hotel, LLC (Mariott Hotel) on January 25, 2024**

3. Based on the above, the proposal should have been found NOT in conformance and should not have been approved. It should be required to go back to staff with direction to address all the above facts and should be done in a fashion that meets the goals and objectives of the Comprehensive Plan for Community and Identity, including adequate public notice and collaboration with neighbors.

Respectfully submitted:



Joan C. Woodard

February 9, 2024

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**COEUR D'ALENE DESIGN REVIEW COMMISSION  
FINDINGS AND ORDER**

**DR-1-24**

**A. INTRODUCTION**

**This matter came before the Design Review Commission (“DRC”) on January 25, 2024, on DR-1-24, a request for a six (6) story hotel with below grade parking along Sherman Avenue and 6th Street in Downtown Coeur d'Alene.**

APPLICANT: CDA HOTEL LLC

LOCATION: The subject property is legally described as CDA & Kings ADD, LTS 1, 2, 3 and 4, BLK 35, Commonly known as 602 & 612 E. Sherman Avenue.

**A. FINDINGS OF FACT:**

**The DRC finds that the following facts, A1 through A46, have been established on a more probable than not basis, as shown on the record before it and on the testimony presented at the public hearing.**

- A1. The subject property is located at 602 and 612 E. Sherman Avenue in the Downtown Core (DC) zoning district, which requires review and approval of the design by the City's DRC.
- A2. The property is subject to the Downtown Core Design Guidelines and the Downtown Development Standards. M.C. Chapter 17.05, Article XI, and § 17.05.705.
- A3. The applicant has submitted all required materials for design review as provided by M.C. § 17.09.325(D) and (E).
- A4. The applicant has completed a project review meeting on August 1, 2023, as required by M.C. § 17.09.325(B).
- AS. The applicant has completed an initial meeting with staff on October 21, 2023, as required by M.C. § 17.325(D).
- A6. The applicant is seeking design review from the DRC at an initial meeting on January 25, 2024.
- A7. The notice of public hearing was published on January 6, 2024, which fulfills the legal requirement for Design Review as provided by M.C. § 17.09.315(A).
- A8. The notice of public hearing was posted on the property on January 11, 2024, which fulfills the proper legal requirement as provided by M.C. § 17.09.315(A).
- A9. One hundred thirty-six (136) notices of public hearing were mailed to all property owners of



record within three hundred feet (300') of the subject property on January 10, 2024, which fulfills the legal requirement as provided by M.C. §17.09.315(A).

- A10. Public testimony was received by the DRC at a public hearing on January 25, 2024.
- A11. The subject property is 22,993 S.F. +/- 0.482 acre as shown by the application and verified by GIS.
- A12. The existing zoning is Downtown Core District as shown by the City's zoning map.
- A13. Sherman Avenue, in the project vicinity, is designated as a Vehicle-Oriented Street pursuant to the City's Downtown Design Guidelines.
- A14. 6<sup>th</sup> Street, in the project vicinity, is designated as a Pedestrian-Oriented Street pursuant to the City's Downtown Design Guidelines.
- A15. The project is below the allowable floor area ratio (FAR) as provided in M.C. § 17.05.685(A). The maximum allowed FAR in the DC zoning district is 4.0. The project shows a FAR of 0.994 based on a lot size of 20,993 square feet and a building square footage of 20,886 square feet. The applicant does not need and has not requested any FAR bonuses.
- A16. The proposed project would be 6 stories and 75' tall, which is below the maximum allowable height of 200' in the DC zoning district pursuant to M.C. § 17.05.690(A).
- A17. The 50' tower spacing requirements does not apply because the building would not be taller than 75' pursuant to M.C. § 17.05.695(B).
- A18. M.C. §17.05.725(A)(3) requires 0.5 parking stalls per unit in the DC zoning district. The proposed project has 131 hotel rooms and provides 130 parking spaces enclosed within the structure, which is 65 more than is required by City Code and the Downtown Development Regulations.
- A19. The proposed canopy meets the minimum depth to provide weather protection per the DC design guidelines. The DC design guidelines require a minimum depth of a canopy or awning to be 5'. The 5' deep canopies associated with the building meet the minimum requirement to provide protection for pedestrians from weather. The exception is at the main entry, which has a shorter canopy at 3.5 feet in depth. However, the recessed entry provides additional protection. The applicant has requested a **design departure for Weather Protection related to the vertical dimension between the underside of the canopy or awning and the sidewalk**. Per the DC design guidelines, the vertical dimension between the underside of the canopy or awning and the sidewalk shall be at least 8' and no more than 12'. The proposed design has a canopy height starting at 9'11" above the sidewalk and has a clearance for pedestrian and vehicular safety signage suspended from the canopy above the parking garage entrance at a height of 9'11". As the sidewalk slopes down at an average of 2.8% to the west, the canopy's vertical height increases to 14'11" at the northwest corner of the project, which is 2'11" above the maximum allowable height. Along 6th Street at the lowest grade, the canopy would have a vertical dimension of 17'10". The requested design departure is to exceed a portion of the canopy to extend above the 12' maximum design guideline. The architect outlines the justification as the departure of the canopy height would still meet the weather protection requirement for pedestrians, the canopy would maintain a consistent horizontal aesthetic that would allow for the storefront windows to remain a consistent size and allow for maximum interior daylight. Stepping down of the canopy to meet the guideline would adversely affect the aesthetic quality of the architecture. Maintaining a consistent horizontal plane with the canopy also defines the base of the building, which is an important aspect of the design guidelines. The canopy will have a metal frame finish, with a wood plank soffit. These canopies will also have recessed

downlights to provide lighting under the opaque covering. The applicant maintains the design of the proposed canopy with the increased vertical dimension and overall aesthetic is a significant improvement over what could have otherwise been built under minimum standards and guidelines. The applicant provided references to applicable sections of the Comprehensive Plan, including **Community & Identity**: Goal CI 2 (*Maintain a high quality of life for residents and businesses that make Coeur d'Alene a great place to live and visit*), Objective CI 2.1 (*Maintain the community's friendly, welcoming atmosphere and its small-town feel*), and Objective CI 2.2 (*Support programs that preserve historical collections, key community features, cultural heritage, and traditions*), and the key characteristics of the Downtown as highly walkable with a defined urban form that attracts area residents and tourists to the area. The design departure request includes two exhibits showing how the canopy would look if it were to meet the guideline. (WEATHER PROTECTION) - **DESIGN DEPARTURE REQUESTED**

- A20. The following Downtown Design Guidelines and development standards are not applicable: Screening of Parking Lots, Parking Lot Landscaping, and Gateways. The 131 parking spaces for the project are fully enclosed within the building and, therefore, would not trigger parking lot screening or landscaping requirement. The location of the subject property is not in a Gateway. The Gateways are key intersections within and around the edges of downtown that require special treatment and include the intersections of Sherman Ave. and Second St., Sherman Ave. and Fourth St., Front Ave. and Fourth St., and Sherman Ave. and Seventh St.
- A21. The four existing street trees will be replaced with street trees per City standards and will include new 5'x5' tree planting areas around the trees.
- A22. The existing sidewalk on Sherman Avenue from the back of curb to the property line is 14.8'. The distance from the new 5'x5' tree planting areas to the property line is approximately 8' 6". A 7' 0" wide clear pedestrian travel area will be maintained. (SIDEWALK USES - CLEAR WALKWAY)
- A23. An 18" wide area between the property line and the pedestrian travel area will be used for planting containers along Sherman Avenue. (SIDEWALK USES - STOREFRONT AREA)
- A24. Three (3) existing curb cuts will be removed - one along 6th Street and two along Sherman Avenue. Only one new 24" wide curb cut will be required on Sherman Avenue for the project. No curb cuts will be on 6th Street, which is a pedestrian-oriented street. For the new curb cut required for the driveway into the parking structure, the sidewalk pattern and material will carry across the driveway. (WIDTH AND SPACING OF CURB CUTS)
- A25. The trash area will be located behind the building off of the alley on the southeast corner of the property and will be screened from view on all sides. The enclosure will be constructed with brick to match the building and will have an opaque decorative architectural gate. (SCREENING OF TRASH)
- A26. Loading and service areas will be located within the parking structure. (SCREENING OF SERVICE AREAS)
- A27. Exterior lighting on the building will be recessed in the roof canopies at the ground floor-level to provide pedestrian lighting. Guestroom balcony roofs will have lighting and the upper roof deck will have lighting to highlight the building corner. Fully shielded wall sconces will be provided on either side of the main entry doors. (LIGHTING INTENSITY BUILDING LIGHTING)
- A28. There is one existing single-arm streetlight at the corner of Sherman and 6th Street that will remain. There are two existing post street lights along Sherman Avenue. One light will remain in its current location, and the other will be shifted to allow for the new curb cut into the parking structure. There are no existing street lights along 6th Street. (LIGHTING INTENSITY - BUILDING LIGHTING)

- A29. The DC zoning district has a 0' front and side yard setback, unless providing usable public space forecourts or vegetative screening of parking structures. Buildings may be set back from the sidewalk a maximum of 20' for public space or entries, or a maximum of 10' for vegetative screening. Setting facades close to the street may be accomplished through base structures that extend out to the sidewalk, not necessarily the full height of the building. The building meets this requirement. The street level facade along the Pedestrian-Oriented 6th Street is to the back of the sidewalk along the property line. A portion of the project on the corner of Sherman and 6th Street has a dining patio for the use of hotel guests, but it has a base structure that extends out to the sidewalk. (MAXIMUM SETBACK)
- A30. The proposed building is oriented to Sherman Avenue. The building facade along Sherman incorporates numerous windows as well as an entrance canopy and signage. The facade along 6th Street incorporates windows. The primary building entrance faces Sherman and is centered in the building facade. An outdoor patio at the ground level activates the street corner at Sherman Avenue and 6th Street. (ORIENTATION TO THE STREET)
- A31. The DC Design Guidelines require the principal entry to have two elements. The main building entrance is centered on the facade along Sherman Avenue and welcomes pedestrians with an overhanging canopy as well as a recess in the main building wall. Those are both allowed design elements. Some form of weather protection shall also be provided. Both the canopy and the recess provide added weather protection for pedestrians. These features, along with clear signage, help identify this visually prominent entrance (ENTRANCES)
- A32. The proposed structure incorporates a top, middle and base, as required by the DC zoning district (MASSING)
- The top section of the building is distinguished by overhanging roofs, an open roof deck with trellis, and additional windows. The main material is a dark metal panel, with accent metal panels. (TOP)
  - The middle section of the building has a regular pattern of guestroom windows surrounded by dark and light color brick veneer. Also, there are some dark and accent metal panels to connect the base to the top. (MIDDLE)
  - The base of the building features a large amount of storefront glazing and canopies to define the ground level. The finish is a combination of light grey brick, darker composite panels accented with horizontal wood siding with a decorative concrete plinth. (BASE)
  - The base of the building aligns with the property lines of the lot, but steps back above the ground floor level to allow for the required 10-foot setback over 45 feet above grade. The only parts of the building that extend past these setbacks are roof overhangs and balconies. The only part of the building that is taller than 75 feet is the elevator penthouse, which is much smaller than the 8000 SF Tower Floor Size restriction at 176 SF, and is over the minimum Tower Separation of 50 feet noted in the Site Performance Standards. At approximately 77 feet tall, the overall building height is well below the maximum 200 ft building height. (BUILDING BULK)
  - Sherman Avenue has a mix of low-and mid-rise buildings, which align well with the scale of the plinth of the proposed hotel. The overall mass of the building helps transition from these shorter structures to the high-rise residential buildings on Front Avenue. (CITY BLOCK ELEVATIONS)
- A33. The ground floor of the building has the most amount of character to provide visual interest to pedestrians, including: Pedestrian-scaled signs to identify the building entry; seasonal planting in multiple planters against the building along Sherman Avenue; metal canopies above the ground floor storefront windows; accent wall sconces on either side of the main entrance; and a decorative concrete plinth to

ground the building. The ground level also features an elevated patio at the corner of Sherman and 6<sup>th</sup> Street to add a further level of detail in this area. (GROUND LEVEL DETAILS)

- A34. The proposed structure would meet the minimum glazing requirement for Ground Floor Windows by providing 40% window and glazed door area in the "window zone" of the façade along Sherman Avenue and a "26% "window" area" in the window zone along the 6th Street façade (GROUND FLOOR WINDOWS)
- A35. The DC Design Guidelines require a visual connection between activities inside and outside the building. Ground level facades oriented to pedestrian-oriented streets require a minimum of 60% transparency and vehicular-oriented streets require a minimum of 40% transparency. The proposed structure meets the transparency requirement for ground floor windows with a minimum of 60% transparency. (GROUND FLOOR WINDOWS)
- A36. The proposed design is in compliance with the treatment of blank walls. The street-facing walls of the building are mostly broken up by windows and doors, but there are additional architectural features that break up the impact of the walls, including: a concrete plinth that varies in height depending on the grade change (from 1-2 up to 6'-0"); a change in brick materials above the ground floor level, acting as a "belt course" for the building; recess the façade at least 2'-0" in depth; and roof overhangs/canopies at the ground floor level and upper roof level that vary from 3'-0" to 5'-0" in depth. Additional features at the pedestrian level include contrasting wall material and vegetated planter boxes (TREATMENT OF BLANK WALLS)
- A37. The parking for the project is screened by being designed as part of the building. Other than the entrance, the parking is hidden from view. The main floor parking is integrated into the "plinth" on the no-street facing facades. (SCREENING OF PARKING STRUCTURES)
- A38. The building design doesn't include any pitched roofs. The typical roofline of the building includes a 3' overhanging cornice to create a prominent edge against the sky. At recessed wall locations, this overhang extends 5'6" feet past the wall face, creating an even more dramatic cornice. Additionally, the building features accent tower elements of varying heights and a roof deck with a large trellis to add increased interest at the roof edge. (ROOF EDGE)
- A39. The proposed building is designed with extended parapets to screen a majority of the rooftop equipment. The only rooftop mechanical equipment that extends above the main parapet is the Elevator Penthouse, which will be surrounded by a framed wall and finished in the same dark metal panels as part of the main building façade. (SCREENING OF ROOFTOP MECHANICAL EQUIPMENT)
- A40. The DC design guidelines require new projects to relate to the context of the Downtown's historical features. The existing site is a vacant property with lawn, so the project doesn't include any renovation or redevelopment. As a new construction project, the proposed building relates to the surrounding context through: the use of brick as a predominant exterior finish; the massing of the building with a base, middle, and top; the scale of the building as a steppingstone between the smaller buildings along Sherman Avenue and the high-rise residential Parkside Building. The design of the building is a contemporary structure that relates to the primarily modern surrounding architecture. (UNIQUE HISTORIC FEATURES)
- A41. The two main building signs are placed on the vertical-wood-siding-finished vertical towers of the building for wayfinding by automobile traffic, in lieu of pylon signs. These signs are 188 SF and 36 SF, respectively. Additionally, channel letter signs are located above the ground floor canopies to designate the main entrance and the parking entrances. These signs are 42 SF for the main entrance and 14 SF (each) for the two parking entrances. There are two placard signs on either side of the main

entrance doors for pedestrian wayfinding. (INTEGRATION OF SIGNS WITH ARCHITECTURE)

- A42. The signage for the building was selected from the Brand's standard signage options. Their designs are highly graphic for brand identity, but also offer a variety of installations and styles including typical wall signs, channel letter wall signs, freestanding channel letter entry signs, as well as smaller pedestrian-oriented placard signs at the entry doors. The freestanding channel letter sign at the entry canopy is supported by brackets and directs pedestrians to the building entry. (CREATIVITY/INDIVIDUALITY OF SIGNS)
- A43. The total building signage would total 302 square feet, which would be under the City's maximum sign allowance of 603 square feet under the Sign Code based on the property frontage. (SIGN ALLOWANCE)
- A44. The DC zoning district requires that building floors over 45' in height above grade shall be stepped back 10' from the right-of-way on 6th Street. The project design does meet this requirement. The base of the building aligns with the property lines of the lot, but steps back above the ground floor level to allow for the required 10-foot setback over 45 feet above grade. The only parts of the building that extend past these setbacks are roof overhangs and balconies. (UPPER LEVEL STEPBACK)
- A45. The Planning Department has provided a recommended condition of approval relating to consistency with the approved design, as noted below.
- A46. The City Engineer has provided recommended conditions of approval for consideration by the DRC to ensure compliance with City Codes related to pedestrian safety, as noted below.

The DRC heard testimony from the public and the applicant, and based on the public record adopt all 46 Findings of Fact. The DRC concludes that the proposal is in conformance with the applicable design standards and the request for the design departure satisfies the criteria in accordance with a design departure. The increased height of the canopy for snow and rain protection would not have a detrimental effect on the project. The building does provide a high degree of craftsmanship, building design and quality of materials. This is a thoughtful and comprehensive approach to the design.

## **B. CONCLUSIONS OF LAW**

**Based on the foregoing Findings of Fact, the Design Review Commission makes the following Conclusions of Law.**

1. This proposal is in conformance with the following applicable Downtown Development Standards and other applicable Municipal Code requirements:
  - Floor Area Ratio (FAR)
  - Height
  - Upper Level Stepback
  - Required Parking Ratio
  - Street Trees
  - Sign Allowance
  - Curb Cuts
2. The requested Design Departure for Weather Protection has satisfied the criteria for approving a design departure.
  - The requested departure does meet the intent statements relating to applicable development standards and design guidelines.
  - The departure will not have a detrimental effect on nearby properties or the city as a whole.

- The project's building does exhibit a high degree of craftsmanship, building detail, architectural design, or quality of materials that are not typically found in standard construction. In order to meet this standard, an applicant must demonstrate to the planning director that the project's design offers a significant improvement over what otherwise could have been built under minimum standards and guidelines.
  - The proposed departure is part of an overall, thoughtful and comprehensive approach to the design project as a whole .
  - The project is consistent with the comprehensive plan and any applicable plan.
3. This proposal is in conformance with the Downtown Design Guidelines with regard to the following design standards and guidelines with conditions:
- Location of Parking
  - Sidewalk Uses (Amenity Zones, Clear Walkways, and Storefront Area)
  - Width and Spacing of Curb Cuts
  - Screening of Trash/Service Areas
  - Lighting Intensity
  - Maximum Setback
  - Orientation to the Street
  - Entrances
  - Massing
  - Ground Level Details
  - Ground Floor Windows
  - Weather Protection (***NOTE: Design Departure requested***)
  - Treatment of Blank Walls
  - Screening of Parking Structures
  - Roof Edge
  - Screening of Rooftop Mechanical Equipment
  - Unique Historic Features
  - Integration of Signs with Architecture
  - Creativity/Individuality of Signs

### C. DECISION

**The Design Review Commission, pursuant to the foregoing Findings of Fact and Conclusions of Law, grants design review approval of the application for a six (6) story hotel with below story hotel with below grade parking along Sherman Avenue, located at 602 & 612E. Sherman Avenue, Coeur d'Alene, Idaho, with the following conditions.**

#### Conditions:

1. The proposed design shall be substantially similar to those submitted with Item DR-1-24.
2. Sidewalks along Sherman Ave and 6<sup>th</sup> Street must be brought into compliance with the ADA.
3. Any existing driveway approaches not being used with the proposed development shall be removed.
4. The applicant shall complete a traffic study including a pedestrian safety study.
5. Pedestrian safety features recommended by the study and approved by the City shall be installed.
6. Explore with staff the opportunity to enhance the concrete band and wall along 6<sup>th</sup> Street to enhance the pedestrian-oriented street. Consider architectural features and/or artwork.

Motion by Commissioner Ingalls, seconded by Commissioner Pereira, to adopt the foregoing Findings of Fact, Conclusions of Law, and Order, and grant design review approval of the application.

ROLL CALL:

Commissioner Priest	Voted Aye
Commissioner Ingalls	Voted Aye
Commissioner Snodgrass	Voted Aye
Commissioner Pereira	Voted Aye
Commissioner Lemmon	Voted Aye
Chairman Messina	Voted Aye

Motion to approve carried by a 6 to 0 vote.



CHAIRMAN TOM MESSINA

Dated: February 21, 2024



# MINUTES





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**DESIGN REVIEW COMMISSION  
MINUTES**

**JANUARY 25, 2024**

**Conference Room #6, City Hall**

**THURSDAY**

**12:00 pm**

**COMMISSIONERS PRESENT:**

Greta Snodgrass  
Tom Messina (Chairman)  
Michael Pereira (Vice-Chairman)  
Jef Lemmon  
Jon Ingalls  
Skip Priest

**STAFF MEMBERS PRESENT:**

Hilary Patterson, Community Planning Director  
Tami Stroud, Associate Planner  
Traci Clark, Administrative Assistant

**CALL TO ORDER:**

The meeting was called to order by Chairman Messina at 12:00 p.m.

**MINUTES: \*\*\*ITEMS BELOW ARE CONSIDERED TO BE ACTION ITEMS**

November 9, 2023 – Design Review Commission Meeting

Motion by Commissioner Ingalls, seconded by Commissioner Priest, to approve the minutes of the Design Review Commission meeting on November 9, 2023. Motion Carried.

**COMMISSION COMMENTS:**

Chairman Messina stated how the hearing will proceed and what will take place. He explained what the Design Review Commission does and the guidelines they have to go by. The decision the Commission makes is based on the strict guidelines. Other items such as parking, height, etc. it is not discussed during this hearing. Those items do not influence the decision of the Commission. The Applicant is doing this by right, and staff will educate us what the zoning is and what they can do on this piece of property following strict codes and zoning. This item does not have to go in front of the Planning and Zoning Commission or City Council.

Commissioner Priest would like to state he does not feel he has a conflict of interest. Staff had requested he bring this to the attention to the Commission and the Public. He is the President of the McEuen Homeowners' Association, which is a nearby building to this property. He is also on the Downtown Strategic Planning Community. This project was not brought up in those meetings. He is the neighborhood block watch captain, and works with the CDA Police Department. He does not believe they are a conflict of interest in anyway.

**STAFF COMMENTS:**

Tami Stroud, Associate Planner, stated regarding the public comments, the meeting for the Commission

will include a period of time for public comments, each person that wishes to comment shall be allowed the maximum of 3 minutes. Any public comment on the proposed project should be on matters related to the adopted design standards and guidelines. No comment shall be taken on matters which cannot be modified by the Commission, including, but not limited to, basic zoning requirements, nor area ratio, building height, density, use, parking, access, engineering, building codes, etc. The Chairman has the power to conduct the meeting in an orderly manner including a reasonably limited debate determining whether by comments by the applicant or the public are appropriate or within the purview of the Commission and ensuring that any decision that the Commission has arrived at collectively.

**PUBLIC COMMENTS:**

None.

**NEW BUSINESS**

1. Applicant: CDA Hotel, LLC  
Location: 602 & 612 E. Sherman Ave  
Request: Proposing to build six (6) story Marriott Hotel with a parking structure three stories underground for guest parking (DR-1-24)

Ms. Stroud provided the following statements:

Michael Nilson, architect with the Richardson Design Partnership, on behalf of CDA Hotel LLC, is requesting a First Meeting with the Design Review Commission for a 6-story Marriott Hotel. The applicant participated in a Project Review Meeting and an Initial Meeting with Planning Staff as required by Municipal Code § 17.09.325(D). The proposed project will have approximately 131 rooms, a fitness center, rooftop bar, outdoor patio, and parking structure for guest parking which continues three stories underground. The subject property is in the Downtown Core (DC) zoning district, and must adhere to the Downtown Coeur d'Alene Design Guidelines.

**DECISION POINT:**

Should the Design Review Commission approve the design for the 6-story Marriott Hotel located at 602 and 612 E. Sherman Avenue in the Downtown Core (DC) zoning district either with or without conditions, or direct modifications to the project's design and require a second meeting?

**PROJECT OVERVIEW**

The site is located on a 20,993 SF parcel along Sherman Avenue between 6<sup>th</sup> and 7<sup>th</sup> Streets. The property is currently vacant and is a grassy lot previously used for the "Live After 5" music events several years ago. The parcels are comprised of 2-lots that will be consolidated for building permit purposes. The property abuts Idaho Trust Bank directly to the east. Parkside Condominiums are located to the south, across the alley from the proposed hotel. The applicant is proposing a six-story (6) hotel structure with 131 guestrooms. A ground floor dining area with an outdoor patio, bar and fitness center will be available for hotel guests. A rooftop bar and lounge will be open to hotel guests and the public. Parking for hotel guests will be provided in the underground parking structure, which continues three stories underground with a total of 130 parking spaces, 8 of which are on the ground floor. The rooftop bar and lounge, open to the public, is exempt from parking because it is less than 3,000 S.F. The total height of the building is 77'-0" feet tall which includes the elevator penthouse, and is below the maximum height allowed in the Downtown Core (DC) which is 200' tall. The proposed project is located in the DC (Downtown Core) zoning district, and must adhere to the (DC) Downtown Core Design Guidelines and Standards.

**DC- Downtown Core** – This district is envisioned to have the highest intensity uses, especially retail, office, residences, and hotels contained within low-rise and high-rise buildings. Shops and restaurants would be located along key streets. Major public spaces and buildings would anchor the district. Over time, parking would be increasingly located within structures.

- The proposed design is in compliance with the treatment of blank walls. The street-facing walls of the building are mostly broken up by windows and doors, but there are additional architectural features that break up the impact of the walls, including: a concrete plinth that varies in height depending on the grade change (from 1'-2" up to 6'-0"); a change in brick materials above the ground floor level, acting as a "belt course" for the building; recesses in the façade at least 2'-0" in depth; and roof overhangs/canopies at the ground floor level and upper roof level that vary from 3'-0" to 5'-0" in depth. Additional features at the pedestrian level include contrasting wall material and vegetated planter boxes (TREATMENT OF BLANK WALLS)
- The parking for the project is screened by being designed as part of the building. Other than the entrance, the parking is hidden from view. The main floor parking is integrated into the "plinth" on the no-street facing façades.(SCREENING OF PARKING STRUCTURES)
- The building design doesn't include any pitched roofs. The typical roofline of the building includes a 3' overhanging cornice to create a prominent edge against the sky. At recessed wall locations, this overhang extends 5'6" feet past the wall face, creating an even more dramatic cornice. Additionally, the building features accent tower elements of varying heights and a roof deck with a large trellis to add increased interest at the roof edge. (ROOF EDGE)
- The proposed building is designed with extended parapets to screen a majority of the rooftop equipment. The only rooftop mechanical equipment that extends above the main parapet is the Elevator Penthouse, which will be surrounded by a framed wall and finished in the same dark metal panels as part of the main building façade. (SCREENING OF ROOFTOP MECHANICAL EQUIPMENT)
- The DC design guidelines require new projects to relate to the context of the downtown's historical features. The existing site is a lawn-covered dog park, so the project doesn't include any renovation or redevelopment. As a new construction project, the proposed building relates to the surrounding context through: the use of brick as a predominant exterior finish; the massing of the building with a base, middle, and top; the scale of the building as a steppingstone between the smaller buildings along Sherman Avenue and the high-rise residential Parkside Building. The design of the building as a contemporary structure that relates to the primarily modern surrounding architecture. (UNIQUE HISTORIC FEATURES)
- The two main building signs are placed on the vertical-wood-siding-finished vertical towers of the building for wayfinding of automobile traffic, in lieu of pylon signs. These signs are 188 SF and 36 SF, respectively. Additionally, channel letter signs are located above the ground floor canopies to designate the main entrance and the parking entrances. These signs are 42 SF for the main entrance and 14 SF (each) for the two parking entrances. There are two placard signs on either side of the main entrance doors for pedestrian wayfinding. (INTEGRATION OF SIGNS WITH ARCHITECTURE)
- The signage for the building was selected from the Brand's standard signage options. Their designs are highly graphic for brand identity, but also offer a variety of installations and styles including typical wall signs, channel letter wall signs, freestanding channel letter entry signs, as well as smaller pedestrian-oriented placard signs at the entry doors. The freestanding channel letter sign at the entry canopy is supported by brackets and directs pedestrians to the building entry. (CREATIVITY/INDIVIDUALITY OF SIGNS)
- The total building signage would total 302 square feet, which would be under the City's maximum sign allowance of 603 square feet under the Sign Code based on the property frontage. (SIGN ALLOWANCE)

- The DC zoning district requires that building floors over 45' in height above grade shall be stepped back 10' from the right-of-way on 6<sup>th</sup> Street. The project design does meet this requirement. The base of the building aligns with the property lines of the lot, but steps back above the ground floor level to allow for the required 10' setback over 45' above grade. The only parts of the building that extend past these setbacks are roof overhangs and balconies. UPPER LEVEL STEPBACK)
- The following design guidelines and development standards are not applicable: Screening of Parking Lots, Parking Lot Landscaping, and Gateways.
- The Planning Department has provided a recommended condition of approval relating to consistency with the approved design, as noted below.

The City Engineer has provided recommended conditions of approval for consideration by the DRC to ensure compliance with City Codes related to pedestrian safety, as noted below.

Chris Bosley, City Engineer provided comments during the project review meeting held on August 1<sup>st</sup>, 2023. An updated site plan was submitted and additional comments have been provided below based on the updated site plan and renderings submitted for the proposed hotel. The City Engineer will coordinate with the development team to discuss the proposed conditions on the following: The applicant shall complete a traffic study including a pedestrian safety study that illustrates how conflicts with pedestrians will be managed.

- Pedestrian safety features recommended by the study and approved by the City shall be installed during construction.
- Sidewalks along Sherman Ave and 6<sup>th</sup> Street must be brought into ADA compliance, including replacement of cracked and broken slabs.
- Any existing driveway approaches not being used with the proposed development shall be removed. The below conditions will need to be met prior to permit sign-off.

#### **STAFF EVALUATION OF FACTS**

- The subject property is located at 602 and 612 E. Sherman Avenue in the Downtown Core (DC) zoning district, which requires review and approval of the design by the City's Design Review Commission.
- The property is subject to the Downtown Core Design Guidelines and the Downtown Development Standards.
- The applicant has submitted all required materials for design review.
- The applicant has completed a project review meeting on August 1, 2023.
- The applicant has completed an initial meeting with staff on October 21, 2023.
- The applicant is seeking design review from the Design Review Commission at an initial meeting on January 25, 2024.
- 136 public hearing notices were mailed on January 10, 2024.
- The public hearing notice was published in the Coeur d'Alene Press on January 6, 2024.
- The subject property was posted with the public hearing notice on January 11, 2024.
- Sherman Avenue in the project vicinity is designated as a Vehicle-Oriented Street.
- 6<sup>th</sup> Street in the project vicinity is designated as a Pedestrian-Oriented Street.
- The applicant has requested a design departure for Weather Protection as noted below.
- The subject property is 20,993 square feet and the building square footage would be 20,886 square feet, which is 99.4% site coverage. This equates to less than 1.0 FAR, which is less than

is allowed by right with the DC zoning district. No FAR bonuses have been requested. (FAR BONUSES)

- The proposed project would be 6 stories and 75' tall, which is below the maximum allowable height of 200' in the DC zoning district. (BUILDING HEIGHT)
- The DC zoning district requires 0.5 parking stalls per unit. The proposed project would have 131 hotel rooms and provides 130 parking spaces enclosed within the structure, which is 65 more than is required by the Downtown Development Standards (Restaurants less than 3,000 S.F. are exempt from parking requirements.) (PARKING COUNT & LOCATION)
- The four existing street trees will be replaced with street trees per City standards and will include new 5'x5' tree planting areas around the trees. (SIDEWALK USES – AMENITY ZONES)
- The existing sidewalk on Sherman Avenue from the back of curb to the property line is 14.8'. The distance from the new 5'x5' tree planting areas to the property line is approximately 8'-6". A 7'-0" wide clear pedestrian travel area will be maintained. (SIDEWALK USES – CLEAR WALKWAY)
- An 18" wide area between the property line and the pedestrian travel area will be used for planting containers along Sherman Avenue. (SIDEWALK USES – STOREFRONT AREA)
- Three (3) existing curb cuts will be removed – one along 6<sup>th</sup> Street and two along Sherman Avenue. Only one new 24" wide curb cut will be required on Sherman Avenue for the project. No curb cuts will be on 6<sup>th</sup> Street, which is a pedestrian-oriented street. For the new curb cut required for the driveway into the parking structure, the sidewalk pattern and material will carry across the driveway. (WIDTH AND SPACING OF CURB CUTS)
- The trash area will be located behind the building off of the alley on the southeast corner of the property and will be screened from view on all sides. The enclosure will be constructed with brick to match the building and will have an opaque decorative architectural gate. (SCREENING OF TRASH)
- Loading and service areas will be located within the parking structure. (SCREENING OF SERVICE AREAS)
- Exterior lighting on the building will be recessed in the roof canopies at the ground floor level to provide pedestrian lighting. Guestroom balcony roofs will have lighting and the upper roof deck will have lighting to highlight the building corner. Fully shielded wall scones will be provided on either side of the main entry doors. (LIGHTING INTENSITY – BUILDING LIGHTING)
- There is one existing single-arm tall streetlight at the corner of Sherman and 6<sup>th</sup> Street that will remain. There are two existing post streetlights along Sherman Avenue. One light will remain in its current location, and the other will be shifted to allow for the new curb cut into the parking structure. There are no existing streetlights along 6<sup>th</sup> Street. (LIGHTING INTENSITY – STREET LIGHTING)
- The DC zoning district has a 0' front and side yard setback, unless providing usable public space, forecourts or vegetative screening of parking structures. Buildings may be set back from the sidewalk a maximum of 20' for public space or entries, or a maximum of 10' for vegetative screening. Setting façades close to the street may be accomplished through base structures that extend out to the sidewalk, not necessarily the full height of the building. The building meets this requirement. The street level façade along the Pedestrian-Oriented 6<sup>th</sup> Street is set up to the back of the sidewalk along the property line. A portion of the project on the corner of Sherman and 6<sup>th</sup> Street has a dining patio for the use of hotel guests but it has a base structure that extends out to the sidewalk. (MAXIMUM SETBACK)
- The proposed building is oriented to Sherman Avenue. The building façade along Sherman incorporates numerous windows as well as an entrance canopy and signage. The façade along 6<sup>th</sup> Street incorporates windows. The primary building entrance faces Sherman and is centered in the



building façade. An outdoor patio at the ground level activates the street corner at Sherman Avenue and 6<sup>th</sup> Street. (ORIENTATION TO THE STREET)

- The DC design guidelines require the principal entry to have two elements. The main building entrance is centered on the façade along Sherman Avenue and welcomes pedestrians with an overhanging canopy as well as a recess in the main building wall. Those are both allowed design elements. Some form of weather protection shall also be provided. Both the canopy and the recess provide added weather protection for pedestrians. These features, along with clear signage, help identify this visually prominent entrance. (ENTRANCES)
- The proposed structure incorporates a top, middle and base, as required by the DC zoning district (MASSING)
  - The top section of the building is distinguished by overhanging roofs, an open roof deck with trellis, and additional windows. The main material is a dark metal panel, with accent metal panels. (TOP)
  - The middle section of the building has a regular pattern of guestroom windows surrounded by dark and light color brick veneer. Also, there are some dark and accent metal panels to connect the base to the top. (MIDDLE)
  - The base of the building features a large amount of storefront glazing and canopies to define the ground level. The finish is a combination of light grey brick, darker composite panels accented with horizontal wood siding with a decorative concrete plinth. (BASE)
  - The base of the building aligns with the property lines of the lot, but steps back above the ground floor level to allow for the required 10-foot setback over 45 feet above grade. The only parts of the building that extend past these setbacks are roof overhangs and balconies. The only part of the building that is taller than 75 feet is the elevator penthouse, which is much smaller than the 8000 SF Tower Floor Size restriction at 176 SF and is over the minimum Tower Separation of 50 feet noted in the Site Performance Standards. At approximately 77 feet tall, the overall building height is well below the maximum 200 ft building height. (BUILDING BULK)
  - Sherman Avenue has a mix of low-and mid-rise buildings, which align well with the scale of the plinth of the proposed hotel. The overall mass of the building helps transition from these shorter structures to the high-rise residential buildings on Front Avenue. (CITY BLOCK ELEVATIONS) The ground floor of the building has the most amount of character to provide visual interest to pedestrians, including: Pedestrian-scaled signs to identify the building entry; seasonal planting in multiple planters against the building along Sherman Avenue; metal canopies above the ground floor storefront windows; accent wall sconces on either side of the main entrance; and a decorative concrete plinth to ground the building. The ground level also features an elevated patio at the corner of Sherman and 6<sup>th</sup> Street to add a further level of detail in this area. (GROUND LEVEL DETAILS)
- The proposed structure would meet the minimum glazing requirement for Ground Floor Windows by providing 40% window and glazed door area in the “window zone” of the façade along Sherman Avenue and 26% “window area” in the window zone along the 6<sup>th</sup> Street façade (GROUND FLOOR WINDOWS)
- The DC design guidelines require a visual connection between activities inside and outside the building. Ground level façades oriented to pedestrian-oriented streets require a minimum of 60% transparency and vehicular-oriented streets require a minimum of 40% transparency. The proposed structure would meet the transparency requirement for ground floor windows with a minimum of 60% transparency. (GROUND FLOOR WINDOWS)

## DECISION POINT



The DRC should grant the application in Item DR-1-24, a request by Michael Nilson, The Richardson Design Partnership, on behalf of CDA Hotel LLC, a six (6) story hotel with below grade parking along Sherman Avenue, located at 602 & 612 E Sherman Avenue, Coeur d'Alene, Idaho, be approved with or without conditions, or determine that the project would benefit from an additional DRC Meeting to review project changes in response to the first DRC Meeting or if it is deemed necessary based on all the circumstances.

Ms. Stroud concluded her presentation.

### **Commission Comments:**

Chairman Messina asked Ms. Stroud regarding the guidelines, there is a lighting intensity standard. He asked if this applies to lit signage as well as lighting for the entrance to the hotel and entrance to the parking.

Ms. Stroud replied the signage will be at the end of the facts and findings and has some comments from staff detailing the signage. On page 16 of the staff report regarding lighting intensity this will give you more information. Staff did speak with Kelley Setters, Deputy City Clerk, she stated in her report the illumination must not create an unsafe or hazardous distraction to others. The brightness or intensity of lighting for a sign including an electronic message display shall not exceed 5,000 nits from dark to dusk or 500 nits from dusk to dawn. The applicant will provide more information to verify the signage that will be installed.

Ms. Patterson, Community Planning Director replied there are no illuminating signs, digital or reader boards, or flashing signs. These are all prohibited in the DC district.

Ms. Patterson stated she wanted to give more guidance on the lighting guidelines for the principal entrance. The lighting is more for the pedestrians. There is also weather protection at the entrance of the building. Ms. Patterson also clarified that the pedestrian-oriented street designation on Sherman Avenue stops at Sixth Street. Sixth Street is the pedestrian-oriented Street and Sherman is a vehicle-oriented street in the vicinity of the project.

Commissioner Lemmon asked if the pedestrians are only allowed access into the building on Sherman Avenue.

Ms. Patterson replied that is correct. The pedestrians will have access only at the front entrance on Sherman Avenue along with the vehicles using the parking structure. The vehicles will also exit the parking structure off of the alley in the back of the building.

Commissioner Lemmon asked if the applicant wanted the pedestrians to have access off of Sixth Street could they do a design departure.

Ms. Patterson replied yes, but they have not asked for one.

Commissioner Priest stated there is an exit into the alley, and asked is that only for vehicles exiting into the alley out of the parking structure.

Ms. Patterson replied that vehicles could exit out onto the alley or onto Sherman Avenue. She further clarified that Sherman Avenue is vehicle-oriented that is why they have the vehicle access on Sherman Avenue and not on Sixth Street, which is pedestrian-oriented.

Chairman Messina opened the public hearing and swore in the applicant and the public as a group.

### **Applicant Testimony:**

The applicant provided the following statements:

Parker Lange who is with the development team, introduced himself and wanted to let the Commission know he will answer any questions the Commission will have throughout the presentation.

Michael Nilson, the architect from The Richardson Design Partnership from Salt Lake City introduced himself. The project sits on 0.482 acres. The primary occupancy is the hotel, a parking garage that has three (3) stories underground. On the sixth floor of the hotel, there will be an open bar for the public and guests. The first level will also have a bar breakfast area, that is part of the hotel amenities. There will be 231 rooms and 130 parking stalls. The design guidelines require the location of parking to be located within the foot print of the building, where it is not on the outside of the building taking up street parking. There will be 14 bike racks and accessible van stalls, 28 compact parking spaces, 97 standard stalls and 5 accessible stalls on all levels of the parking structure.

The amenity zone in the DC guidelines will have four existing street trees on Sherman Avenue and one on Sixth Street. Those will be removed during the construction, but they will be replaced. The sidewalk will have a clearance of 8'6" and the minimum is 7'. They will take the extra 18" to create a store front area where they will be placing above-ground container planting to enhance the walking experience along the sidewalk. The curb cuts that are existing will be removed and a street light will also be moved on Sherman Avenue. The trash pickup is located on the alley side and will be screened with appropriate materials; this will be locked down. The lighting for the building will have recessed lighting that will be tucked into the canopies or right against the alcove. There is no uplighting. The sconces are all covered and lit on the bottom. There are three existing street lights surrounding the property. The setbacks along Sixth Street will be a close to the property line. There is a small portion of the building that is recessed. This will be used for an outdoor patio used by hotel guests during the day. The main pedestrian entrance will be in the middle of this project facing Sherman Avenue and the vehicle entrance is on the east side of the building off of Sherman Avenue. The entrances will have canopy coverings to provide weather protection. There will be some marquee signs on either side of the door. Massing is required on three distinct levels of the building. A strong base - this will be a darker brick, lots of store front windows, metal and a top, this will be a metal but looks like wood for the durability. The sixth story will be an open patio for guests and the public. The impact is minimal with a 10' setback on the property line. The height will be 75', but they could go up to 200 feet. The ground level details include pedestrian scale signs on the building. They will have seasonal plantings along the building. The canopies are required to be 5' projecting from the building and to be a maximum of 12', this is where the design departure comes in. The canopy's start at 9'11" above the sidewalk which will give adequate clearance for any hanging signs for pedestrian and vehicular traffic. The sidewalk does slope on a 2% grade on the Sherman Avenue side and a 3% grade on the Sixth Street side. In order to keep the integrity of the canopy with the linear design of the building, they breach the 12' mark throughout the building and this breach happens across Sixth Street. This provides the rain and snow protection. It covers 94% of the façade along Sherman Avenue and 81% along Sixth Street. This fits with the aesthetic design of the building. He would like to keep the aesthetic as is.

This design departure satisfies the Comprehensive Plan for hospitality uses, to bolster Coeur d'Alene as a tourist attraction and maintain the community friendly welcoming atmosphere with a small town feel. Coeur d'Alene recognizes and celebrates its historical and cultural roots, Coeur d'Alene recognizes the past and present of the Coeur d'Alene Tribe and its connection with the natural environment with the Coeur d'Alene identity as a cultural center of North Idaho and creates social connections with wide variety of activities events and public spaces for community members to gather year around. This project will facilitate such gathering as a hospitality destination. This will bring tourism in the community and feed the businesses on Sherman Avenue and Coeur d'Alene's downtown and will provide the nightlife activity with the roof top bar that will have the view of the city and the lake. The parking structure will provide adequate parking for this project while keeping the walkable feel of the streets. There are no blank walls that are over 30'. The parking is fully screened except for the access. The roof edge will have a cornice, and it will have a 3' canopy overhang.

All of the mechanical elements on the roof top will be screened. This project is at the edge of Coeur d'Alene's historical district and near more contemporary construction. The building will have the more dominant material of brick on its base. This building acts as a stepping stone between the small buildings along Sherman Avenue and the high-rise residential Parkside building. The signs will look like wood and is part of the Marriott

brand. It will only be lit up for the words – the signage won't have additional lighting. There needs to be a 40'x40' view triangle of the corner of Sherman Avenue and Sixth Street, which is provided.

The applicant showed the materials that will be used on the project to the Commissioners by passing around samples and showing renderings of the building with the materials.

Chairman Messina asked about the outdoor decking material and if it would be some sort of concrete.

Mr. Nilson replied that yes, it will be a concrete deck. The deck on the outdoor terrace will be a recessed pedestal system. When the water accumulates it will flow down into a drain system.

Chairman Messina asked about the roofing material on the roof top and asked what material and what color it would be.

Mr. Nilson replied it will have a membrane; the color is typically white because it helps decrease the heat affect. The darker the color more heat radiates up. There are multiple colors to choose from.

Mr. Nilson states they do comply with the FAR. They are allowed to do bonuses but they did not request any. The area that they can build they are under the allowable FAR numbers. They are allowed 83,972 square feet and they are providing 76,007 square feet. They do not need to make the building any bigger than it needs to be.

Mr. Nilson concluded with this presentation.

#### **Commission Discussion:**

Chairman Messina would like to know about the outdoor decking on the top floor in regards to the tower behind it. Would it be over by the Sweet Lou's restaurant so this is not right up against the towers itself.

Mr. Nilson states that the deck will take up the whole Sixth Street frontage. The rooftop patio is 10 feet plus. It is an additional foot off of the property.

Chairman Messina asked for further clarification on its proximity to living spaces in Parkside. He asked the applicant to clarify if the outdoor decking is not directly against living spaces, and closer to mechanical equipment and the deck that is up above Sweet Lou's restaurant on the Parkside building. The applicant verified that the outdoor space will be parallel to the Sweet Lou's Restaurant and not someone's apartment in the tower. Chairman Messina asked if they could work with the Arts Commission to put something on the blank wall on the pedestrian side of Sixth Street.

Mr. Nilson replied yes, he will work on putting something on the wall.

Commissioner Lemmon asked about how much glazing was done on the wall along Sixth Street.

Mr. Nilson replied there was 26% glazing, and noted that the grade does drop toward the alley making it difficult to put more glazing.

Commissioner Lemmon asked about the transparency of the windows.

Ms. Patterson replied the percentage of glass is one requirement and the transparency is the other. All of the windows on the ground floor are 60% transparency on both street frontages.

Mr. Nilson stated this is not obscured glazing. People can see in and out of the windows. They do want the interaction between the hotel lobby and the street.

Commissioner Pereira asked if all the parking will still remain on the curbs.

Ms. Patterson replied yes. There is no on-street parking going away.

Mr. Nilson stated there will be a security gate parking that has tickets. He states that by code the height to park in the parking garage will be 8'4" clearance to accommodate a tall truck.

Chairman Messina asked the applicant if he can touch on construction staging.

Mr. Lange replied they are working with city staff and their contractor now. They are expecting about two years of construction at this site. They are hoping to start sometime this year on the project.

Commissioner Pereira asked why they did not go to the 220'.

Mr. Lange replied that they wanted to have a building that would fit the needs of how many guests they could accommodate and the economic conditions.

Commissioner Priest asked about the underground parking and that they would be digging about 15' away from another underground parking structure. This is not New York with rock solid foundation. Has city staff looked at the potential negative impact on structural reliability for both buildings.

Ms. Patterson replied yes. This is handled through of the city departments and staff are working with the applicant and engineering. The Thomas George building that is under construction right now is a bigger engineering feat because of the parking in the McEuen parking structure.

Commissioner Lemmon stated he has concerns with the traffic on Sherman Avenue.

Mr. Lange replied that the guest traffic is signed as such. They will all enter on Sherman Avenue because we are required to have vehicle traffic only on Sherman Avenue because of the city ordinance. The guests would exit out onto the alley or if they are dropped off at the front of the building.

Chairman Lemmon asked can you make a left turn into the parking structure if you are heading west on Sherman Ave.

Ms. Stroud interjected that on page 46 of the staff report under the recommendation of the conditions of approval, there was a project review where Chris Bosley the City Engineer did look at this project and the applicant did not have the designs completed at that time. They needed feedback from City staff and looking at the design and then receiving recommendations from city staff. The applicant then provided his site plan, and proposed access. Mr. Bosley went over everything and added the comments and conditions, sidewalks along Sherman Avenue and Sixth Street must be into ADA compliance, any existing driveway approaches not being used with the proposed development shall be removed. The applicant shall complete a traffic study including a pedestrian safety study. Mr. Bosely will provide the study and then his recommendations will follow.

Chairman Messina asked what if there are any changes today. What happens then.

Ms. Stroud replied if there is a significant change, the Design Review Committee would have another Commission meeting.

Commissioner Pereira stated that the rooftop bar will be packed with people.

Commissioner Ingalls stated that there are 22 design elements. There are 19 total on this design because three are not applicable. The Commission is very limited in their role. They are only here to address the design.

Chairman Messina stated when the public comments, they need to focus on the design and not that housing or something else that needs to go on this property. The Commission has a decision based on the Findings.

Ms. Stroud presented the pages of Findings of Fact to the Commission.

Commissioner Lemmon asked if the Commission could be moved to a second meeting if the Commission wants to see a traffic study.

Ms. Patterson replied no.

Chairman Messina also stated no, that has nothing to do with the Design Commission.

Commissioner Ingalls commented that the public needs to make comments on the design elements. The Commission is judging their decision on the facts that are presented.

**Public Testimony open:**

Robert Mason, stated that he had read the public comment from Joan Woodard and made some good points and wants the Commission to re-read it. The Design standards that were adopted in 2006 encourage the appeal to existing and new residents and preserve the views of Tubbs Hills.

Aileen Koler, states that so many people here between the Lofts and Parkside would ditto what Mr. Mason said and Joan Woodard's letter. One of the statements that keep being said is we are trying to preserve a small-town feel. This building is beautiful, but is not preserving anything.

Brad Jordan, stated he has lived in Coeur d'Alene his whole life. This is not a small town anymore, that would be Kellogg or Saint Maries. The City of CDA is pushing over 60,000 people. Kootenai County is pushing 200,000.00 population. He stated this is a great project. He was part of the downtown revitalization in the mid-80's. When his business was downtown, more than half of the storefronts were vacant the sidewalks were narrow. There were no street trees and there were pot holes. The downtown has come a long way and it is getting better and better every day. This project is a good project. It's in a transitional area at the end of the downtown, it could use more street traffic. It's not doing any good as an empty lot. We need people in the downtown, that is what makes business run. He has worked on the high-rise ordinance and the Commission needs to make sure there is a high level of design and quality. The design of this building is great. It encourages street activity and they have used high quality materials. He encourages the Commission to approve this. It is a fantastic project.

Derek Hutchison is opposed and wants to know where the public can go and speak about not wanting this building in downtown Coeur d'Alene. He would also like to know where the employees are going to park.

Chairman Messina replied the public can always speak at the City Council meetings during public comment.

Rebecca Olivieri is opposed. She stated this does not fit with the small-town elements. She appreciates what the downtown has been through and what happened in the 1980's. The decisions that are made today will impact the small downtown and change the character forever. You can't undo putting in corporate hotels which are the antithesis of a small-town feel. The essence of Coeur d'Alene is the downtown area. She feels that this building will work better further east on Sherman. The location that this project will be built on will change the downtown feel. She cited fact A-19 that the Marriott had considered the Comprehensive Plan. She feels this does not meet that plan.

Cindy Donato stated she just moved here from St. Louis in August at 609 Sherman (the Lofts) in August of 2023. The property value will change, and this not keeping within the small-town feel. Her view of the Lake will be completely obscured. She asked if this is not stopped here, where else can the residents go. The architects and the project people have not involved the community at all.



Chairman Messina would like Ms. Patterson, City Planning Director to explain the process of how the process works.

Ms. Patterson replied that this is the first meeting with the Design Review Commission and depending on how the Commission votes - if it meets the design guidelines and if the design is approved with or with conditions. If the Commission requires another meeting, the commission has to be very specific about what needs to be addressed of the items including the Design Departure. The Commission will direct the applicant to come back for a second meeting. If the Design Commission were to deny the application, then the applicant could appeal to City Council. It could go to a third meeting if the Commission chooses to do so. If this is approved today and the neighborhood wants to appeal the decision, they can appeal within 15 days of the Commission's decision. This would go to City Council and would be publicly noticed. The appellant would be required to show how it does not meet the Design Guidelines. It cannot be because they do like the building or that it would be blocking views, too tall, too big or that it's a hotel use. This is the public process. There is no city requirement to meet with the neighbors to say, hey do you like my design or not. This process follows the State and the City process for receiving the public's input in a public hearing.

Chairman Messina asked if this project would go to City Council.

Ms. Patterson replied only if the project gets an appeal from the public and they will have 15 days to appeal the decision from today.

Ms. Donato stated that this is a disservice to the Community.

Emily Johnston stated she works for the Ashley Financial Group in the Parkside building. She is 26 years old and moved here from TN. She came to Coeur d'Alene on vacation, fell in love with the area and moved here two weeks later. The architecture and the history, in the town is huge. Growth is important but feels that the building does not have a small-town feel. It looks like every other town in the U.S.

Eva Carlton stated the project only has the location of parking. They are not providing enough parking. Where are the locals and the workers going to be parking. She has a vrbo a couple blocks from this project. Most of the people are renting cars from the airport. They are not taking ubers. They don't use the cars. They park them and they leave them. Where will the maybe 50 workers going to be parking, maybe McEuen Park and maybe on the street in front of her house. She thinks the parking structure should have another floor to put in more parking. The building is great, but it has inadequate parking.

Cheryl McHale stated she didn't realize the public was not going to be allowed to speak other than the Design items. She opposed the project. It looks beautiful but it does not speak to the residents., She feels there is not enough parking in Downtown Coeur d'Alene. This will obliterate any parking that is available now. This does not feel like a small-town project, but more like a cookie cutter project, it is not unique. This will not add anything to the community.

Ron Hartman stated he is a concerned property owner who pays taxes. He feels that new growth does not pay for itself, because all of our property taxes. His concerns are there has been no study done on the additional hours required by the police department and who will pay for those hours, and more people in city parks. This requires more maintenance and upkeep. This will fall on the homeowners and make the property taxes increase and not on the applicant. The community should not be caring the burden and there should be studies on how much additional police hours are needed, park maintenance and usage before the project is approved. The applicant needs to understand his total operating costs to have a place in the city.

Amber Hellar stated she is sorry for the interruption while Ms. Stroud was doing her presentation. She is new to Coeur d'Alene, and moved here for the small-town kind of feel. She is from Boulder City, NV. Her concern is that a lot of people rent out part of their homes as Airbnb. This hotel will affect those people who have this as their income. The older folks who are on fixed incomes rent out their basements. She would rather support them than putting another corporate hotel in. This is a Marriott. This does not sit well. It's beautiful and they

are way below the height requirements, which is appreciated, but this is not a good fit for downtown Coeur d'Alene.

Barb Letchet stated building does not fit in the downtown. Building belongs in the east end of Sherman. This as a more modern flair. Coeur d'Alene downtown does not need a bigger place. She grew up in Wallace. This building does not have an older feel. She is into quaint buildings. This kind of structure she does not like. She wasn't here when the tall buildings came in. Also, this will take away business from the downtown bars. She is a tourist and likes the history but this building is going too modern.

Ben Prohaska, states he is from Idaho Trust Bank, the adjoining neighbor on the corner for the past 12 years. He has been in business in Coeur d'Alene for over 30 years. He questioned how the east side of the building has been addressed and that there is a blank wall adjoining his building and there is a lack of details on the blank space. This is primarily on the design guidelines. He proposes that the commission table this matter until this issue is addressed. He is in opposition.

Camille Hutchison commented on the last three items of the criteria. Of the 25 guidelines you can plo p this hotel anywhere and it looks any other hotel anywhere. But you look across the street and you see a historic building. She doesn't understand how this building fits the historic part of the downtown feel. She suggested they push this project further east; it fits better. Her other question is parking. She has two kids and this will put a huge strain on the parking. She has a business in the downtown and this will put a huge strain on the parking in the downtown along with her personal home that is close by. She agreed with the comments her mother made that spoke up earlier. It is people that are slamming their car doors all night long coming home from the bars. Why not push this project further east. She would love this project pushed further on down Sherman. She would not have to fight to get to her mail with a hotel traffic across the street.

Shelly Moore addressed the location of parking and asked for assurance that a study will be done about the impacts of the hotel on the neighboring building so that the people in Parkside are not going to have any damage to the building.

Mr. Lange answered yes, there have been studies done.

Ms. Moore spoke up and asked if everyone heard that and remember that Mr. Lange said "yes."

Chairman Messina stated that the applicants can address that issue and that City staff addresses those issues, and work together so that nothing falls down.

Ms. Moore wants to make sure everyone has heard that from Chairman Messina and wants to address the staging of the construction, what exactly is the answer. You just said you are working on it. We want to know, where are you going to be staging, and how is that not going to affect our lives.

Chairman Messina stated he felt the staging was not going to be in the alley or interfere with the alley, but he is not sure. Maybe along Sixth Street, but the City Engineer will sit down with the applicant and make sure that the city traffic is not impacted and that you can get out of your building, etc.

Ms. Moore also asked what are the recourses after this meeting, can they write something to the City Council.

Chairman Messina stated there is an appeal process to the City Council if you or anyone wants to appeal whatever this decision will be today. You have to prove to the City Council why the decision made here today should be reversed. You have to only talk about the design review items and have to prove the facts that those items are wrong and why your appeal should be approved by City Council.

Heidi Romero stated that the decision has already been made that we are getting a hotel unless that we can prove that the design is wrong, is that correct.



Chairman Messina replied the commission's mission is based on the design. The applicant has the right to build on their property per the zoning that the city has put in place many years ago.

Scott Carlton commented that there is a rundown facility on the other end of Sherman right off the freeway. It's as big of space as this one. You can build just as easy down there, you would not interrupt anything, you would have access to the freeway, and it would help build up that east end of Sherman. You could shuttle service to downtown and all these problems would go away. That would be the perfect location.

**Public Testimony closed**

**Commission Discussion:**

Commissioner Ingalls stated the comments today did not address the guidelines and the decision that the commission needs to make. With respect to the blank east wall, there is articulation and different materials. It has been broken up nicely. If you look, at the design standard and really dig down, the blank wall standard only applies to the abutting streets, which are Sixth Street and Sherman Avenue. He indicated that the chairman pointed at one part of the wall that is blank but it's below the definition of what a blank wall is. If you want throw some art on it, he would support that, but he feels this standard has been met. There is no need for another meeting. He knows people are not going to be pleased about a hotel at this location, whether it's two stories or less. The items that he sees are adequately addressed.

Commissioner Lemmon questioned the blank wall is it only the streetscapes.

Ms. Patterson replied if you look at the standards it specifically reads the abutting streets, which is immediately next to the building.

Commissioner Lemmon stated that circulation and parking is a mess, but we can't do anything about it. It would be helpful to go off of Sixth Street as a design departure to allow for a parking entrance. With the alley being 15 feet that entrance is not going to get used and turning is going to be a nightmare. Mr. Bosley needs to address this. If we have to use Sixth Street as a pedestrian-oriented street and Sherman Avenue as a vehicular-oriented street there is a concrete wall that is 6' tall where the windows are up but they are not on ground level. There needs to be benches, planting areas, or setbacks to make it more pedestrian oriented. He does not think the wall has been addressed as a pedestrian wall and he would like to see this wall change if it's not going to be accessed for the parking, which he thinks it should be and not on Sherman Avenue. But he does not get to make that choice even though its part of the entrances. It is very dark with no lighting and maybe if you wrap the patio area and set the windows back along Sixth Street. He asked if there can be some planting materials used. The awning would be okay but it is 14' in air. It is not going to protect you from anything.

Commissioner Snodgrass stated there are no public street lights shown along Sixth Street which means it is going to be very dark. She would like to see street lights. It would be nice if there could be some benches added or actual public use features would be important. The guidelines states that trees should be 20 to 40 feet apart; right know it looks like maybe they are 50 feet apart.

Mr. Nilson, the applicant, would like to address the question regarding the historical feature question. Coeur d'Alene has two elements of historical features. One that is west of this project this is turn of the century which is the late turn of the century. This block is the transition because you have some very modern building starting on Sixth Street with the two residential tower buildings. Our approach was to use materials that are historical such as brick. We are not using stucco; we are using solid materials. On this side of the city is more of a contemporary style. We are dividing the building in three ways.

Commissioner Lemmon states there is the Masonic building and there is more of contemporary architecture that way on the street.

Commissioner Pereira stated the Marriott has more historical architecture features than the two new towers have. The two new towers have no historical features and they were approved.

Chairman Messina asked about Sixth Street and if there is any landscaping needed.

Ms. Patterson replied that the urban forester, Nick Goodwin, would make sure all requirements would be met regarding street trees.

Chairman Messina stated the canopy fits the guidelines, the lighting is under the canopy, and in his opinion, there is no second meeting needed.

DRAFT

Commissioner Priest asked about the pedestrian-oriented street guideline that calls for a lively, friendly pedestrian street. Is there any other guidance for us to look at.

Ms. Patterson replied there are factors that make it fit in the guidance such as store front windows, lighting, signage, landscaping, and no vehicle access.

Commissioner Priest wants to clarify that a lot of work went into to make Sherman Avenue pedestrian friendly which is a vehicle orientated street. He asked how has this been defined in the past from the commission. As a new member, he's curious in terms of whether or not it's required additional planning and asked are there additional criteria as far as anything historically regarding discussions about creating a lively pedestrian friendly environment as far as that street in concerned.

Ms. Patterson replied the most recent example would be the Thomas George approval than came through this Commission and they had to do a design departure because they had there parking garage access on a pedestrian-oriented street. There were concerns about vehicles with that project as well. There was additional signage required. They had to have some details on the sidewalk to make it very clear for the vehicles leaving and entering to look for pedestrians coming through, as well as additional planting and a canopy with flashing lights. There was an approval with conditions to meet the guidelines on the approval criteria.

Chairman Messina clarified with Ms. Patterson if the commission put conditions on the findings how that works with city staff moving forward with applicant.

Ms. Stroud and Ms. Patterson replied if it's something small the applicant would comply and move forward.

Mr. Nilson replied he understood that the pedestrian side of the building that the city wanted the building up against the property line so anything that would need to be added they would have to go away from the property line. The building is designed to go up against the sidewalk. Adding benches or planters we would not be conforming with the city guidelines for a clear walkway because it would be too narrow.

Ms. Patterson stated that if benches were a condition that they couldn't be placed in the public right-of-way without an agreement with the city.

Mr. Lange commented that they could work with a local artist regarding the wall to add an artistic feature on the bottom panel.

Commissioner Lemmon suggested on the pedestrian street side there is too much concrete on the wall. He thinks Sherman Avenue should be the pedestrian street but he does not get to make that decision. It's not very friendly to walk along a concrete wall.

Chairman Messina asked about moving the windows back without moving the structure back.

Mr. Lange replied that is the dining area and the exit corridor. Moving the windows would be in conflict with the fire code for that area.

Chairman Messina suggested working with the Arts Commission on the whole wall along Sixth Street and make more of a historical art rendering of the City of Coeur d'Alene and said the applicant could work with staff.

Commissioner Ingalls agreed that this could work with architectural detailing as well. The applicant could use some concrete detailing that is rough formed or a split faced sort of treatment.

### **Discussion Closed**

**Motion by Commissioner Ingalls, seconded by Commissioner Pereira, to approve Item DR-1-24 with the five proposed conditions and an additional condition to have the applicant team work with city staff to enhance the concrete band and wall along Sixth Street to enhance the pedestrian-oriented street, considering architectural features and/or artwork. Motion carried.**

ROLL CALL:

Commissioner Ingalls	Voted	Aye
Commissioner Lemmon	Voted	Aye
Chairman Messina	Voted	Aye
Commissioner Pereira	Voted	Aye
Commissioner Snodgrass	Voted	Aye
Commissioner Priest	Voted	Aye

Motion to approve carried by 6 a 0 vote.


**ADJOURNMENT:**

Motion by Commissioner Lemmon, seconded by Commissioner Snodgrass to adjourn the meeting. Motion carried.


The meeting was adjourned at 2:45 p.m.

Prepared by Traci Clark, Administrative Assistant

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# STAFF REPORT



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**DESIGN REVIEW COMMISSION  
STAFF REPORT**

**FROM:** TAMI STROUD, ASSOCIATE PLANNER  
**DATE:** JANUARY 25, 2024  
**SUBJECT:** DR-1-24: REQUEST FOR THE FIRST MEETING WITH THE DESIGN REVIEW COMMISSION FOR A PROPOSED SIX-STORY MARRIOTT HOTEL IN THE DC (DOWNTOWN CORE) DISTRICT

**LOCATION:** A 20,993 SF PARCEL LOCATED AT 602 E. SHERMAN AVENUE  
A PARCEL OF LAND BEING DESCRIBED AS CDA & KINGS ADD, LTS 1,2, 3 AND 4, BLK 35 AND MORE COMMONLY DESCRIBED AS 602 AND 612 E. SHERMAN AVENUE

**APPLICANT / OWNER:**  
CDA Hotel LLC  
1450 Twin Lakes Avenue, Suite 201  
Bozeman, MT 59718

**ARCHITECT:**  
Michael Nilson  
The Richardson Design Partnership  
510 South 600 East  
Salt Lake City, UT 84102

**APPLICANT'S REQUEST**

Michael Nilson, architect with the Richardson Design Partnership, on behalf of CDA Hotel LLC, is requesting a First Meeting with the Design Review Commission for a 6-story Marriott Hotel. The applicant participated in an Initial Meeting with Planning Staff as required by Municipal Code § 17.09.325(D). The proposed project will have approximately 131 rooms, a fitness center, rooftop bar, outdoor patio, and parking structure for guest parking which continues three stories underground. The subject property is in the Downtown Core (DC) zoning district, and must adhere to the Downtown Coeur d'Alene Design Guidelines.

**DECISION POINT:**

Should the Design Review Commission approve the design for the 6-story Marriott Hotel located at 602 AND 612 E. SHERMAN AVENUE in the Downtown Core (DC) zoning district either with or without conditions, or direct modifications to the project's design and require a second meeting?

**DESIGN REVIEW AUTHORITY:**

The Design Review Commission ("DRC") is tasked with reviewing the project to ensure compliance with all applicable design standards and guidelines. This project is located within the Downtown Core (DC) zoning district and located on a block that is designated as a vehicular-oriented street. The DRC will provide feedback to the applicant and staff on how the applicable design standards and guidelines affect and enhance the project. The DRC will provide direction to the applicant, and may suggest changes or recommendations to the proposed project. The DRC may render a decision during the First Meeting, or request an Optional Second Meeting.

All exterior projects south of the midblock of Lakeside/Coeur d'Alene, all street façade alterations, and all exterior expansions trigger review by the Design Review Commission if located in the Downtown Core (DC) zoning district. (Municipal Code § 17.09.320(A))

A development applicant shall participate in the design review process as required by this Article before substantive design decisions are fixed and difficult or expensive to alter. The City will work with the applicant in a collaborative fashion so that the goals of both the City and the applicant can be met to the greatest degree possible, and to address the concerns of neighbors and the community. In order for this process to work effectively, the applicant must be willing to consider options for the project's basic form, orientation, massing, relationships to existing sites and structures, surrounding street and sidewalks, and appearance from a distance. (Municipal Code § 17.09.325)

The applicant has the obligation to prove that the project complies with the adopted design standards and guidelines, which serve as the basis for the design review. The design review commission may not substitute the adopted standards and guidelines with other criteria of its own choosing. Nor may it merely express individual, personal opinions about the project and its merits. Nevertheless, it may apply its collective judgment to determine how well a project comports with the standards and guidelines and may impose conditions to ensure better or more effective compliance. It also must be recognized that there will be site specific conditions that need to be addressed by the commission as it deliberates. The commission is authorized to give direction to an applicant to rectify aspects of the design to bring it more into compliance. The commission is authorized to approve, approve with conditions or deny a design following the Optional Second Meeting with the applicant. (Municipal Code § 17.03.330)

The Design Review Commission may grant or deny the application, or grant the application with such conditions as are, in its judgment, necessary to ensure conformity to the adopted standards and guidelines. The Commission shall make written findings to support its decision, specifically stating how the project conforms to the adopted design standards and guidelines or how it does not. A copy of the Commission's decision shall be mailed to the applicant and the Director shall make the commission's decision available for public inspection. The Commission has the power to table a decision to a later date and request an additional meeting. (Municipal Code § 17.03.335)

**PROPERTY LOCATION MAP:**



**AERIAL PHOTO:**





**BIRDSEYE AERIAL PHOTO:**



**PROJECT OVERVIEW**

The site is located on a 20,993 SF parcel along Sherman Avenue between 6<sup>th</sup> and 7<sup>th</sup> Streets. The property is currently vacant and is a grassy lot previously used for the “Live After 5” music events several years ago. The parcels are comprised of 2-lots that will be consolidated for building permit purposes. The property abuts Idaho Trust Bank directly to the east. Parkside Condominiums are located to the south, across the alley from the proposed hotel. The applicant is proposing a six-story (6) hotel structure with 131 guestrooms. A ground floor dining area with an outdoor patio, bar and fitness center will be available for hotel guests. A rooftop bar and lounge will be open to hotel guests and the public. Parking for hotel guests will be provided in the underground parking structure, which continues three stories underground with a total of 130 parking spaces, 8 of which are on the ground floor. The rooftop bar and lounge, open to the public, is exempt from parking because it is less than 3,000 S.F. The total height of the building is 77'-0” feet tall which includes the elevator penthouse, and is below the maximum height allowed in the Downtown Core (DC) which is 200’ tall. The proposed project is located in the DC (Downtown Core) zoning district, and must adhere to the (DC) Downtown Core Design Guidelines and Standards.

- **PROPOSED BUILDING AREA:** (excluding floors dedicated to parking, elevators, staircases, mechanical spaces and basement)

**SITE AREA:** 22,993 S.F. 0.482 ACRES

**FLOOR AREA RATIO (FAR):** 22,993 SF X 4 (FAR)

**FAR ALLOWED:** 83,972 S.F.

**FAR PROVIDED:** 76,007 S.F.

(Building Total Area, includes space applied to parking, mechanical spaces, elevator & stair shafts, common area and street level retail.)

### **DESIGN REVIEW PROCESS:**

A Project Review meeting with staff was held on **August 1, 2023**. During that meeting, staff discussed the proposed project with the property owner and applicant's representative and provided concerns and code requirements that needed to be addressed. The below massing model was submitted as part of the Project Review application submittal. Staff did an analysis of the proposed hotel based on the code requirements noted in the Basic Development Standards and Downtown Design Guidelines. The proposed hotel meets the Basic Development Standards noted in the Downtown Core (DC) zoning district. Staff provided feedback to the applicant's architect addressing each Downtown Core Design Guideline and providing details to the design team on how they can meet the guidelines where deficiencies were noted in the Project Review meeting staff report provided by Planning staff.

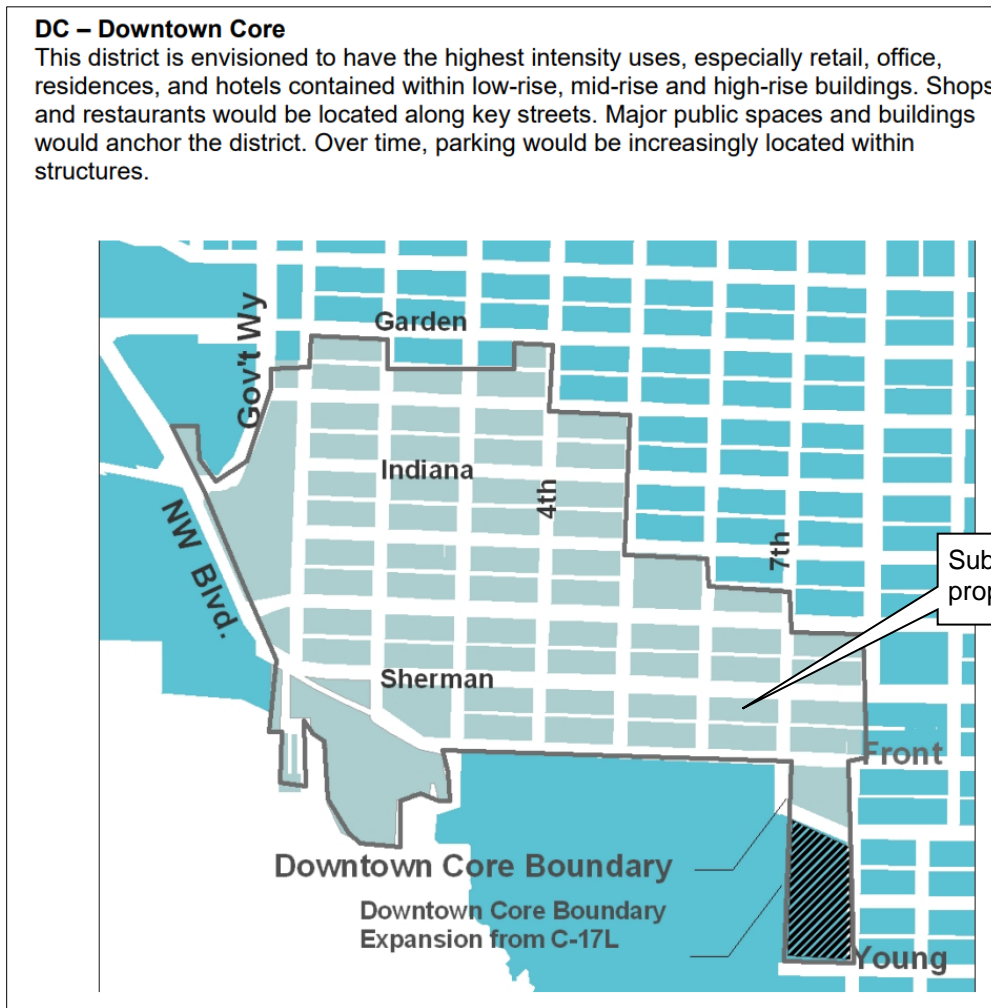
### **“Marriott AC Hotel” Project Review Meeting held on August 1, 2023 (Massing Model Perspective.)**



- The project architect addressed the items noted in the Project Review meeting staff report and provided the required updates for staff for review for code compliance in the Downtown Core (DC) zoning district.

On **October 16, 2023** the applicant provided updated information in order to schedule the required Initial Meeting with staff. The Initial meeting with staff was held on **October 30, 2023**. During the meeting, staff reviewed the DC Downtown Coeur d'Alene Guidelines and Development Regulations and discussed the following items with the applicant team:

- A. Guidelines that apply to the proposed development,
- B. Any FAR Bonuses to be requested, and
- C. Requested Design Departures.





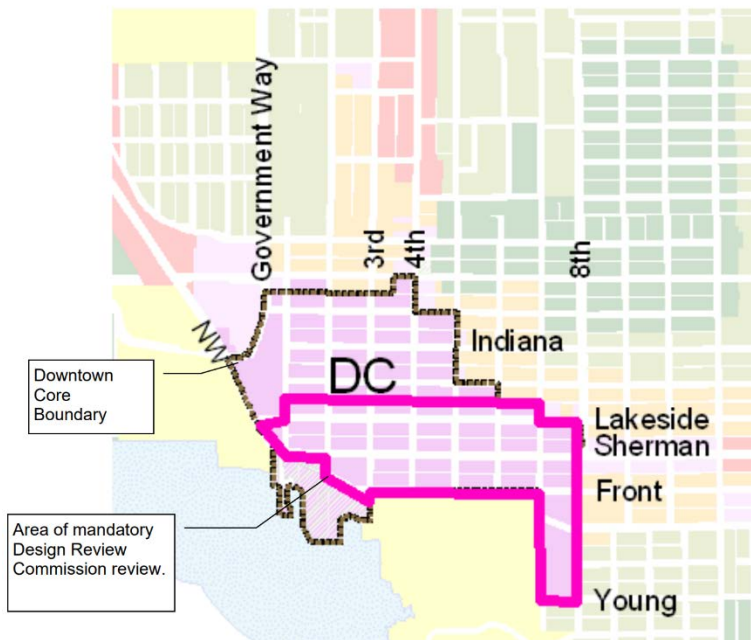
## DESIGN GUIDELINES

### DOWNTOWN COEUR D'ALENE

#### DOWNTOWN DESIGN GUIDELINES

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District Boundaries





**Aerial Photo (showing existing conditions):**





**SITE PHOTO – 1:** View along Sherman Avenue street frontage looking southwest at the subject property.



**SITE PHOTO – 2:** View from Sherman Avenue along the street frontage looking south at a portion of the subject property and the abutting property to the west (Idaho Independent Bank).





**SITE PHOTO – 3:** View along the Sherman Avenue street frontage, west of the subject property, looking south at Parkside Tower and the abutting bank's parking lot with McEuen Terrace and Parkside Condos in the background.



**SITE PHOTO – 4:** View from the eastern side of a portion of the subject property looking north at the neighboring condo building and office.





**SITE PHOTO – 5:** View from the south side of Sherman Avenue in front of the subject property looking west along Sherman Avenue.



**SITE PHOTO – 6:** View along the northwest side of the subject property looking east toward t McEuen Terrace.



## DESIGN REVIEW ANALYSIS

The following pages provide an overview of the required design guidelines and the project components.

### Applicable Downtown Core Design Guidelines:

The following design standards and guidelines are applicable to the proposed project, unless otherwise noted. The DRC shall review the proposed design to ensure compliance with these criteria.

- Location of Parking
- ~~Screening of Parking Lots (N/A)~~
- ~~Parking Lot Landscaping (N/A)~~
- Sidewalk Uses
- Width And Spacing of Curb Cuts
- Screening of Trash/Service Areas
- Lighting Intensity
- ~~Gateways (N/A)~~
- Maximum Setback
- Orientation to the Street
- Entrances
- Massing
- Ground Level Details
- Ground Floor Windows
- Weather Protection
- Treatment of Blank Walls
- Screening of Parking Structures
- Roof Edge
- Screening Of Rooftop Mechanical Equipment
- Unique Historic Features
- Integration of Signs with Architecture
- Creativity/Individuality Of Signs

The applicant has provided a detailed analysis of how they believe the project complies with all required design guidelines on pages 31-35. The Applicant's Narrative is also attached.

## **Design Departures:**

The applicant has requested a design departure for the Weather Protection Design Guideline as it relates to the maximum canopy height. The proposed canopy meets the minimum depth to provide weather protection per the DC design guidelines. The DC design guidelines require a minimum depth of a canopy or awning to be 5'. The 5' deep canopies associated with the building meet the minimum requirement to provide pedestrians from weather. The exception is at the main entry, which has a shorter canopy at 3.5 feet in depth. However, a recessed entry provides additional protection. The applicant has requested a design departure for Weather Protection related to the vertical dimension between the underside of the canopy or awning and the sidewalk. Per the DC design guidelines, the vertical dimension between the underside of the canopy or awning and the sidewalk shall be at least 8' and no more than 12'. The proposed design has a canopy height starting at 9'11" above the sidewalk and has a clearance for pedestrian and vehicular safety signage suspended from the canopy above the parking garage entrance at a height of 9'11". As the sidewalk slopes down at an average of 2.8% to the west, the canopy's vertical height increases to 14'11" at the northwest corner of the project, which is 2'11" above the maximum allowable height. Along 6th Street at the lowest grade, the canopy would have a vertical dimension of 17'10". The requested design departure is to exceed a portion of the canopy to extend above the 12' maximum design guideline. The architect outlines the justification as the departure of the canopy height would still meet the weather protection requirement for pedestrians, the canopy would maintain a consistent horizontal aesthetic that would allow for the storefront windows to remain a consistent size and allow for maximum interior daylight. Stepping down of the canopy to meet the guideline would adversely affect the aesthetic quality of the architecture. Maintaining a consistent horizontal plane with the canopy also defines the base of the building, which is an important aspect of the design guidelines. The canopy will have a metal frame finish, with a wood plank soffit. These canopies will also have recessed downlights to provide lighting under the opaque covering. The applicant maintains the design of the proposed canopy with the increased vertical dimension and overall aesthetic is a significant improvement over what could have otherwise been built under minimum standards and guidelines. The design departure request includes an exhibit showing how the canopy would look if it were to meet the guideline. (WEATHER PROTECTION) – DESIGN DEPARTURE REQUESTED

## **DESIGN DEPARTURE CRITERIA:**

An applicant may request a design departure from any of the design guidelines adopted pursuant to this section. The planning director will review all requests for design departures on projects not subject to design review commission review under section [17.09.315](#) of this title. In order for the planning director to approve a design departure, he or she must find that:

1. The requested departure does/does not meet the intent statements relating to applicable development standards and design guidelines.

2. The departure will/will not have a detrimental effect on nearby properties or the city as a whole.
3. The project's building(s) exhibits a high degree of craftsmanship, building detail, architectural design, or quality of materials that are not typically found in standard construction. In order to meet this standard, an applicant must demonstrate to the planning director that the project's design offers a significant improvement over what otherwise could have been built under minimum standards and guidelines.
4. The proposed departure is/is not part of an overall, thoughtful and comprehensive approach to the design of the project as a whole.
5. The project must be consistent with the comprehensive plan and any applicable plan. (Ord. 3328 §8, 2008; Ord. 3192 §10, 2004)

These same review criteria are used by the Design Review Commission in considering a design departure request.

***Applicant's Design Departure Request:***

January 22, 2024  
Tami Stroud  
Associate Planner  
City of Coeur d'Alene

Tami,  
On behalf of the developers of the AC Hotel Marriott at 602 & 612 E Sherman Avenue, I would like to apply for a design departure from the Downtown Coeur d'Alene Design Guidelines in regard to the Weather Protection section.

The Weather Protection guidelines states:

*"The Vertical dimension between the underside of a canopy or awning and the sidewalk shall be at least 8 feet and no more than 12 feet."*

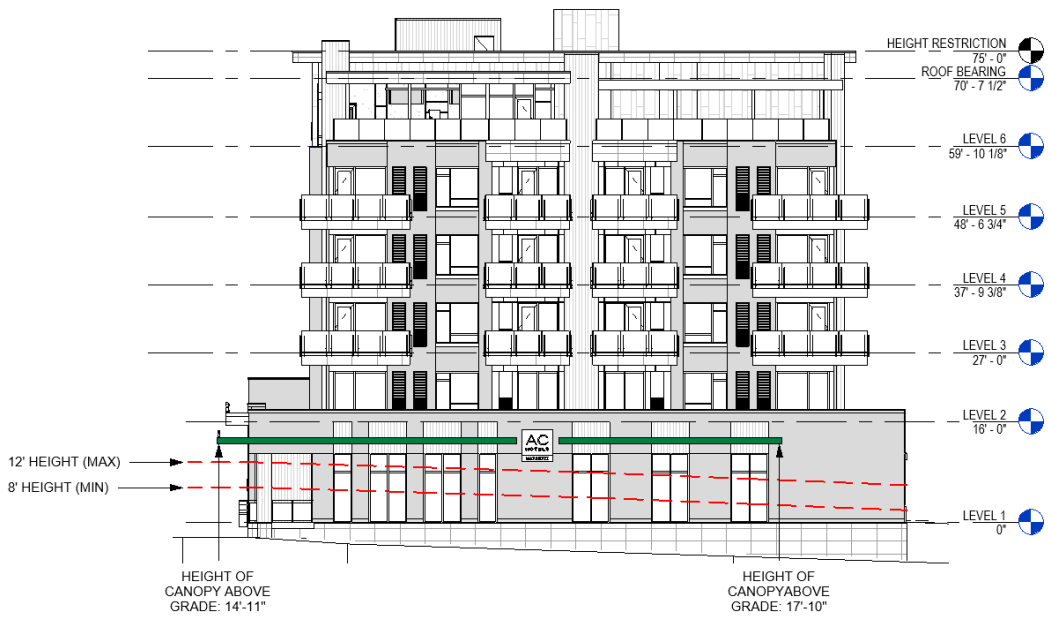
Starting near the northeast corner of our project, along Sherman Avenue, the canopy height starts at 9 feet 11 inches above the sidewalk, well within the design guideline range. To maintain clearance for any pedestrian and vehicular safety signage that will need to be suspended from the canopy above the parking garage entrance, the height of 9 feet 11 inches established. As the canopy extends west along Sherman Avenue, the sidewalk slopes down at an average of 2.8%. Due to this slope, the height of the canopy increases to 14 feet 11 inches at the northwest corner of the project, which is 2 feet 11 inches above the maximum allowable height in the guideline. I therefore request a design departure to allow a portion of the canopy to extend above the 12 foot maximum as described in the design guidelines based on the following opinions:

1. The canopy as designed meets the intent of the design guideline by providing pedestrians with cover from rainfall and snow.
2. The canopy as designed remains horizontal along the facade and does not change height (except at the hotel's main entrance), aesthetically it does not have a detrimental effect on nearby properties or the city as a whole.



3. The canopy as design covers 94% of the façade along Sherman Avenue and 81% of the façade along 6<sup>th</sup> Street. The Design Guidelines do not dictate how much of the façade needs to be covered with Canopy. With over 85% of the Sherman and 6<sup>th</sup> Street facades covered with canopy, this project offers a significant improvement over what otherwise could have been built under minimum standards and guidelines.
4. The Canopy as designed fits aesthetically with the whole of the building. The strong horizontal plane of the canopy helps define the base of the building which is an important aspect of the design guidelines.
5. If the canopy were to step down with the grade along Sherman Avenue and 6<sup>th</sup> Steet to maintain the maximum and minimum height requirement of the guidelines, the canopy would eventually intersect the exterior windows of the project on the main level and would adversely affect the interior daylight experience from individuals inside the project. From the exterior, the stepping down of the canopy along the slope of the sidewalk would adversely affect the aesthetic quality of the architecture.
6. This project is consistent with the comprehensive plan in the following ways:
  - a. It satisfies the comprehensive plan's desire for hospitality uses to help bolster Coeur d'Alene as a tourist destination and maintain the community's friendly, welcoming atmosphere and its smalltown feel.
  - b. Coeur d'Alene recognizes and celebrates its historical and cultural roots, relationship with the Coeur d'Alene Tribe, both past and present, and its connections with the natural environment. Coeur d'Alene's identity as the cultural center of North Idaho creates opportunities for social connections through a wide variety of events, activities, and public places for community members to gather year-round. This project will facilitate such gatherings as a hospitality destination.
  - c. This project will help Maintain a high quality of life for residents and businesses that make Coeur d'Alene a great place to live and visit.
  - d. It provides for nightlife activities in the form of an upscale bar and outdoor patio located on the top floor of the hotel that commands impressive view of the city and the lake, not only for the hotel guests, but for the residents of Coeur d' Alene
  - e. Its structured parking provides parking capacity for this project while keeping the walkable feel of the streets.

Below is an exhibit showing the Sherman Ave and 6<sup>th</sup> Street façades. The green areas represent the canopy and the red dashed lines represent the maximum and minimum heights as described in the guidelines as they follow the slope of the sidewalk.



I appreciate your consideration of this design departure and look forward to your questions or comments.

Sincerely,

Michael Nilson  
Project Architect

# DOWNTOWN DESIGN GUIDELINES

## DEFINITIONS

For the purposes of the Downtown Design Guidelines, the following definitions apply:

**Gateways:** Gateways are key intersections within and around the edges of downtown that require special treatment. The gateways are:

- Intersection of Sherman Ave. and Second St.
- Intersection of Sherman Ave. and Fourth St.
- Intersection of Front Ave. and Fourth St.
- Intersection of Sherman Ave. and Seventh St.

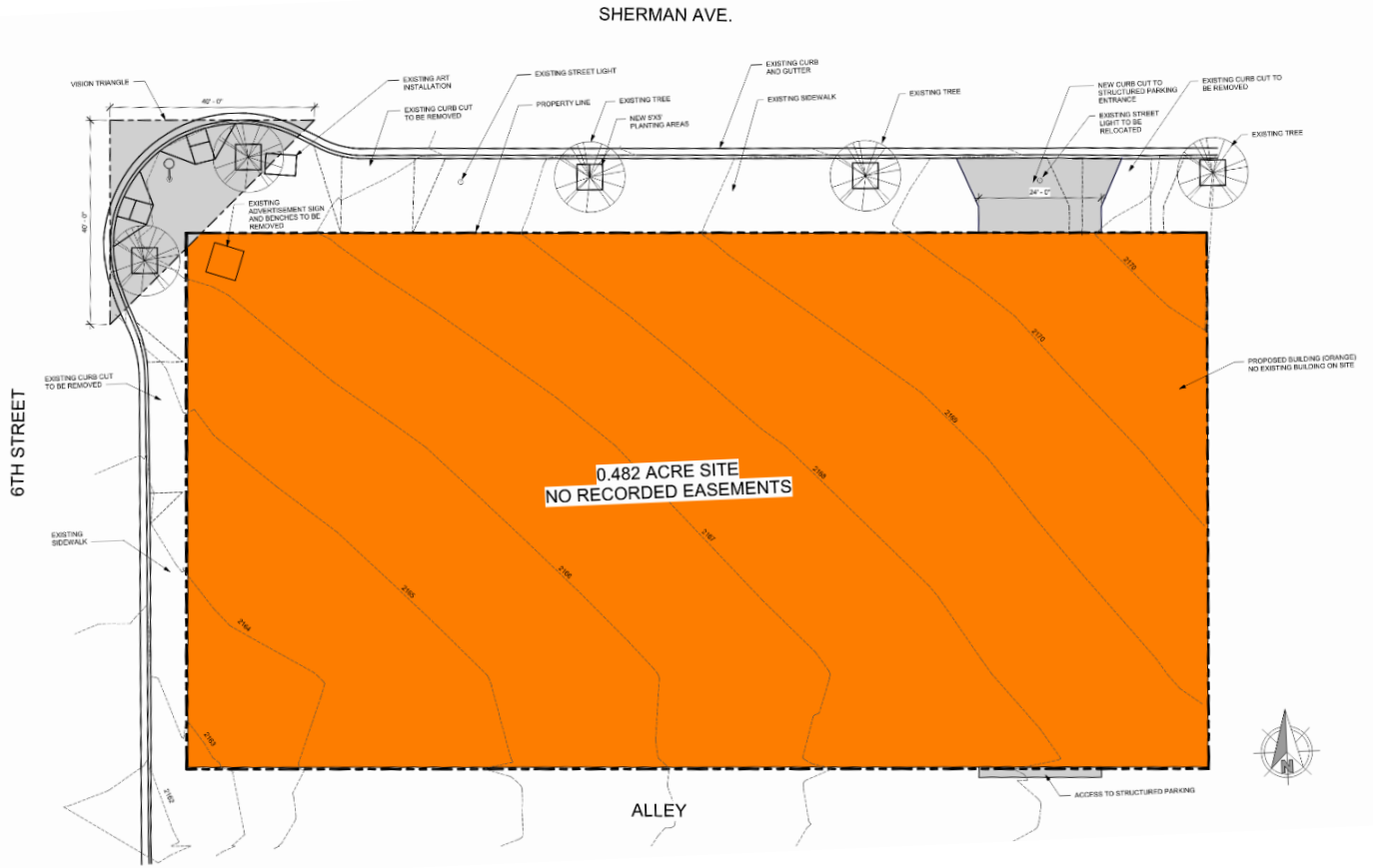
**Pedestrian-Oriented Streets:** Streets that are intended to have a lively, pedestrian friendly environment in the downtown. The pedestrian-oriented streets are:

- Sherman Ave. from Second St. to Sixth St.
- Second Ave. from Lakeside Ave. to Sherman Ave.
- Third St. from Lakeside Ave. to Front Ave.
- Fourth St. from Lakeside Ave. to Front Ave.
- Fifth St. from Lakeside Ave. to Front Ave.
- Sixth St. from Lakeside Ave. to Front Ave.

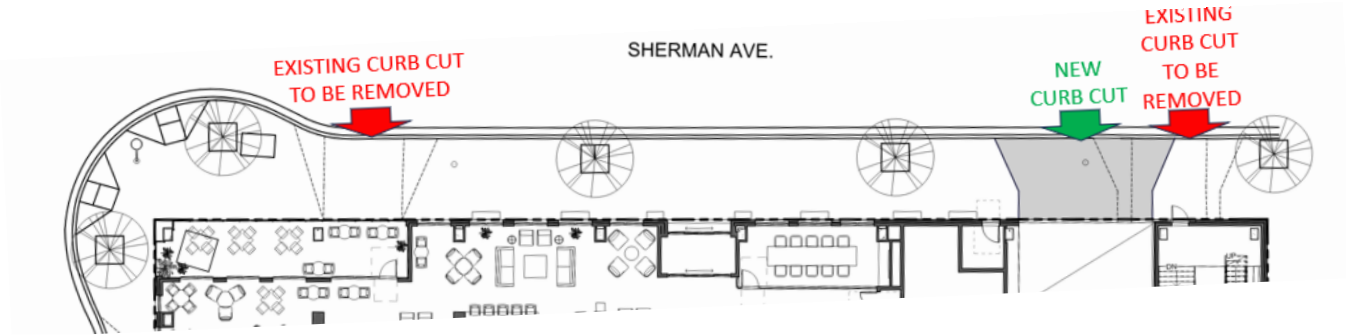
**Vehicular-Oriented Streets:** Streets that are intended to present a lively and inviting environment as vehicles drive through the downtown. The vehicular-oriented streets are:

- Northwest Blvd. from Government Way to First St.
- Lakeside Ave. from Government Way to Seventh St.
- Sherman Ave. from First St. to Second St.
- Sherman Ave. from Sixth St. to Eighth St.
- Front Ave. from Second St. to Seventh St.

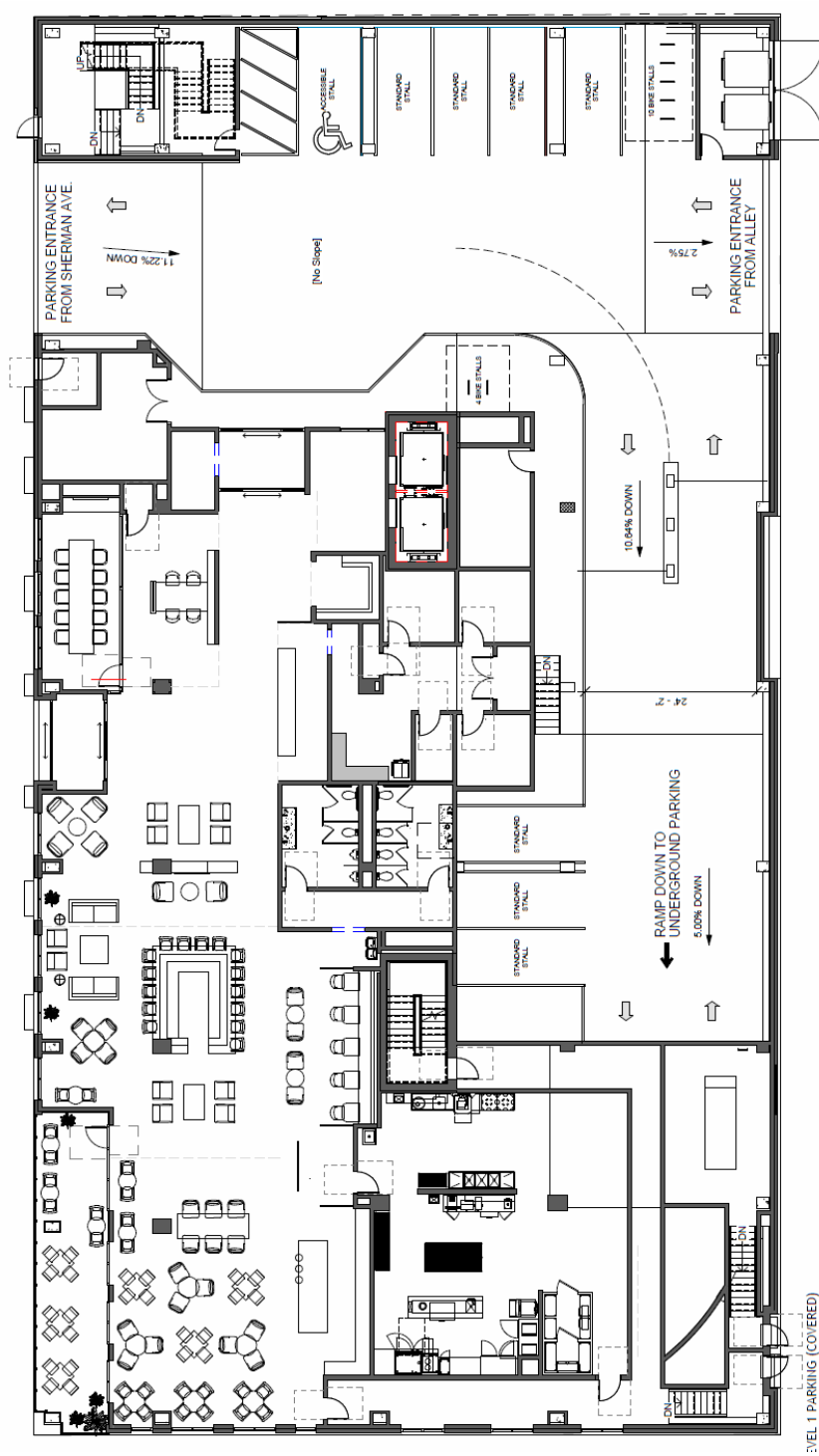
# SITE PLAN:



## PROPOSED ACCESS FROM SHERMAN AVENUE:



# LEVEL 1: WITH COVERED PARKING

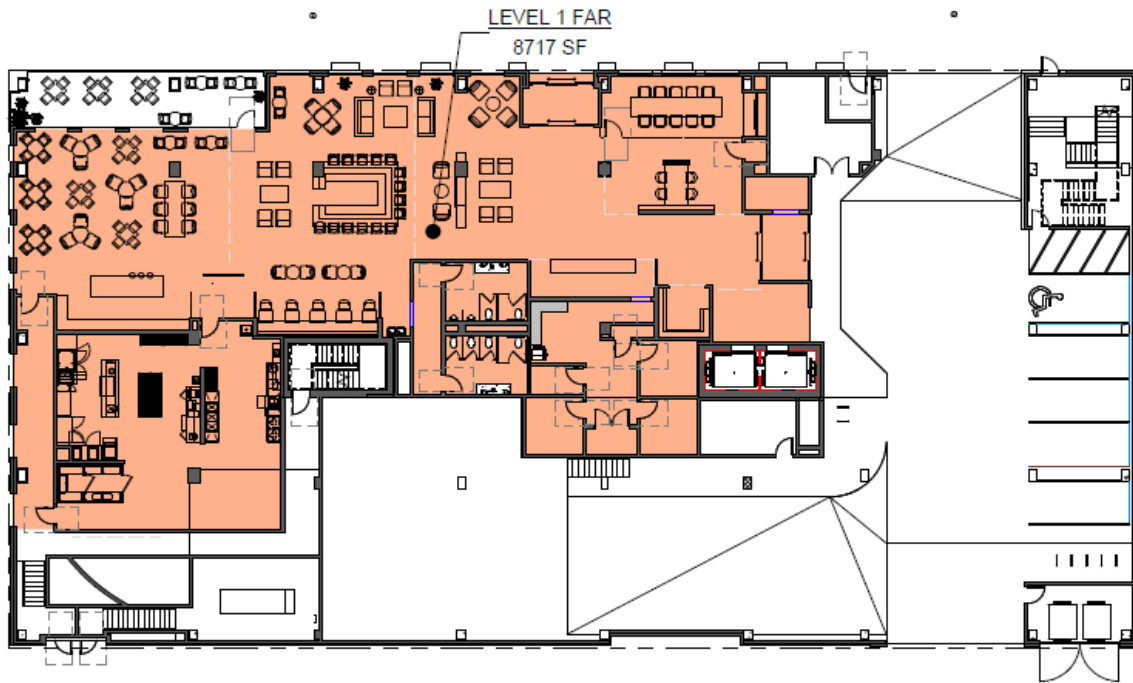


# BELOW GRADE PARKING - TYPICAL



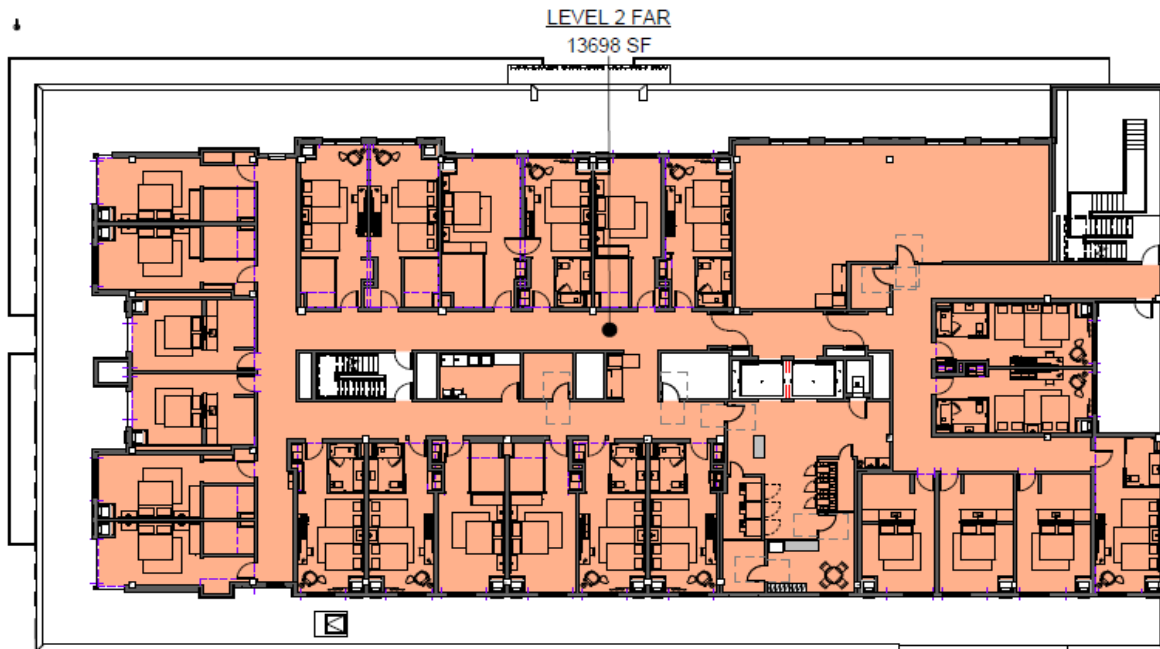
BELOW GRADE PARKING - TYPICAL LAYOUT

## FLOOR PLANS: L1



1 Level 1 (FAR)  
DR1 1" = 20'-0"

## FLOOR PLANS:L2



Level 2 (FAR)  
4" = 20'-0"



### FLOOR PLANS:L3

LEVEL 3 FAR

13362 SF



3 Level 3 (FAR)  
DR1 1" = 20'-0"

### FLOOR PLANS:L4

LEVEL 4 FAR

13360 SF



Level 4 (FAR)

**FLOOR PLANS:L5**

LEVEL 5 FAR

13360 SF

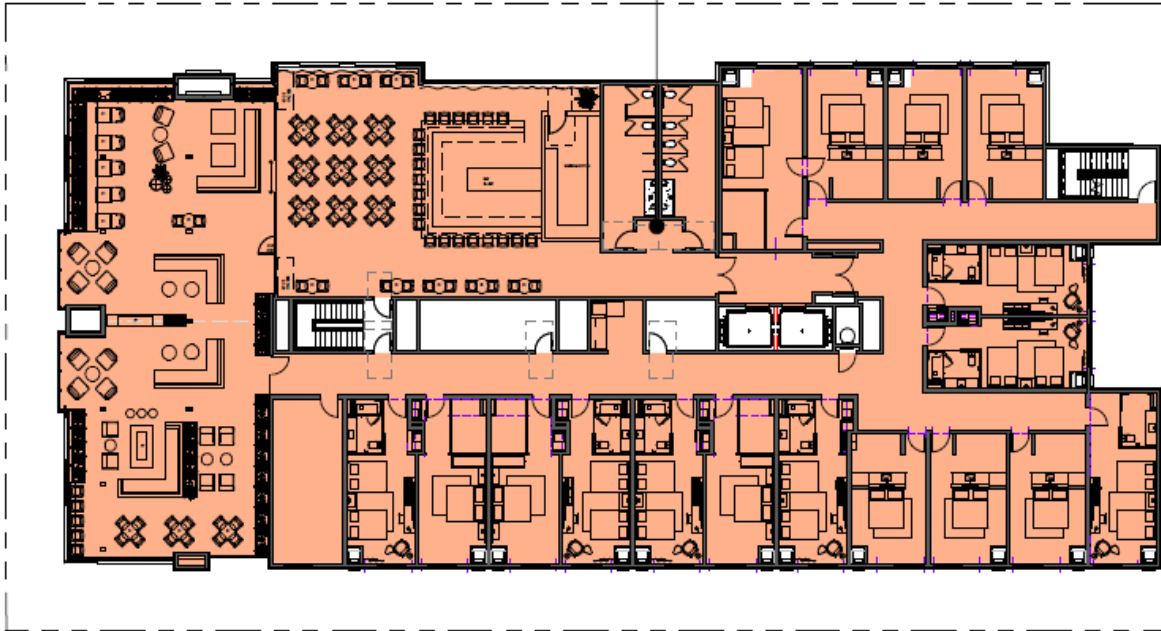


Level 5 (FAR)

**FLOOR PLANS:L6**

LEVEL 6 FAR

13511 SF

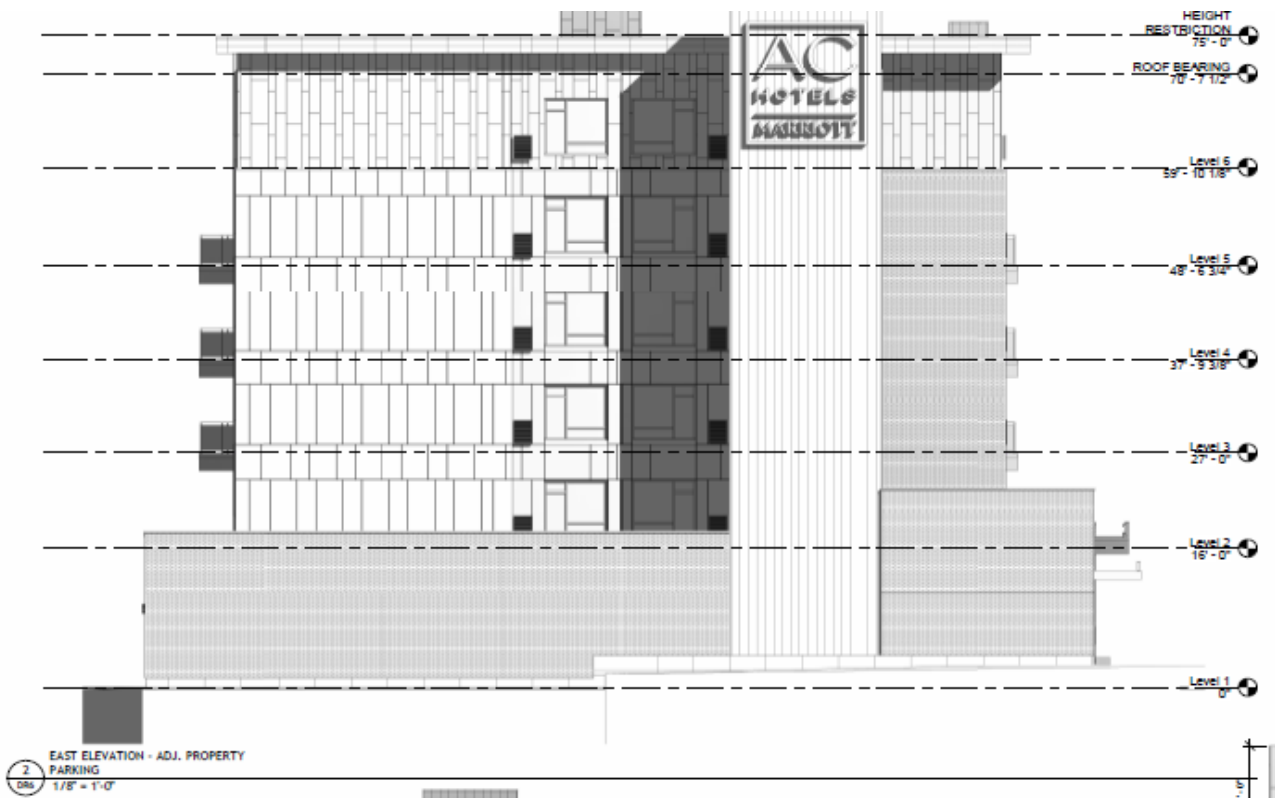


6 Level 6 (FAR)

## STREET ELEVATIONS: NORTH ELEVATION- SHERMAN AVENUE



## STREET ELEVATION: EAST ELEVATION – ADJACENT PROPERTY



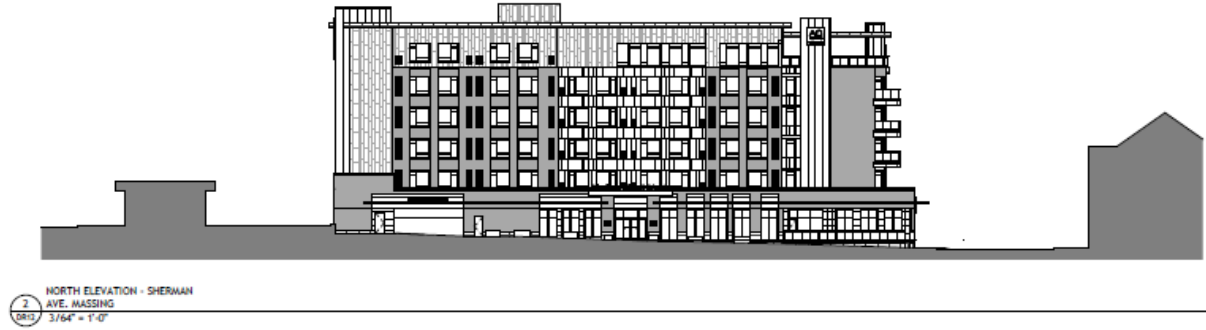
## STREET ELEVATIONS: SOUTH ELEVATION- ALLEY SIDE



## STREET ELEVATIONS: WEST ELEVATION – 6<sup>TH</sup> STREET



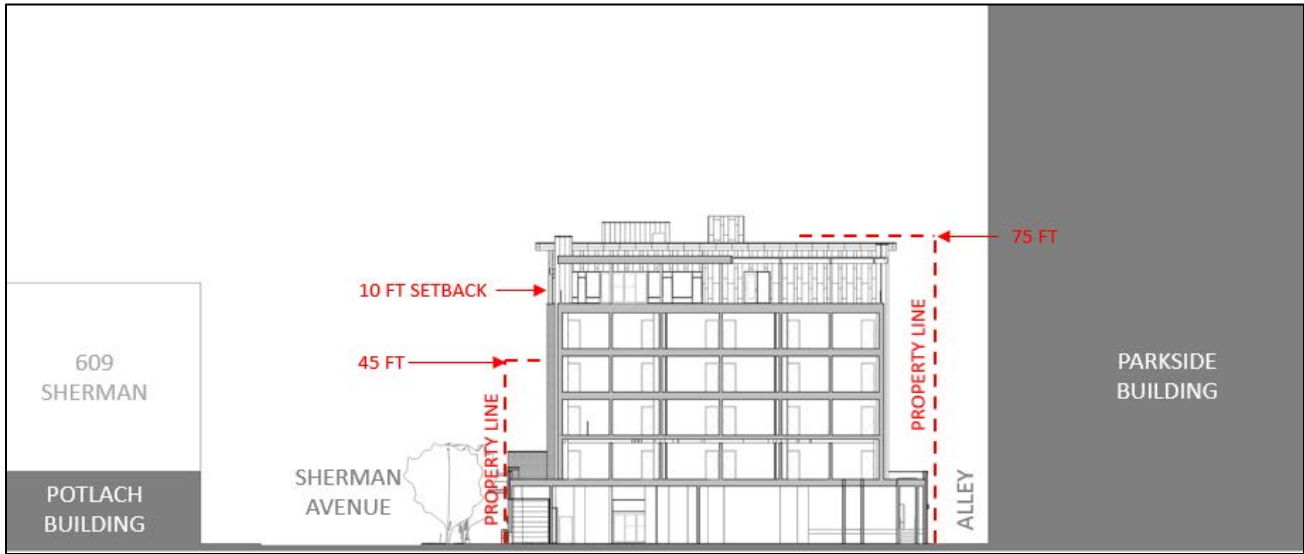
**MASSING, ORIENTATION, CONTEXT: NORTH ELEVATION SHERMAN**



**MASSING, ORIENTATION, CONTEXT: WEST ELEVATION 6<sup>TH</sup> STREET**



**SHERMAN AVENUE MASSING SECTION:**



**6<sup>TH</sup> STREET MASSING SECTION:**



**RENDERINGS: LOOKING SOUTH FROM SHERMAN AVENUE:**



**VIEW LOOKING SOUTH AT THE VEHICLE ENTRY/EXIT INTO THE STRUCTURE ALONG SHERMAN AVENUE:**



**STREETS AND ENGINEERING COMMENTS:**

Chris Bosley, City Engineer provided comments during the project review meeting held on August 1<sup>st</sup>, 2023. An updated site plan was submitted and additional comments have been provided below based on the updated site plan and renderings submitted for the proposed hotel. The City Engineer will coordinate with the development team to discuss the proposed conditions on the following page:

*(Staff comments continue on the following page.)*



- The applicant shall complete a traffic study including a pedestrian safety study that illustrates how conflicts with pedestrians will be managed.
- Pedestrian safety features recommended by the study and approved by the City shall be installed during construction.
- Sidewalks along Sherman Ave and 6<sup>th</sup> Street must be brought into ADA compliance, including replacement of cracked and broken slabs.
- Any existing driveway approaches not being used with the proposed development shall be removed. The below conditions will need to be met prior to permit sign-off.

**VIEW LOOKING WEST ON 6<sup>TH</sup> STREET:**



**VIEW LOOKING WEST ALONG THE GROUND LEVEL AT SHERMAN AND 6<sup>TH</sup> STREET:**



**NORTHEAST CORNER:**



**SOUTHWEST CORNER:**



**APPLICANT'S DESIGN GUIDELINES WORKSHEET: Downtown Core (DC)**

The applicant has provided a response and additional details on how the project has met the required Downtown Coeur d'Alene Guidelines and Development Regulations as noted in the applicant's Design Guideline worksheet below.

*Response from applicant:*

**22. Location of Parking**

Parking for the project is located within the building footprint itself. Parking takes a portion of the main (Street) level and continues three stories underground. This project does not contain any exposed surface parking lots.

Required Parking Ratio (Residential & Hotels)

- Min 0.5 stalls per unit
- Max. 2 stalls per unit

Provided Parking Stalls:

- 131 Units
- 130 Stalls
- Ratio = 0.99 stall per Unit

**2. Screening of Parking Lots**

Parking for the project is located within the building footprint itself (shown in red). The only exposed portions of the parking lot are the vehicular entrances off Sherman Avenue and the alley to the south of the property.

**3. Parking Lot Landscaping**

Parking for the project is located within the building footprint itself (shown in red above) and therefore parking lot landscape is not required.

**4. Sidewalk Uses**

**4.1 Amenity Zones:** Currently there are four trees planted along the property line facing Sherman. The trees are spaced 67', 54', and 68' respectively. Two of the trees have tree grates, the others have exposed soil. There is one tree along the property line facing 6<sup>th</sup> Street planted with a tree grate. This tree is located at the northwest corner of the property. It will be confirmed that these trees have a DBH of less than 20 inches. These trees will be removed during construction and replaced in the existing locations. This project includes creating new 5'x5' planting areas around the trees.

**4.2 Clear Walkway:** The existing sidewalk on Sherman Avenue from the back of curb to the property line is 14.8'. The distance from the new 5'x5' tree planting areas to the property line is approximately 8'-6". A 7'-0" wide clear pedestrian travel area will be maintained.

**4.3 Storefront Area:** An 18" wide area between the property line and the pedestrian travel area will be used for planting containers along Sherman Avenue.

### ***5. Width and Spacing of Curb Cuts***

Two curb cuts on Sherman and one curb cut on 6<sup>th</sup> street currently exist. All three of these existing curb cuts will be removed. The project only requires one 24' wide curb cut on Sherman; no curb cuts are being proposed on the Pedestrian-Oriented 6<sup>th</sup> Street. The sidewalk pattern and material will carry across the driveway. This project will not be sharing a driveway as it is not feasible.

### ***6. Screening of Trash/Service Areas***

The trash area is located within the building footprint, off the alley on the southeast corner of the property. The trash area will be screened from view on all sides. The two sides and rear of the enclosure will match the exterior brick material. At the front of the enclosure will be an opaque decorative architectural gate. Loading and service areas do not face any residential areas. Loading and service areas are located within the parking garage.

### ***7. Lighting Intensity***

**7.1 Building Lighting:** The majority of the exterior building lighting will be recessed lights in the roof canopies at the ground floor level to provide light to pedestrians, at the guestroom balcony roofs to provide light to the guests, and at the upper roof deck to highlight the building corner. Fully-shielded wall sconces will be added on either side of the main entry doors to highlight the entry.

**7.2 Street Lighting:** There is one existing single-arm tall streetlight at the corner of Sherman and 6<sup>th</sup> Street that will remain. There are two existing post streetlights along Sherman Avenue. One light will remain in its current location, and the other will be shifted to allow for the new curb cut into the parking structure. There are no existing streetlights along 6<sup>th</sup> Street.

### ***8. Gateways***

The Corner of Sherman and 6<sup>th</sup> Street is not classified as a "Gateway" intersection in the Downtown Design Guidelines.

An existing public art installation exists on the southeast corner of Sherman and 6<sup>th</sup> Street.

## **9. Maximum Setback**

The street level façade along the Pedestrian-Oriented 6<sup>th</sup> Street is set up to the back of the sidewalk along the property line. A portion of the project on the corner of Sherman and 6<sup>th</sup> Street has a dining patio for the use of hotel guests but it has a base structure that extends out to the sidewalk.

## **10. Orientation to the Street**

The proposed building is oriented to Sherman Avenue. The building façade along Sherman incorporates numerous windows as well as an entrance canopy and signage. The façade along 6<sup>th</sup> Street incorporates windows. The primary building entrance faces Sherman and is centered in the building façade. An outdoor patio at the ground level activates the street corner at Sherman Avenue and 6<sup>th</sup> Street.

## **11. Entrances**

The main building entrance is centered on the façade along Sherman Avenue and welcomes pedestrians with an overhanging canopy as well as a recess in the main building wall. Both the canopy and the recess provide added weather protection for pedestrians. These features, along with clear signage, help identify this visually prominent entrance.

## **12. Massing**

**12.1 Top:** The top section of the building is distinguished by overhanging roofs, an open roof deck with trellis, and additional windows. The main material is a dark metal panel, with accent metal panels.

**12.2 Middle:** The middle section of the building has a regular pattern of guestroom windows surrounded by dark and light color brick veneer. Also, there are some dark and accent metal panels to connect the base to the top.

**12.3 Base:** The base of the building features a large amount of storefront glazing and canopies to define the ground level. The finish is a combination of light grey brick, darker composite panels accented with horizontal wood siding with a decorative concrete plinth.

**12.4 Building Bulk:** The base of the building aligns with the property lines of the lot, but steps back above the ground floor level to allow for the required 10-foot setback over 45 feet above grade. The only parts of the building that extend past these setbacks are roof overhangs and balconies. The only part of the building that is taller than 75 feet is the elevator penthouse, which is much smaller than the 8000 SF Tower Floor Size restriction at 176 SF and is over the minimum Tower Separation of 50 feet noted in the Site Performance Standards. At approximately 77 feet tall, the overall building height is well below the maximum 200 ft building height.

**12.5 City Block Elevations:** Sherman Avenue has a mix of low-and mid-rise buildings, which align well with the scale of the plinth of the proposed hotel. The overall mass of the building helps transition from these shorter structures to the high-rise residential buildings on Front Avenue.

### **13. Ground Level Details**

The ground floor of the building has the most amount of character to provide visual interest to pedestrians, including:

- Pedestrian-scaled signs to identify the building entry.
- Seasonal planting in multiple planters against the building along Sherman Avenue.
- Metal canopies above the ground floor storefront windows.
- Accent wall sconces on either side of the main entrance.
- A decorative concrete plinth to ground the building.

The ground level also features an elevated patio at the corner of Sherman and 6<sup>th</sup> Street to add a further level of detail in this area.

### **14. Ground Floor Windows**

The building has been designed with many storefront windows along Sherman Avenue and 6<sup>th</sup> Street that will have clear vision glass into the Lobby, Bar/Lounge, Conference Room, and Corridor spaces (unblocked by shelving). Sherman Avenue has 45% window and glazed door area in the “window zone” of the façade. 6<sup>th</sup> Street has 26% window area in the “window zone” of the façade. All ground-floor windows will have a minimum of 60% transparency.

### **15. Weather Protection**

The building is designed with 5-foot-deep canopies around nearly the entire length of the Sherman Avenue and 6<sup>th</sup> Street façades for weather protection. The exception is at the main entry, which has a shorter canopy at 3.5 feet in depth. However, a recessed entry provides additional protection. This canopy is also 16 feet high, to accentuate the entrance to the building. The main canopies maintain a consistent level height around the building, but due to the sloping grades, the height of the canopy varies. The minimum height above grade is approximately 10 feet. The canopy will have a metal frame finish, with a wood plank soffit. These canopies will also have recessed downlights to provide lighting under the opaque covering.

### **16. Treatment of Blank Walls**

The street-facing walls of the building are mostly broken up by windows and doors, but there are additional architectural features that break up the impact of the walls, including:

1. A concrete plinth that varies in height depending on the grade change (from 1'-2" up to 6'-0").
2. A change in brick materials above the ground floor level, acting as a “belt course” for the building.
3. Recesses in the façade at least 2'-0" in depth.
4. Roof overhangs/canopies at the ground floor level and upper roof level that vary from 3'-0" to 5'-0" in depth.

Additional features at the pedestrian level include contrasting wall material and vegetated planter boxes.



### ***17. Screening of Parking Structures***

The Parking Structure is incorporated into the main hotel building, within the building footprint (shown in red), with a portion of the Main Floor (ground level) allocated to parking, as well as three underground levels. There isn't a separate parking structure to be screened. The Main Floor parking is integrated into the "plinth" on the non-street-facing façades.

### ***18. Roof Edge***

The building design doesn't include any pitched roofs. The typical roofline of the building includes a 3-foot overhanging cornice to create a prominent edge against the sky. At recessed wall locations, this overhang extends 5.5 feet past the wall face, creating an even more dramatic cornice. Additionally, the building features accent tower elements of varying heights and a roof deck with a large trellis to add increased interest at the roof edge.

### ***19. Screening of Rooftop Mechanical Equipment***

The proposed building is designed with extended parapets to screen a majority of the rooftop equipment. The only rooftop mechanical equipment that extends above the main parapet is the Elevator Penthouse, which will be surrounded by a framed wall and finished in the same dark metal panels as part of the main building façade.

### ***20. Unique Historical Features***

The existing site is a lawn-covered dog park, so the project doesn't include any renovation or redevelopment. As a new construction project, the proposed building relates to the surrounding context through:

- The use of brick as a predominant exterior finish.
- The massing of the building with a base, middle, and top.
- The scale of the building as a steppingstone between the smaller buildings along Sherman Avenue and the high-rise residential Parkside Building.
- The design of the building as a contemporary structure that relates to the primarily modern surrounding architecture.

### ***21. Integration of Signs with Architecture***

The two main building signs are placed on the vertical-wood-siding-finished vertical towers of the building for wayfinding of automobile traffic, in lieu of pylon signs. These signs are 188 SF and 36 SF, respectively. Additionally, channel letter signs are located above the ground floor canopies to designate the main entrance and the parking entrances. These signs are 42 SF for the main entrance and 14 SF (each) for the two parking entrances. Lastly, there are two placard signs on either side of the main entrance doors for pedestrian wayfinding. These two signs are 4 SF each. The total building signage area is 302 SF, which is less than the maximum 603 SF allowed based on the frontage.

## **22. Creativity/Individuality of Signs**

The signage for the building was selected from the Brand's standard signage options. Their designs are highly graphic for brand identity, but also offer a variety of installations and styles including typical wall signs, channel letter wall signs, freestanding channel letter entry signs, as well as smaller pedestrian-oriented placard signs at the entry doors. The freestanding channel letter sign at the entry canopy is supported by brackets and directs pedestrians to the building entry.

We are happy to answer any questions you may have during your review. Thank you in advance for your time and careful consideration.

**RENDERING: VIEW FROM THE INTERSECTION OF SHERMAN AND 6<sup>TH</sup> STREET LOOKING SOUTH**



EXTERIOR RENDERING AT DUSK

**PERSPECTIVE VIEW- FROM SHERMAN AVENUE AT MAIN ENTRY AND VEHICULAR ACCESS.**



**PERSPECTIVE VIEW- MAIN ENTRANCE OFF OF SHERMAN AVENUE:**



**PERSPECTIVE VIEW- PARTIAL NORTH ELEVATION- SHERMAN AVENUE:**





**RENDERING: SOUTHEAST VIEW**



**Per the Downtown Design Guidelines:**

**C. Maximum Building Height**

Buildings within this district shall only be permitted to exceed 75 feet if they comply with the bulk, spacing, and setback standards indicated in the sections that follow. Buildings that comply with the standards, as well as accumulate sufficient Floor Area Ratio through bonuses, may extend as high as shown in the chart below.

	Height	Height w/architectural feature <sup>2</sup> .
Base	75 ft.	83 ft.
Base + Bonus	200 ft	220 ft.

**Notes:**

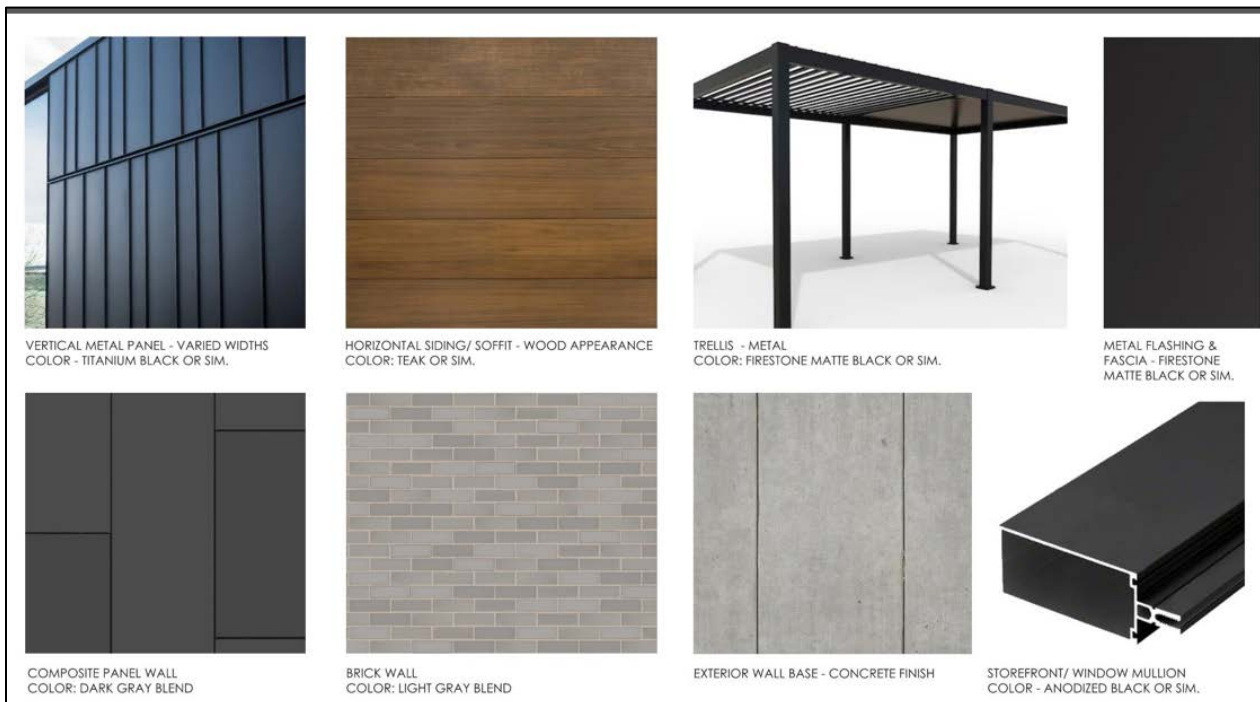
1. Mechanical penthouses, stair/elevator overruns, and antennae may be excluded from Building Height calculation provided they are no more than 15 feet above the roof deck.
2. The Base Building height may be increased by up to 10% if the top is designed as a non-habitable, architectural element. This element may extend above the increased height limit.



**EVALUATION:**

The maximum height in the DC (Downtown Core) zoning district is 200' without any architectural feature to allow for an increased maximum height of 220'. With an architectural feature, the maximum height increases to 220'. The proposed structure is 75' with an additional 11' for the stair overrun and HVAC screening.

**EXTERIOR MATERIAL SAMPLE BOARD:**





## NORTH ELEVATION (SHERMAN AVENUE) PROPOSED MATERIALS



### STAFF EVALUATION OF FACTS

- The subject property is located at 602 and 612 E. Sherman Avenue in the Downtown Core (DC) zoning district, which requires review and approval of the design by the City's Design Review Commission.
- The property is subject to the Downtown Core Design Guidelines and the Downtown Development Standards.
- The applicant has submitted all required materials for design review.
- The applicant has completed a project review meeting on August 1, 2023.
- The applicant has completed an initial meeting with staff on October 21, 2023.
- The applicant is seeking design review from the Design Review Commission at an initial meeting on January 25, 2024.
- 136 public hearing notices were mailed on January 10, 2024.
- The public hearing notice was published in the Coeur d'Alene Press on January 6, 2024.
- The subject property was posted with the public hearing notice on January 11, 2024.
- Sherman Avenue in the project vicinity is designated as a Vehicle-Oriented Street.
- 6<sup>th</sup> Street in the project vicinity is designated as a Pedestrian-Oriented Street.
- The applicant has requested a design departure for Weather Protection as noted below.

- The subject property is 20,993 square feet and the building square footage would be 20,886 square feet, which is 99.4% site coverage. This equates to less than 1.0 FAR, which is less than is allowed by right with the DC zoning district. No FAR bonuses have been requested. (FAR BONUSES)
- The proposed project would be 6 stories and 75' tall, which is below the maximum allowable height of 200' in the DC zoning district. (BUILDING HEIGHT)
- The DC zoning district requires 0.5 parking stalls per unit. The proposed project would have 131 hotel rooms and provides 130 parking spaces enclosed within the structure, which is 65 more than is required by the Downtown Development Standards (Restaurants less than 3,000 S.F. are exempt from parking requirements.) (PARKING COUNT & LOCATION)
- The four existing street trees will be replaced with street trees per City standards and will include new 5'x5' tree planting areas around the trees. (SIDEWALK USES – AMENITY ZONES)
- The existing sidewalk on Sherman Avenue from the back of curb to the property line is 14.8'. The distance from the new 5'x5' tree planting areas to the property line is approximately 8'-6". A 7'-0" wide clear pedestrian travel area will be maintained. (SIDEWALK USES – CLEAR WALKWAY)
- An 18" wide area between the property line and the pedestrian travel area will be used for planting containers along Sherman Avenue. (SIDEWALK USES – STOREFRONT AREA)
- Three (3) existing curb cuts will be removed – one along 6<sup>th</sup> Street and two along Sherman Avenue. Only one new 24" wide curb cut will be required on Sherman Avenue for the project. No curb cuts will be on 6<sup>th</sup> Street, which is a pedestrian-oriented street. For the new curb cut required for the driveway into the parking structure, the sidewalk pattern and material will carry across the driveway. (WIDTH AND SPACING OF CURB CUTS)
- The trash area will be located behind the building off of the alley on the southeast corner of the property and will be screened from view on all sides. The enclosure will be constructed with brick to match the building and will have an opaque decorative architectural gate. (SCREENING OF TRASH)
- Loading and service areas will be located within the parking structure. (SCREENING OF SERVICE AREAS)
- Exterior lighting on the building will be recessed in the roof canopies at the ground floor level to provide pedestrian lighting. Guestroom balcony roofs will have lighting and the upper roof deck will have lighting to highlight the building corner. Fully shielded wall scones will be provided on either side of the main entry doors. (LIGHTING INTENSITY – BUILDING LIGHTING)
- There is one existing single-arm tall streetlight at the corner of Sherman and 6<sup>th</sup> Street that will remain. There are two existing post streetlights along Sherman Avenue. One light will remain in its current location, and the other will be shifted to allow for the new curb cut into the parking structure. There are no existing streetlights along 6<sup>th</sup> Street. (LIGHTING INTENSITY – STREET LIGHTING)

- The DC zoning district has a 0' front and side yard setback, unless providing usable public space, forecourts or vegetative screening of parking structures. Buildings may be set back from the sidewalk a maximum of 20' for public space or entries, or a maximum of 10' for vegetative screening. Setting façades close to the street may be accomplished through base structures that extend out to the sidewalk, not necessarily the full height of the building. The building meets this requirement. The street level façade along the Pedestrian-Oriented 6<sup>th</sup> Street is set up to the back of the sidewalk along the property line. A portion of the project on the corner of Sherman and 6<sup>th</sup> Street has a dining patio for the use of hotel guests but it has a base structure that extends out to the sidewalk. (MAXIMUM SETBACK)
- The proposed building is oriented to Sherman Avenue. The building façade along Sherman incorporates numerous windows as well as an entrance canopy and signage. The façade along 6<sup>th</sup> Street incorporates windows. The primary building entrance faces Sherman and is centered in the building façade. An outdoor patio at the ground level activates the street corner at Sherman Avenue and 6<sup>th</sup> Street. (ORIENTATION TO THE STREET)
- The DC design guidelines require the principal entry to have two elements. The main building entrance is centered on the façade along Sherman Avenue and welcomes pedestrians with an overhanging canopy as well as a recess in the main building wall. Those are both allowed design elements. Some form of weather protection shall also be provided. Both the canopy and the recess provide added weather protection for pedestrians. These features, along with clear signage, help identify this visually prominent entrance. (ENTRANCES)
- The proposed structure incorporates a top, middle and base, as required by the DC zoning district (MASSING)
  - The top section of the building is distinguished by overhanging roofs, an open roof deck with trellis, and additional windows. The main material is a dark metal panel, with accent metal panels. (TOP)
  - The middle section of the building has a regular pattern of guestroom windows surrounded by dark and light color brick veneer. Also, there are some dark and accent metal panels to connect the base to the top. (MIDDLE)
  - The base of the building features a large amount of storefront glazing and canopies to define the ground level. The finish is a combination of light grey brick, darker composite panels accented with horizontal wood siding with a decorative concrete plinth. (BASE)
  - The base of the building aligns with the property lines of the lot, but steps back above the ground floor level to allow for the required 10-foot setback over 45 feet above grade. The only parts of the building that extend past these setbacks are roof overhangs and balconies. The only part of the building that is taller than 75 feet is the elevator penthouse, which is much smaller than the 8000 SF Tower Floor Size restriction at 176 SF and is over the minimum Tower Separation of 50 feet noted in the Site Performance Standards. At approximately 77 feet tall, the overall building height is well below the maximum 200 ft building height. (BUILDING BULK)
  - Sherman Avenue has a mix of low-and mid-rise buildings, which align well with the scale of the plinth of the proposed hotel. The overall mass of the building helps transition from these shorter structures to the high-rise residential buildings on Front Avenue. (CITY BLOCK ELEVATIONS)

The ground floor of the building has the most amount of character to provide visual interest to pedestrians, including: Pedestrian-scaled signs to identify the building entry; seasonal planting in multiple planters against the building along Sherman Avenue; metal canopies above the ground floor storefront windows; accent wall sconces on either side of the main entrance; and a decorative concrete plinth to ground the building. The ground level also features an elevated patio at the corner of Sherman and 6<sup>th</sup> Street to add a further level of detail in this area. (GROUND LEVEL DETAILS)

- The proposed structure would meet the minimum glazing requirement for Ground Floor Windows by providing 40% window and glazed door area in the “window zone” of the façade along Sherman Avenue and 26% “window area” in the window zone along the 6<sup>th</sup> Street façade (GROUND FLOOR WINDOWS)
- The DC design guidelines require a visual connection between activities inside and outside the building. Ground level façades oriented to pedestrian-oriented streets require a minimum of 60% transparency and vehicular-oriented streets require a minimum of 40% transparency. The proposed structure would meet the transparency requirement for ground floor windows with a minimum of 60% transparency. (GROUND FLOOR WINDOWS)
- The proposed canopy meets the minimum depth to provide weather protection per the DC design guidelines. The DC design guidelines require a minimum depth of a canopy or awning to be 5’. The 5’ deep canopies associated with the building meet the minimum requirement to provide pedestrians from weather. The exception is at the main entry, which has a shorter canopy at 3.5 feet in depth. However, a recessed entry provides additional protection. The applicant has requested a design departure for Weather Protection related to the vertical dimension between the underside of the canopy or awning and the sidewalk. Per the DC design guidelines, the vertical dimension between the underside of the canopy or awning and the sidewalk shall be at least 8’ and no more than 12’. The proposed design has a canopy height starting at 9’11” above the sidewalk and has a clearance for pedestrian and vehicular safety signage suspended from the canopy above the parking garage entrance at a height of 9’11”. As the sidewalk slopes down at an average of 2.8% to the west, the canopy’s vertical height increases to 14’11” at the northwest corner of the project, which is 2’11” above the maximum allowable height. Along 6th Street at the lowest grade, the canopy would have a vertical dimension of 17’10”. The requested design departure is to exceed a portion of the canopy to extend above the 12’ maximum design guideline. The architect outlines the justification as the departure of the canopy height would still meet the weather protection requirement for pedestrians, the canopy would maintain a consistent horizontal aesthetic that would allow for the storefront windows to remain a consistent size and allow for maximum interior daylight. Stepping down of the canopy to meet the guideline would adversely affect the aesthetic quality of the architecture. Maintaining a consistent horizontal plane with the canopy also defines the base of the building, which is an important aspect of the design guidelines. The canopy will have a metal frame finish, with a wood plank soffit. These canopies will also have recessed downlights to provide lighting under the opaque covering. The applicant maintains the design of the proposed canopy with the increased vertical dimension and overall aesthetic is a significant improvement over what could have otherwise been built under minimum standards and guidelines. The applicant provided references to applicable sections of the Comprehensive Plan, including Community & Identity: Goal CI 2 (Maintain a high quality of life for residents and businesses that make Coeur d’Alene a great place to live and visit), Objective CI 2.1 (Maintain the community’s friendly, welcoming atmosphere and its small-town feel), and Objective CI 2.2 Support programs that preserve historical collections, key community features, cultural heritage, and

traditions), and the key characteristics of the Downtown as highly walkable with a defined urban form that attracts area residents and tourists to the area. The design departure request includes two exhibits showing how the canopy would look if it were to meet the guideline. (WEATHER PROTECTION) – DESIGN DEPARTURE REQUESTED

The proposed design is in compliance with the treatment of blank walls. The street-facing walls of the building are mostly broken up by windows and doors, but there are additional architectural features that break up the impact of the walls, including: a concrete plinth that varies in height depending on the grade change (from 1'-2" up to 6'-0"); a change in brick materials above the ground floor level, acting as a "belt course" for the building; recesses in the façade at least 2'-0" in depth; and roof overhangs/canopies at the ground floor level and upper roof level that vary from 3'-0" to 5'-0" in depth. Additional features at the pedestrian level include contrasting wall material and vegetated planter boxes (TREATMENT OF BLANK WALLS)

- The parking for the project is screened by being designed as part of the building. Other than the entrance, the parking is hidden from view. The main floor parking is integrated into the "plinth" on the no-street facing façades.(SCREENING OF PARKING STRUCTURES)
- The building design doesn't include any pitched roofs. The typical roofline of the building includes a 3' overhanging cornice to create a prominent edge against the sky. At recessed wall locations, this overhang extends 5'6" feet past the wall face, creating an even more dramatic cornice. Additionally, the building features accent tower elements of varying heights and a roof deck with a large trellis to add increased interest at the roof edge. (ROOF EDGE)
- The proposed building is designed with extended parapets to screen a majority of the rooftop equipment. The only rooftop mechanical equipment that extends above the main parapet is the Elevator Penthouse, which will be surrounded by a framed wall and finished in the same dark metal panels as part of the main building façade. (SCREENING OF ROOFTOP MECHANICAL EQUIPMENT)
- The DC design guidelines require new projects to relate to the context of the downtown's historical features. The existing site is a lawn-covered dog park, so the project doesn't include any renovation or redevelopment. As a new construction project, the proposed building relates to the surrounding context through: the use of brick as a predominant exterior finish; the massing of the building with a base, middle, and top; the scale of the building as a steppingstone between the smaller buildings along Sherman Avenue and the high-rise residential Parkside Building. The design of the building as a contemporary structure that relates to the primarily modern surrounding architecture. (UNIQUE HISTORIC FEATURES)
- The two main building signs are placed on the vertical-wood-siding-finished vertical towers of the building for wayfinding of automobile traffic, in lieu of pylon signs. These signs are 188 SF and 36 SF, respectively. Additionally, channel letter signs are located above the ground floor canopies to designate the main entrance and the parking entrances. These signs are 42 SF for the main entrance and 14 SF (each) for the two parking entrances. There are two placard signs on either side of the main entrance doors for pedestrian wayfinding. (INTEGRATION OF SIGNS WITH ARCHITECTURE)
- The signage for the building was selected from the Brand's standard signage options. Their designs are highly graphic for brand identity, but also offer a variety of installations and styles including typical wall signs, channel letter wall signs, freestanding channel letter entry signs, as well as smaller pedestrian-oriented placard signs at the entry doors. The freestanding

channel letter sign at the entry canopy is supported by brackets and directs pedestrians to the building entry. (CREATIVITY/INDIVIDUALITY OF SIGNS)

- The total building signage would total 302 square feet, which would be under the City's maximum sign allowance of 603 square feet under the Sign Code based on the property frontage. (SIGN ALLOWANCE)
- The DC zoning district requires that building floors over 45' in height above grade shall be stepped back 10' from the right-of-way on 6<sup>th</sup> Street. The project design does meet this requirement. The base of the building aligns with the property lines of the lot, but steps back above the ground floor level to allow for the required 10-foot setback over 45 feet above grade. The only parts of the building that extend past these setbacks are roof overhangs and balconies. (UPPER LEVEL STEPBACK)
- The following design guidelines and development standards are not applicable: Screening of Parking Lots, Parking Lot Landscaping, and Gateways.
- The Planning Department has provided a recommended condition of approval relating to consistency with the approved design, as noted below.
- The City Engineer has provided recommended conditions of approval for consideration by the DRC to ensure compliance with City Codes related to pedestrian safety, as noted below.

## **RECOMMENDED CONDITIONS OF APPROVAL**

### **Planning:**

1. The proposed design shall be substantially similar to those submitted with Item DR-1-24.

### **Engineering:**

2. Sidewalks along Sherman Ave and 6<sup>th</sup> Street must be brought into ADA compliance.
3. Any existing driveway approaches not being used with the proposed development shall be removed.
4. The applicant shall complete a traffic study including a pedestrian safety study.
5. Pedestrian safety features recommended by the study and approved by the City shall be installed.

## **DESIGN REVIEW COMMISSION'S ROLE**

The DRC may provide input on the proposed design and shall identify any changes to the proposed project which are needed in order for the project to comply with the required design standards and guidelines. The DRC must determine, based on the information before it, whether the proposed project meets the applicable Downtown Development Guidelines,. The DRC should identify the specific elements that meet or do not meet the guidelines in its Record of Decision.



## **DECISION POINT**

The DRC should grant the application in Item DR-1-24, a request by Michael Nilson, The Richardson Design Partnership, on behalf of CDA Hotel LLC, a six (6) story hotel with below grade parking along Sherman Avenue, located at 602 & 612 E Sherman Avenue, Coeur d'Alene, Idaho, be approved with or without conditions, or determine that the project would benefit from an additional DRC Meeting to review project changes in response to the first DRC Meeting or if it is deemed necessary based on all the circumstances.

### **Attachments:**

Application & Applicant's Narrative

## ***Applicant's Narrative:***

December 1, 2023

City of Coeur d'Alene  
Planning Department  
710 E. Mullan Ave  
Coeur d'Alene, ID 83814

RE: Design Review Application Narrative  
AC Hotels Marriott Coeur d'Alene  
602-612 E. Sherman Ave.  
Coeur d'Alene, Idaho 83814

Ladies and Gentlemen:

On behalf of Providence Development (Applicant, or "Providence"), we are submitting this written narrative as part of the Design Review application for the development of a new hotel on a vacant 0.482 acre parcel of land located at 602 E. and 612 E. Sherman Ave.

The Richardson Design Partnership, LLC. (TRDP) has coordinated and substantially prepared this Design Review Application package to demonstrate compliance with the City of Coeur d'Alene (City) design guidelines and standards. TRDP also designed the hotel.

### Project Description:

Providence has extensive experience developing best-in-class real estate projects that support and improve the communities in which they operate. Providence calls it: *"Helping communities reach their full potential"*. The proposed project is an upscale select service hotel and bar with approximately 131 rooms that caters to both short-term guests as well as bar patrons. The goal is to attract visitors and locals alike.

The building is designed with neutral gray and warm wood tones to allow the beauty of the natural environment of this area to shine. The predominantly brick finish façades blend with the many brick buildings nearby. Lighting is provided to light sidewalks around the site, while also preserving the dark skies of the region. Metal canopies protect and large planted containers line the sidewalk along Sherman Avenue to further enliven the pedestrian experience. The corner of the building hosts the hotel bar, that includes an outdoor patio, which will bring activity to the pedestrian-friendly streets. The hotel also provides a rooftop bar and lounge that can accommodate large gatherings of both hotel guests and locals, that will have stunning views of the nearby lake and natural environment from its outdoor deck. There is also a fitness center and large amount of parking in the underground parking structure that will have limited visibility to neighbors.

The proposed hotel location at the corner of Sherman Avenue and 6<sup>th</sup> Street is an ideal spot for a hotel of this size and caliber. The mid-rise hotel will have views of both Coeur d'Alene Lake and Lake Fernan from its upper floors, as well as access to McEuen Park, Tubbs Hill, and the nearby beaches of Coeur d'Alene Lake for

recreational activities. The retail and restaurant options along Sherman Avenue will provide many amenities within walking distance (amounting to a walkable score of 85, or very walkable), but also quick access by car to the I-90 freeway down Sherman. There are also two bus stops around the corner on Lakeside Avenue. If hotel guests want more variety than provided by the hotel bar, some of the best restaurants in the City – including Fire Artisan Pizza and Crafted Tap House & Kitchen – are just steps away. Visitors will also appreciate having Coeur d’Alene Coffee, Vault Coffee, and Highlands Day Spa just down the street. This hotel is a perfect complement to the surrounding businesses, and it will support the needs of its neighbors.

Providence, in conjunction with The Richardson Design Partnership, looks forward to welcoming guests into a unique hotel that captures a contemporary look and feel, while adding to the pedestrian experience in the Downtown Core.

**Project Overview of Proposed Development:**

Site Area:	20,993 S.F.	0.482 Acres
Total Building Area:	155,429 S.F.	
Building Footprint:	20,886 S.F.	99.4% Site Coverage
Building Height:	6 Stories	
Parking:	130 Stalls	
Guestrooms:	131 Guestrooms	

We have organized this narrative to generally coincide with The City of Coeur d’Alene Downtown Core (DC) Commercial Design Guidelines.

Thank you in advance for your time and careful consideration of this application.

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# APPLICANT'S APPLICATION

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# DESIGN REVIEW APPLICATION

**STAFF USE ONLY**  
Date Submitted: 12-1-23 Received by: T. Stroud Fee paid: \$1006.00 Project # DR-1-24

## REQUIRED SUBMITTALS

PAID

DEC 01 2023

**Application Fee: \$ 700.00**

Publication Fee: \$300.00

Mailing Fee: \$6.00 per public hearing

CITY OF COEUR D'ALENE

A **COMPLETE APPLICATION** is required at time of application submittal, as determined and accepted by the Planning Department located at <http://cdaid.org/1105/departments/planning/application-forms>.

- Completed application form**
- Application, Publication, and Mailing Fees**
- A report(s) by an Idaho licensed Title Company:** Owner's list and three (3) sets of mailing labels with the owner's addresses prepared by a title company, using the last known name/address from the latest tax roll of the County records. This shall include the following:
  1. All property owners within 300ft of the external boundaries. **\* Non-owners list no longer required\***
  2. All property owners with the property boundaries.
- A report(s) by an Idaho licensed Title Company:** Title report(s) with correct ownership easements, and encumbrances prepared by a title insurance company and a copy of the tax map showing the 300ft mailing boundary around the subject property. The report(s) shall be a full Title Report and include the Listing Packet.
- A written narrative:** Description of proposal and/or property use.
- A legal description:** in MS Word compatible format, together with a meets and bounds map stamped by a licensed Surveyor.
- Infill Design Guideline Worksheet:** (Attached) Please fill out the appropriate Infill Worksheet for your project.

## APPLICATION DOCUMENTS:

**A. Purpose of Application Submittals:** Purpose of Application Submittals: A development applicant shall participate in the design review process as required by this Article before substantive design decisions are fixed and difficult or expensive to alter. The City will work with the applicant in a collaborative fashion so that the goals of both the City and the applicant can be met to the greatest degree possible, and to address the concerns of neighbors and the community.

In order for this process to work effectively, the applicant must be willing to consider options for the project's basic form, orientation, massing, relationships to existing sites and structures, surrounding street and sidewalks, and appearance from a distance.

**B. Materials to Be Submitted for Initial Meeting with Planning Staff:** Not later than fifteen (15) days before the Initial Meeting with staff, the applicant must submit the supplemental and updated information required by this subsection to the Director. If all required items are not submitted two weeks prior to the scheduled meeting, the Director may postpone the Initial



THE  
RICHARDSON  
DESIGN  
PARTNERSHIP  
L.L.C.

## TRANSMITTAL LETTER

To: Tami Stroud

From: Michael Nilson

Company: Coeur d'Alene Planning  
Department

Date: 11/30/2023

Address:  
City of Coeur d'Alene  
710 E. Mullan Ave  
Coeur d'Alene, ID 83814

Project:  
**AC Hotels Coeur d'Alene**

Re: **Design Review Submittal**

Project No.:  
**23-138**

We are sending the following:

- |   |  |  |
|---|--|--|
| <input checked="" type="checkbox"/> Original Drawings | <input type="checkbox"/> Shop Drawings       | <input type="checkbox"/> Samples         |
| <input type="checkbox"/> Reports                      | <input type="checkbox"/> Blue Print Drawings | <input type="checkbox"/> Other           |
| <input type="checkbox"/> Change Order                 | <input type="checkbox"/> Specifications      | <input type="checkbox"/> Shop submittals |

These are transmitted as checked below:

- |  |   |  |
|--|---|--|
| <input checked="" type="checkbox"/> For approval | <input type="checkbox"/> Approved as noted      | <input type="checkbox"/> Copies for approval     |
| <input checked="" type="checkbox"/> For your use | <input type="checkbox"/> Return for corrections | <input type="checkbox"/> Copies for distribution |
| <input checked="" type="checkbox"/> As requested | <input type="checkbox"/> Submit                 | <input type="checkbox"/> Correction prints       |
| <input type="checkbox"/> For review and comment  | <input type="checkbox"/> Resubmit               | <input type="checkbox"/> No Exceptions Noted     |
| <input type="checkbox"/> Approved as submitted   | <input type="checkbox"/> Return                 |  |

Number of Copies

1

Description:

Design Review Submittal Package containing application, check payment, required documentation, and thumb drive with electronic copy of technical drawings, written narrative, and Powerpoint presentation

Remarks:

Signed: Michael Nilson

Title: Project Architect

- Our Messenger
- Mail
- Your Messenger
- Express



- Meeting to a later date. Prior to the Initial Meeting with Planning staff, all Floor Area Ratio (F.A.R.) development bonuses must be approved by the Community Planning Director, or his or her designee.
- After the Initial Meeting, the Director shall schedule the Second Meeting with the Commission for a date not less than thirty (30) days after the Initial Meeting. In the Director's discretion, any meeting may be scheduled at an earlier or later date if it is in the best interests of the Commission, the applicant, or staff.
1. A complete application (including the applicable fee); and
  2. A site map, showing property lines, rights of way, easements, topography, existing and proposed building footprints (if applicable), major landscaped areas, parking, access, sidewalks amenities and public areas; and
  3. A context map, showing building footprints and uses of parcels within three hundred feet (300'); and
  4. A written narrative including: A summary of the development plan including the areas for each use, number of floors, ~~etc.~~ total square footage and total acreage, and any information that will clarify the proposed project); and; a detailed description of how the project meets each applicable design guideline and design standards, including images/exhibits, and any design departures, and all revisions to the project made as a result of the initial meeting with staff. The narrative shall also include a description and photos detailing proximity to major roads, view corridors, and neighborhood context.
  5. General parking information including the number of stalls, dimensions of the parking stalls, access point(s), circulation plan, any covered parking areas, bicycle parking (included enclosed bike storage areas), and whether the parking will be surface or structured parking; and
  6. An ownership list prepared by a title insurance company, listing the owners of property within a three hundred foot (300') radius of the external boundaries of the subject property. The list shall include the last known name and address of such owners as shown on the latest adopted tax roll of the county; and
  7. Photographs of nearby buildings that are visible from the site, from different vantage points with a key map; and
  8. Views of the site, with a key map; and
  9. A generalized massing, bulk and orientation study of the proposal; and
  10. Elevations of the conceptual design for all sides of the proposal and an elevation along the block, showing massing of the proposal; and
  11. An exhibit showing existing and proposed grade; and
  12. Project inspiration images.



13. Sample of materials and colors, both physically and an electronic copy; and
14. A PowerPoint presentation that includes a detailed description of how the project meets each finding and any design departures, and addressing all of the items required in the narrative.

**C. Materials to Be Submitted for First Meeting with Design Review Commission:** Not later than the first working day of the month, the DRC Meeting, the applicant must submit the items required by this subsection to the Director. If all required items are not submitted in a timely manner, the Director may postpone the Meeting to a later date.

1. All items required for the first meeting with staff with any changes; and
2. A narrative demonstrating all revisions to the project made as a result of the meeting with staff, and referencing the project's compliance with the applicable design guidelines, including images/exhibits, and design departures.
3. A refined site plan with major landscaped areas, parking, access, circulation, sidewalks and public/private amenities; and
4. Refined elevations; and
5. Perspective sketches (but not finished renderings); and
6. A conceptual model is strongly suggested (this can be a computer model).

**D. Materials To Be Submitted For The Optional Second Meeting With Design Review Commission:** At the time of the First Meeting with the DRC, the Commission shall determine whether the review of the project would benefit from an additional DRC Meeting to review project changes in response to the first DRC Meeting or is necessary based on all the circumstances. If the Commission decides that a subsequent Meeting will be beneficial or necessary, the Director or his/her designee shall schedule such meeting in accordance is § 17.09.325(C). Not later than fifteen (15) days before the subsequent Meeting, the applicant must submit the items required by this subsection to the Director. If all required items are not submitted two weeks prior to the scheduled meeting, the Director may postpone the subsequent Meeting to a later date.

1. Refined site plan and elevations for all sides of the proposal; and
2. Large scale drawings of entry, street level facade, site amenities; and
3. Samples of materials and colors, electronic copy of materials and colors, and physical samples of the materials will need to be brought to the meeting; and
4. Finished perspective rendering(s) for all sides; and
5. Elevations; and



6. A narrative demonstrating all revisions to the project made as a result of the previous Meeting.

**DEADLINE FOR SUBMITTALS:**

A complete application and applicable fee for design review under this Article shall be made on a form prescribed by, and filed with, the Director. The completed application must be filed not later than the first working day of the month and the Initial Meeting with the Commission will be held on the fourth Thursday of ~~that~~ the following month, unless otherwise directed by the Commission or Director and duly noticed. The Director shall schedule the Initial Meeting before the Commission upon receipt of the completed application in accordance with this subsection.

All supplemental information to be added to the application file must be received by the Planning Department no later than five (5) working days prior to the meeting date for this item. 17.09.305 TITLE & PURPOSE.

**PUBLIC HEARING NOTICE SIGN TO BE POSTED ON SUBJECT PROPERTY:**

The applicant is required to post a public hearing notice, provided by the Planning Department, on the property at a location specified by the Planning Department. This posting must be done one (1) week prior to the date of the Planning Commission meeting at which this item will be heard. An affidavit testifying where and when the notice was posted, by whom, and a picture of the notice posed on the property is also required and must be returned to the Planning Department.

**APPLICATION INFORMATION**

<b>PROPERTY OWNER:</b> CDA Hotel LLC, a Montana Limited Liability Company		
<b>MAILING ADDRESS:</b> 1450 Twin Lakes Avenue, Suite 201		
<b>CITY:</b> Bozeman	<b>STATE:</b> MT	<b>ZIP:</b> 59718
<b>PHONE:</b> 406-595-4560	<b>FAX:</b>	<b>EMAIL:</b> plange@providencedevco.com
<b>APPLICANT OR CONSULTANT:</b> Michael Nilson, The Richardson Design Partnership		<b>Architect</b> <b>STATUS:</b> ENGINEER <u>OTHER</u>
<b>MAILING ADDRESS:</b> 510 South 600 East		
<b>CITY:</b> Salt Lake City	<b>STATE:</b> Utah	<b>ZIP:</b> 84102
<b>PHONE:</b> 801-349-6543	<b>FAX:</b> 801-355-6880	<b>EMAIL:</b> mnilson@trdp.com

**FILING CAPACITY**

- Recorded property owner as to of 9/7/2023
- Purchasing (under contract) as of \_\_\_\_\_
- The Lessee/Renter as of \_\_\_\_\_
- Authorized agent of any of the foregoing, duly authorized in writing. *(Written authorization must be attached)*

**SITE INFORMATION:**

<b>PROPERTY LOCATION OR ADDRESS OF PROPERTY:</b> 602 & 612 E Sherman Avenue
--

EXISTING ZONING (CHECK ALL THAT APPLY): R-1 <input type="checkbox"/> R-3 <input type="checkbox"/> R-5 <input type="checkbox"/> R-8 <input type="checkbox"/> R-12 <input type="checkbox"/> R-1 <input type="checkbox"/> MH-8 <input type="checkbox"/> NC <input type="checkbox"/> C-17 <input type="checkbox"/> C-17L <input type="checkbox"/> DC <input checked="" type="checkbox"/> LM <input type="checkbox"/> M <input type="checkbox"/> NW <input type="checkbox"/>		
TAX PARCEL # C-1800-035-001-A C-1800-035-003-A	TOTAL NUMBER OF LOTS: 2	ADJACENT ZONING: DC
GROSS AREA/ACRES: .482 acres	CURRENT LAND USE: Vacant Land	ADJACENT LAND USE: Bank/ office/ condominiums
DESCRIPTION OF PROJECT/REASON FOR REQUEST: Construction of 6 story hotel		

**CERTIFICATION OF APPLICANT:**

I, Michael Nilson, being duly sworn, attests that he/she is the applicant of this  
*(Insert name of applicant)*

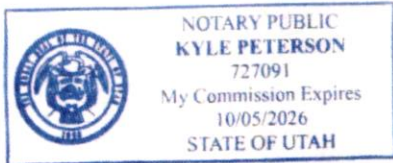
request and knows the contents thereof to be true to his/her knowledge.

Signed: *Michael Nilson*  
*(applicant)*

Notary to complete this section for applicant:

Subscribed and sworn to me before this 30 day of October, 2023.

Notary Public for Idaho Residing at: Salt Lake City



My commission expires: 10/05/2026

Signed: *[Signature]*  
*(notary)*

**CERTIFICATION OF PROPERTY OWNER(S) OF RECORD:**

I have read and consent to the filing of this application as the owner of record of the area being considered in this application.

Name: Parker Lange Telephone No.: 406-595-4560

Address: 1450 Twin Lakes Avenue, Suite 201 Bozeman MT 59718

Signed by Owner: *Parker Lange*

Notary to complete this section for all owners of record:

Subscribed and sworn to me before this 16 day of OCTOBER, 2023.

Notary Public for ~~Idaho~~ MONTANA Residing at: BELGRADE, MT

My commission expires: JULY 21, 27





CITY OF COEUR D'ALENE

PLANNING DEPARTMENT

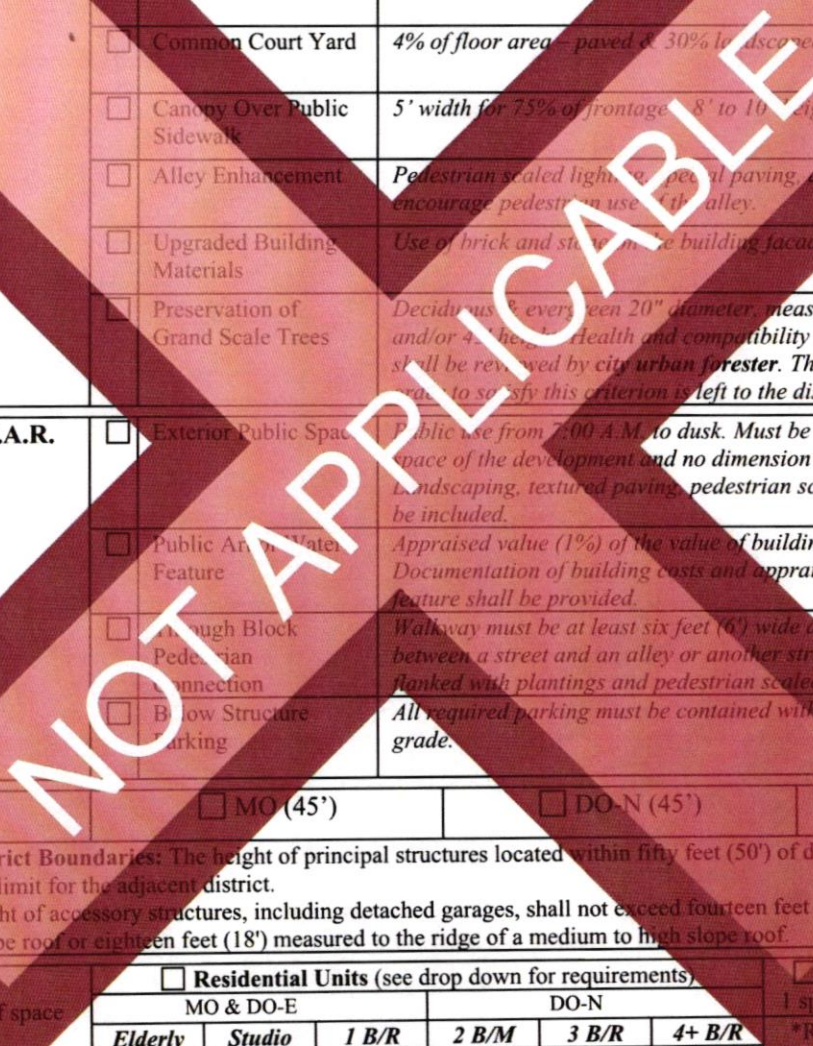
<b><u>Infill Overlay Districts Review Sheet</u></b> <b><u>(17.07.900)</u></b>	REVIEWED BY:	DATE:
--	--------------	-------



Signed: \_\_\_\_\_  
(notary)



<b>INFILL DESIGNATION</b>		<input type="checkbox"/> MO		<input type="checkbox"/> DO-N		<input type="checkbox"/> DO-E	
<b>DESIGN REVIEW REQUIRED</b>		<input type="checkbox"/> YES				<input type="checkbox"/> NO	
<b>ACTIVITY PERMITTED</b> (All 3) (DO-E&N)		<input type="checkbox"/> YES				<input type="checkbox"/> NO	
<b>F.A.R. MULTIPLIER =</b> (bonus items must be provided)  (F.A.R.+ bonus x SF of lot) <b>Grand Total of SF Allowed:</b>	<b>Overlay</b>	<b>Residential</b>		<b>Non-Residential</b>		<b>Combined Maximum</b>	
		<i>Basic</i>	<i>With Bonus</i>	<i>Basic</i>	<i>With Bonus</i>		
	MO	1.0	2.0	0.5	1.0	3.0	
	DO-N	1.0	2.0	0.3	0.9	2.9	
	DO-E	<b>0.5</b>	1.0	0.3	0.6	1.6	
<b>MINOR BONUS =</b> (0.2 each)	<b>F.A.R.</b>	<input type="checkbox"/>	Streetscape Features	<i>Seating, pedestrian lights, trees, or special paving.</i>			
		<input type="checkbox"/>	Common Court Yard	<i>4% of floor area - paved &amp; 30% landscaped</i>			
		<input type="checkbox"/>	Canopy Over Public Sidewalk	<i>5' width for 75% of frontage - 8' to 10' height</i>			
		<input type="checkbox"/>	Alley Enhancement	<i>Pedestrian scaled lighting, special paving, and rear entrances intended to encourage pedestrian use of the alley.</i>			
		<input type="checkbox"/>	Upgraded Building Materials	<i>Use of brick and stone on the building facades that face streets</i>			
		<input type="checkbox"/>	Preservation of Grand Scale Trees	<i>Deciduous &amp; every 20" diameter, measured at 4.5' above ground, and/or 4" dbh. Health and compatibility with the proposed development shall be reviewed by city urban forester. The number of trees preserved in order to satisfy this criterion is left to the discretionary review process.</i>			
<b>MAJOR BONUS =</b> (0.5 each)	<b>F.A.R.</b>	<input type="checkbox"/>	Exterior Public Space	<i>Public use from 7:00 A.M. to dusk. Must be 2% of the total interior floor space of the development and no dimension shall be less than 8'. Landscaping, textured paving, pedestrian scaled lighting, and seating must be included.</i>			
		<input type="checkbox"/>	Public Art or Water Feature	<i>Appraised value (1%) of the value of building construction costs. Documentation of building costs and appraised value of the art or water feature shall be provided.</i>			
		<input type="checkbox"/>	Through Block Pedestrian Connection	<i>Walkway must be at least six feet (6') wide and allow the public to walk between a street and an alley or another street. The walkway must be flanked with plantings and pedestrian scaled lighting.</i>			
		<input type="checkbox"/>	Below Structure Parking	<i>All required parking must be contained within a structure that is below grade.</i>			
<b>HEIGHT =</b>		<input type="checkbox"/> MO (45')		<input type="checkbox"/> DO-N (45')		<input type="checkbox"/> DO-E (35' res. or 38' com.)	
<b>Principal Structures Near District Boundaries:</b> The height of principal structures located within fifty feet (50') of districts having a lower height limit shall not exceed the height limit for the adjacent district.							
<b>Accessory Structures:</b> The height of accessory structures, including detached garages, shall not exceed fourteen feet (14') measured to the high point of a flat or the ridge of a low slope roof or eighteen feet (18') measured to the ridge of a medium to high slope roof.							
<b>PARKING</b> (see main sheet for breakdown of space requirements)	<input type="checkbox"/> Residential Units (see drop down for requirements)				<input checked="" type="checkbox"/> Commercial		<input type="checkbox"/> Shared
	MO & DO-E		DO-N		1 space per 330 SF		Per Plan Dir
	<i>Elderly</i>	<i>Studio</i>	<i>1 B/R</i>	<i>2 B/M</i>	<i>3 B/R</i>	<i>4+ B/R</i>	*Restaurant over 1000SF (1 space per 200 SF)
<b>Grand Total:</b>							*Different uses (20% reduction)
<b>MEETS DESIGN STANDARDS</b> <b>NOTE: If 3 level need "massing"</b> <b>(Base, middle, top)</b>		<input type="checkbox"/> YES				<input type="checkbox"/> NO	



**DESIGN GUIDELINES WORKSHEET FOR: C-17**

In order to approve the request, the Design Review Commission will need to consider any applicable design guidelines for the proposed project (Please fill out and submit with your application)



- *Curb Cuts*
- *Sidewalks Along Street Frontages*
- *Street Trees*
- *Grand Scale Trees.*
- *Walkways*
- *Residential/Parking Lot Screening*
- *Parking Lot Landscaping*
- *Lighting*
- *Screening of Service and Trash Areas*
- *Screening of Rooftop Equipment*
- *Entrance Visible from Street*
- *Windows Facing Street*
- *Treatment of Blank Walls*



**DESIGN GUIDELINES WORKSHEET FOR: East Design Guidelines (DO-E)**

**In order to approve the request, the Design Review Commission will need to consider any applicable design guidelines for the proposed project (Please fill out and submit with your application)**

- General Landscaping
- Screening of Parking Lots
- Screening of Trash/Service Areas
- Lighting Intensity
- Screening of Rooftop Mechanical Equipment
- Curb Cuts: Width and Spacing
- Parking Lot Landscape
- Location of Parking
- Grand Scale Trees
- Identity Elements
- Fences Next to Sidewalks
- Walls Next to Sidewalks
- Curbside Planting Strips
- Unique Historic Features
- Entrances
- Orientation to the Street
- Treatment of Blank Walls
- Integration of Signs with Architecture
- Creative/Individuality of Signs
- Minimum/Maximum Setbacks
- Roof Pitch
- Building Bulk and Spacing

NOT APPLICABLE

**DESIGN GUIDELINES WORKSHEET FOR: East Design Guidelines (DO-N)**

In order to approve the request, the Design Review Commission will need to consider any applicable design guidelines for the proposed project (Please fill out and submit with your application)



- General Landscaping
- Screening of Parking Lots
- Screening of Trash/Service Areas
- Lighting Intensity
- Screening of Rooftop Mechanical Equipment
- Width and Spacing of Curb Cuts
- Parking Lot Landscape
- Location of Parking
- Grand Scale Trees
- Identity Elements
- Fences Next to Sidewalks
- Walls Next to Sidewalks
- Curbside Planting Strips
- Unique Historic Features
- Entrances
- Orientation to the Street
- Massing: Base/middle/top
- Treatment of Blank Walls
- Accessory Buildings
- Integration of Signs with Architecture
- Creative/Individuality of Signs
- Setbacks Adjacent to Single Family
- Minimum/Maximum Setbacks




**DESIGN GUIDELINES WORKSHEET FOR: Downtown Core (DC)**

In order to approve the request, the Design Review Commission will need to consider any applicable design guidelines for the proposed project (Please fill out and submit with your application)

- *Location of Parking*
- *Screening of Parking Lots*
- *Parking Lot Landscaping*
- *Sidewalk Uses*
- *Width And Spacing of Curb Cuts*
- *Screening of Trash/Service Areas*
- *Lighting Intensity*
- *Gateways*
- *Maximum Setback*
- *Orientation To The Street*
- *Entrances*
- *Massing*
- *Ground Level Details*
- *Ground Floor Windows*
- *Weather Protection*
- *Treatment of Blank Walls*
- *Screening of Parking Structures*
- *Roof Edge*
- *Screening Of Rooftop Mechanical Equipment*
- *Unique Historic Features* | *Integration of Signs with Architecture*
- *Creativity/Individuality Of Signs*

**DESIGN GUIDELINES WORKSHEET FOR: MIDTOWN OVERLAY DISTRICT (MO)**

 In order to approve the request, the Design Review Commission will need to consider any applicable design guidelines for the proposed project (Please fill out and submit with your application)



- General Landscaping
- Screening of Parking Lots
- Screening of Trash/Service Areas
- Lighting Intensity
- Screening of Rooftop Mechanical Equipment
- Parking Lot Landscape
- Location of Parking
- Grand Scale Trees
- Identity Elements
- Fences Next to Sidewalks
- Walls Next to Sidewalks
- Curbside Planting Strips
- Unique Historic Features
- Entrances
- Orientation to the Street
- Treatment of Blank Walls
- Integration of Signs with Architecture
- Creativity/Individuality of Signs
- Sidewalk Uses
- Maximum Setback
- Ground Floor Windows
- Ground Level Details
- Roof Edge
- Width and Spacing of Curb Cuts
- Massing: Base/middle/top
- Accessory Buildings
- Setbacks Adjacent To Single Family

NOT APPLICABLE

Providence Development LLC

1300

City of Coeur d'Alene

Date Type Reference  
10/30/2023 Bill

Original Amt.  
1,006.00

Balance Due  
1,006.00

10/30/2023  
Discount  
Check Amount

Payment  
1,006.00  
1,006.00

Providence Operating CDA Sherman

1,006.00

Providence Development LLC

1300

City of Coeur d'Alene

Date Type Reference  
10/30/2023 Bill

Original Amt.  
1,006.00

Balance Due  
1,006.00

10/30/2023  
Discount  
Check Amount

Payment  
1,006.00  
1,006.00

PAYMENT  
RECORD

PAID

DEC 01 2023

CITY OF COEUR D ALENE

Providence Operating CDA Sherman

1,006.00



10503



105031



Rev 2/14

CITY OF COEUR D'ALENE

12/01/2023  
01:27 PM

Receipt No  
02834794

HOTEL DESIGN REVIEW - ANN PROVIDEN DEV LLC

Annexation & Zoning Fees		1,006.00
Total		1,006.00
Check	1300	1,006.00
Change		0.00
Balance :	\$\$0.00	

# AC Hotels Marriott Coeur d'Alene Development Plan: Narrative

December 1, 2023

City of Coeur d'Alene  
Planning Department  
710 E. Mullane Ave  
Coeur d'Alene, ID 83814

RE: Design Review Application Narrative  
AC Hotels Marriott Coeur d'Alene  
602-612 E. Sherman Ave.  
Coeur d'Alene, Idaho 83814

Ladies and Gentlemen:

On behalf of Providence Development (Applicant, or "Providence"), we are submitting this written narrative as part of the Design Review application for the development of a new hotel on a vacant 0.482 acre parcel of land located at 602 E. and 612 E. Sherman Ave.

The Richardson Design Partnership, LLC. (TRDP) has coordinated and substantially prepared this Design Review Application package to demonstrate compliance with the City of Coeur d'Alene (City) design guidelines and standards. TRDP also designed the hotel.

Project Description:

Providence has extensive experience developing best-in-class real estate projects that support and improve the communities in which they operate. Providence calls it: *"Helping communities reach their full potential"*. The proposed project is an upscale select service hotel and bar with approximately 131 rooms that caters to both short-term guests as well as bar patrons. The goal is to attract visitors and locals alike.

The building is designed with neutral gray and warm wood tones to allow the beauty of the natural environment of this area to shine. The predominantly brick finish façades blend with the many brick buildings nearby. Lighting is provided to light sidewalks around the site, while also preserving the dark skies of the region. Metal canopies protect and large planted containers line the sidewalk along Sherman Avenue to further enliven the pedestrian experience. The corner of the building hosts the hotel bar, that includes an outdoor patio, which will bring activity to the pedestrian-friendly streets. The hotel also provides a rooftop bar and lounge that can accommodate large gatherings of both hotel guests and locals, that will have stunning views of the nearby lake and natural environment from its outdoor deck. There is also a fitness center and large amount of parking in the underground parking structure that will have limited visibility to neighbors.

The proposed hotel location at the corner of Sherman Avenue and 6th Street is an ideal spot for a hotel of this size and caliber. The mid-rise hotel will have views of both Coeur d'Alene Lake and Lake Fernan from its upper floors, as well as access to McEuen Park, Tubbs Hill, and the nearby beaches of Coeur d'Alene Lake for recreational activities. The retail and restaurant options along Sherman Avenue will provide many amenities within walking distance (amounting to a walkable score of 85, or very walkable),

# AC Hotels Marriott Coeur d'Alene Development Plan: Narrative

but also quick access by car to the I-90 freeway down Sherman. There are also two bus stops around the corner on Lakeside Avenue. If hotel guests want more variety than provided by the hotel bar, some of the best restaurants in the City – including Fire Artisan Pizza and Crafted Tap House & Kitchen – are just steps away. Visitors will also appreciate having Coeur d'Alene Coffee, Vault Coffee, and Highlands Day Spa just down the street. This hotel is a perfect complement to the surrounding businesses, and it will support the needs of its neighbors.

Providence, in conjunction with The Richardson Design Partnership, looks forward to welcoming guests into a unique hotel that captures a contemporary look and feel, while adding to the pedestrian experience in the Downtown Core.

## **Project Overview of Proposed Development:**

Site Area:	20,993 S.F.	0.482 Acres
Total Building Area:	155,429 S.F.	
Building Footprint:	20,886 S.F.	99.4% Site Coverage
Building Height:	6 Stories	
Parking:	130 Stalls	
Guestrooms:	131 Guestrooms	

We have organized this narrative to generally coincide with The City of Coeur d'Alene Downtown Core (DC) Commercial Design Guidelines.

Thank you in advance for your time and careful consideration of this application.



# AC Hotels Marriott Coeur d'Alene Development Plan: Narrative

## Downtown Core Design Guidelines:

### **1. Location of Parking**

Parking for the project is located within the building footprint itself. Parking takes a portion of the main (Street) level and continues three stories underground. This project does not contain any exposed surface parking lots.

#### Required Parking Ratio (Residential & Hotels)

- Min 0.5 stalls per unit
- Max. 2 stalls per unit

#### Provided Parking Stalls:

- 131 Units
- 130 Stalls
- Ratio = 0.99 stall per Unit

### **2. Screening of Parking Lots**

Parking for the project is located within the building footprint itself (shown in red). The only exposed portions of the parking lot are the vehicular entrances off Sherman Avenue and the alley to the south of the property.

### **3. Parking Lot Landscaping**

Parking for the project is located within the building footprint itself (shown in red above) and therefore parking lot landscape is not required.

### **4. Sidewalk Uses**

**4.1 Amenity Zones:** Currently there are four trees planted along the property line facing Sherman. The trees are spaced 67', 54', and 68' respectively. Two of the trees have tree grates, the others have exposed soil. There is one tree along the property line facing 6th Street planted with a tree grate. This tree is located at the northwest corner of the property. It will be confirmed that these trees have a DBH of less than 20 inches. These trees will be removed during construction and replaced in the existing locations. This project includes creating new 5'x5' planting areas around the trees.

**4.2 Clear Walkway:** The existing sidewalk on Sherman Avenue from the back of curb to the property line is 14.8'. The distance from the new 5'x5' tree planting areas to the property line is approximately 8'-6". A 7'-0" wide clear pedestrian travel area will be maintained.

**4.3 Storefront Area:** An 18" wide area between the property line and the pedestrian travel area will be used for planting containers along Sherman Avenue.

# AC Hotels Marriott Coeur d'Alene Development Plan: Narrative

## **5. Width and Spacing of Curb Cuts**

Two curb cuts on Sherman and one curb cut on 6th street currently exist. All three of these existing curb cuts will be removed. The project only requires one 24' wide curb cut on Sherman; no curb cuts are being proposed on the Pedestrian-Oriented 6th Street. The sidewalk pattern and material will carry across the driveway. This project will not be sharing a driveway as it is not feasible.

## **6. Screening of Trash/Service Areas**

The trash area is located within the building footprint, off the alley on the southeast corner of the property. The trash area will be screened from view on all sides. The two sides and rear of the enclosure will match the exterior brick material. At the front of the enclosure will be an opaque decorative architectural gate. Loading and service areas do not face any residential areas. Loading and service areas are located within the parking garage.

## **7. Lighting Intensity**

**7.1 Building Lighting:** The majority of the exterior building lighting will be recessed lights in the roof canopies at the ground floor level to provide light to pedestrians, at the guestroom balcony roofs to provide light to the guests, and at the upper roof deck to highlight the building corner. Fully-shielded wall sconces will be added on either side of the main entry doors to highlight the entry.

**7.2 Street Lighting:** There is one existing single-arm tall streetlight at the corner of Sherman and 6<sup>th</sup> Street that will remain. There are two existing post streetlights along Sherman Avenue. One light will remain in its current location, and the other will be shifted to allow for the new curb cut into the parking structure. There are no existing streetlights along 6th Street.

## **8. Gateways**

The Corner of Sherman and 6th Street is not classified as a "Gateway" intersection in the Downtown Design Guidelines.

An existing public art installation exists on the southeast corner of Sherman and 6th Street.

## **9. Maximum Setback**

The street level façade along the Pedestrian-Oriented 6th Street is set up to the back of the sidewalk along the property line. A portion of the project on the corner of Sherman and 6th Street has a dining patio for the use of hotel guests but it has a base structure that extends out to the sidewalk.

# AC Hotels Marriott Coeur d'Alene Development Plan: Narrative

## **10. Orientation to the Street**

The proposed building is oriented to Sherman Avenue. The building façade along Sherman incorporates numerous windows as well as an entrance canopy and signage. The façade along 6th Street incorporates windows. The primary building entrance faces Sherman and is centered in the building façade. An outdoor patio at the ground level activates the street corner at Sherman Avenue and 6th Street.

## **11. Entrances**

The main building entrance is centered on the façade along Sherman Avenue and welcomes pedestrians with an overhanging canopy as well as a recess in the main building wall. Both the canopy and the recess provide added weather protection for pedestrians. These features, along with clear signage, help identify this visually prominent entrance.

## **12. Massing**

**12.1 Top:** The top section of the building is distinguished by overhanging roofs, an open roof deck with trellis, and additional windows. The main material is a dark metal panel, with accent metal panels.

**12.2 Middle:** The middle section of the building has a regular pattern of guestroom windows surrounded by dark and light color brick veneer. Also, there are some dark and accent metal panels to connect the base to the top.

**12.3 Base:** The base of the building features a large amount of storefront glazing and canopies to define the ground level. The finish is a combination of light grey brick, darker composite panels accented with horizontal wood siding with a decorative concrete plinth.

**12.4 Building Bulk:** The base of the building aligns with the property lines of the lot, but steps back above the ground floor level to allow for the required 10-foot setback over 45 feet above grade. The only parts of the building that extend past these setbacks are roof overhangs and balconies. The only part of the building that is taller than 75 feet is the elevator penthouse, which is much smaller than the 8000 SF Tower Floor Size restriction at 176 SF and is over the minimum Tower Separation of 50 feet noted in the Site Performance Standards. At approximately 77 feet tall, the overall building height is well below the maximum 200 ft building height.

**12.5 City Block Elevations:** Sherman Avenue has a mix of low-and mid-rise buildings, which align well with the scale of the plinth of the proposed hotel. The overall mass of the building helps transition from these shorter structures to the high-rise residential buildings on Front Avenue.

# AC Hotels Marriott Coeur d'Alene Development Plan: Narrative

## **13. Ground Level Details**

The ground floor of the building has the most amount of character to provide visual interest to pedestrians, including:

- Pedestrian-scaled signs to identify the building entry.
- Seasonal planting in multiple planters against the building along Sherman Avenue.
- Metal canopies above the ground floor storefront windows.
- Accent wall sconces on either side of the main entrance.
- A decorative concrete plinth to ground the building.

The ground level also features an elevated patio at the corner of Sherman and 6th Street to add a further level of detail in this area.

## **14. Ground Floor Windows**

The building has been designed with many storefront windows along Sherman Avenue and 6th Street that will have clear vision glass into the Lobby, Bar/Lounge, Conference Room, and Corridor spaces (unblocked by shelving). Sherman Avenue has 45% window and glazed door area in the “window zone” of the façade. 6th Street has 26% window area in the “window zone” of the façade. All ground-floor windows will have a minimum of 60% transparency.

## **15. Weather Protection**

The building is designed with 5-foot-deep canopies around nearly the entire length of the Sherman Avenue and 6th Street façades for weather protection. The exception is at the main entry, which has a shorter canopy at 3.5 feet in depth. However, a recessed entry provides additional protection. This canopy is also 16 feet high, to accentuate the entrance to the building. The main canopies maintain a consistent level height around the building, but due to the sloping grades, the height of the canopy varies. The minimum height above grade is approximately 10 feet. The canopy will have a metal frame finish, with a wood plank soffit. These canopies will also have recessed downlights to provide lighting under the opaque covering.

## **16. Treatment of Blank Walls**

The street-facing walls of the building are mostly broken up by windows and doors, but there are additional architectural features that break up the impact of the walls, including:

1. A concrete plinth that varies in height depending on the grade change (from 1'-2" up to 6'-0").
2. A change in brick materials above the ground floor level, acting as a “belt course” for the building.
3. Recesses in the façade at least 2'-0" in depth.
4. Roof overhangs/canopies at the ground floor level and upper roof level that vary from 3'-0" to 5'-0" in depth.

Additional features at the pedestrian level include contrasting wall material and vegetated planter boxes.

## **17. Screening of Parking Structures**

# AC Hotels Marriott Coeur d'Alene Development Plan: Narrative

The Parking Structure is incorporated into the main hotel building, within the building footprint (shown in red), with a portion of the Main Floor (ground level) allocated to parking, as well as three underground levels. There isn't a separate parking structure to be screened. The Main Floor parking is integrated into the "plinth" on the non-street-facing façades.

## **18. Roof Edge**

The building design doesn't include any pitched roofs. The typical roofline of the building includes a 3-foot overhanging cornice to create a prominent edge against the sky. At recessed wall locations, this overhang extends 5.5 feet past the wall face, creating an even more dramatic cornice. Additionally, the building features accent tower elements of varying heights and a roof deck with a large trellis to add increased interest at the roof edge.

## **19. Screening of Rooftop Mechanical Equipment**

The proposed building is designed with extended parapets to screen a majority of the rooftop equipment. The only rooftop mechanical equipment that extends above the main parapet is the Elevator Penthouse, which will be surrounded by a framed wall and finished in the same dark metal panels as part of the main building façade.

## **20. Unique Historical Features**

The existing site is a lawn-covered dog park, so the project doesn't include any renovation or redevelopment. As a new construction project, the proposed building relates to the surrounding context through:

- The use of brick as a predominant exterior finish.
- The massing of the building with a base, middle, and top.
- The scale of the building as a steppingstone between the smaller buildings along Sherman Avenue and the high-rise residential Parkside Building.
- The design of the building as a contemporary structure that relates to the primarily modern surrounding architecture.

## **21. Integration of Signs with Architecture**

The two main building signs are placed on the vertical-wood-siding-finished vertical towers of the building for wayfinding of automobile traffic, in lieu of pylon signs. These signs are 188 SF and 36 SF, respectively. Additionally, channel letter signs are located above the ground floor canopies to designate the main entrance and the parking entrances. These signs are 42 SF for the main entrance and 14 SF (each) for the two parking entrances. Lastly, there are two placard signs on either side of the main entrance doors for pedestrian wayfinding. These two signs are 4 SF each. The total building signage area is 302 SF, which is less than the maximum 603 SF allowed based on the frontage.



# AC Hotels Marriott Coeur d'Alene Development Plan: Narrative

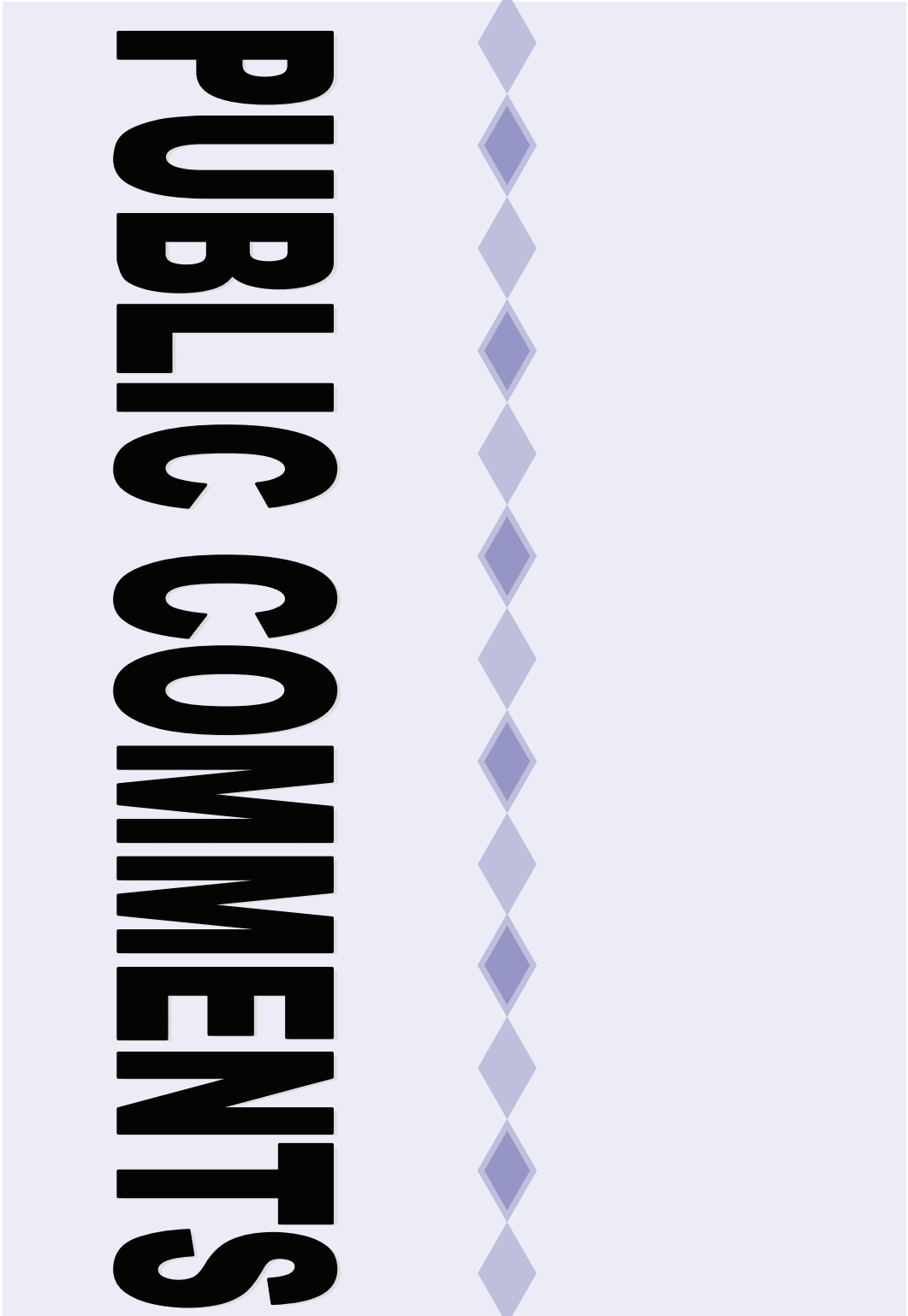
## **22. Creativity/Individuality of Signs**

The signage for the building was selected from the Brand's standard signage options. Their designs are highly graphic for brand identity, but also offer a variety of installations and styles including typical wall signs, channel letter wall signs, freestanding channel letter entry signs, as well as smaller pedestrian-oriented placard signs at the entry doors. The freestanding channel letter sign at the entry canopy is supported by brackets and directs pedestrians to the building entry.

We are happy to answer any questions you may have during your review. Thank you in advance for your time and careful consideration.



# **PUBLIC COMMENTS**



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**From:** Cheryl Stransky  
**To:** [CLARK, TRACI](#)  
**Subject:** DR-1-24 AA Marriott Hotel support  
**Date:** Saturday, May 25, 2024 3:03:37 PM

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CAUTION: This email originated from outside your organization. Exercise caution when opening attachments or clicking links, especially from unknown senders.

I support the building of the Marriott hotel on Sherman Street in CDA. The Marriott organization designed an appropriate and beautiful hotel. We live in a resort/recreation community and this will create more opportunity for success in our community.. The location is perfect and it meets all the city's building code requirements.

Respectfully,

Cheryl Stransky  
5983 N. Colfax street  
Dalton Gardens, Idaho 83815  
[castransky@gmail.com](mailto:castransky@gmail.com)

**From:** Kevin Howard  
**To:** [CLARK, TRACI](#)  
**Subject:** RE: PUBLIC NOTICE HEARING DR-1-24AA CITY COUNCIL MEETING TUESDAY JUNE 4, 2024  
**Date:** Monday, May 20, 2024 6:13:06 AM  
**Attachments:** [image001.png](#)

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CAUTION: This email originated from outside your organization. Exercise caution when opening attachments or clicking links, especially from unknown senders.

Worley Highway District is neutral t the request.

**KEVIN J. HOWARD**  
**DIRECTOR OF HIGHWAYS**  
**WORLEY HIGHWAY DISTRICT**  
**OFFICE: 208-664-0483**

---

**From:** CLARK, TRACI <TCLARK@cdaid.org>  
**Sent:** Friday, May 17, 2024 9:44 AM  
**To:** CLARK, TRACI <TCLARK@cdaid.org>  
**Subject:** PUBLIC NOTICE HEARING DR-1-24AA CITY COUNCIL MEETING TUESDAY JUNE 4, 2024

Greetings,

Attached is a copy of the public hearing notice for the next **City Council Meeting Tuesday June 4, 2024.**

If you have any comments, please let me know.

*Traci Clark*  
*Planning Department, City of Coeur d'Alene*  
*Administrative Assistant*

208.769-2240  
[tclark@cdaid.org](mailto:tclark@cdaid.org)





**From:** Cheyenne Fish  
**To:** [CLARK, TRACI](#)  
**Subject:** Please no new Marriott  
**Date:** Wednesday, April 17, 2024 8:18:25 PM

---

**CAUTION:** This email originated from outside your organization. Exercise caution when opening attachments or clicking links, especially from unknown senders.

I hope this isn't finding you too late. Please don't build the Marriott in downtown Coeur d'Alene. I truly think you'd be doing the city a disservice. Let us remain to our big open skies and nature. Don't rob us Of views and locals and homegrown out. We are barely hanging on as it is.



**From:** Joan Woodard  
**To:** [BOSLEY, CHRIS](#); [CLARK, TRACI](#)  
**Subject:** RE: 602 & 612 E. Sherman Avenue  
**Date:** Monday, February 5, 2024 10:43:44 AM

---

**CAUTION: This email originated from outside your organization. Exercise caution when opening attachments or clicking links, especially from unknown senders.**

Chris:

Thank you for this information, it was very helpful.

I have another question, as it wasn't covered in the applicant's submittal and only alluded to in the staff report. Regarding 6<sup>th</sup> street, the current sidewalk depth for this property is approximately 7 feet. This doesn't match the depth of the sidewalk between this property and Front Street or the depth of the sidewalk across 6<sup>th</sup> street beside the old Masonic building. A more consistent depth could be achieved by moving the current curb out into 6<sup>th</sup> street, similar to the situation at Parkside. Is this what you mean by stating that the curb cuts will be removed, and the sidewalk will be made ADA compliant? What would be the resulting width of the 6<sup>th</sup> street sidewalk? Would this still allow for cars to park on the 6<sup>th</sup> street heading north, as they do now?

Also, there is nothing to indicate whether additional street trees and street lighting will be required to match what was required for Parkside and what exists along the Masonic building. These measures would help with the "pedestrian friendly" appearance of 6<sup>th</sup> and soften the impact of the wall of the new hotel.

Thank you.

Joan C. Woodard  
707-479-5090

---

**From:** BOSLEY, CHRIS <CBOSLEY@cdaid.org>  
**Sent:** Thursday, February 1, 2024 8:10 AM  
**To:** Joan Woodard <joancwoodard@outlook.com>; CLARK, TRACI <TCLARK@cdaid.org>  
**Subject:** RE: 602 & 612 E. Sherman Avenue

Joan,

Thank you for reaching out to the City with your concerns about the proposed development on Sherman Avenue. We try to treat proposed high-density developments equally and fairly and therefore are requiring this developer to provide a similar traffic study to that which was completed for the Thomas George 18-story development at 3<sup>rd</sup> Street and Front Avenue. The traffic study will not only study impacts and potential mitigation measures for traffic generated by the proposed development, but will also consider a pedestrian safety analysis and propose mitigation measures if warranted. It is worth mentioning that approval of their building permit is not contingent on the traffic study as the current zoning allows for the

proposed use up to a maximum of 220 feet in height. However, we require traffic studies on high-density developments to identify any traffic signal timing changes or other improvements/restrictions warranted as a result of the studies. The City generally requires traffic studies to meet the Associated Highway District standards, which begin with a Trip Generation and Distribution Letter to assess potential impacts and any further investigation needed. With this project, as with the Thomas George development, we are also requiring a pedestrian safety study to assess potential conflicts between vehicle ingress/egress and pedestrian traffic on Sherman Avenue and the alley. Additionally, we have also asked the developer to study the impacts to the alley. It is very likely that the alley will become a one-way eastbound alley like many others downtown. To address your concerns about seasonal fluctuations in traffic, we have access to historical traffic counts on the Sherman Avenue corridor. In addition, we have access to the Kootenai Metropolitan Planning Organization traffic model that uses current traffic counts to predict increases 20 years into the future, based on land uses and future developments. We generally do not present the findings of the traffic study to the public, but the information is provided to City Council and the Planning Commission for decision making. This project could go back to the Design Review Committee if changes as a result of necessary mitigation measures are significant. We hope you find this information helpful and answers any questions you have.

Thanks,

**Chris Bosley, PE** | City Engineer | [City of Coeur d'Alene](#)  
[cbosley@cdaid.org](mailto:cbosley@cdaid.org) | T 208-769-2216 | F 208-769-2284

---

**From:** Joan Woodard <[joancwoodard@outlook.com](mailto:joancwoodard@outlook.com)>  
**Sent:** Friday, January 26, 2024 2:00 PM  
**To:** BOSLEY, CHRIS <[cbosley@cdaid.org](mailto:cbosley@cdaid.org)>; CLARK, TRACI <[tclark@cdaid.org](mailto:tclark@cdaid.org)>  
**Subject:** 602 & 612 E. Sherman Avenue

**CAUTION: This email originated from outside your organization. Exercise caution when opening attachments or clicking links, especially from unknown senders.**

Chris and Traci:

I wasn't able to attend yesterday's meeting, but I've attached a letter with my concerns about the Traffic Study that I believe you all made a condition of approval for the hotel.

I look forward to your response.

Joan C. Woodard  
707-479-5090

**From:** [BOSLEY, CHRIS](#)  
**To:** [Joan Woodard](#); [CLARK, TRACI](#)  
**Subject:** RE: 602 & 612 E. Sherman Avenue  
**Date:** Tuesday, February 6, 2024 2:42:47 PM

---

Joan,

By requiring the development to move the curb to accommodate a wider sidewalk, it would eliminate on-street parking. I do not plan to require that. The curb cuts to be removed are old driveway approaches that exist on 6<sup>th</sup> Street and Front Avenue. They will be replaced with standard curb and gutter since they will no longer be used.

Staff is looking into your questions regarding street trees and lights.

Thanks,

Chris

---

**From:** Joan Woodard <joancwoodard@outlook.com>  
**Sent:** Monday, February 5, 2024 10:44 AM  
**To:** BOSLEY, CHRIS <CBOSLEY@cdaid.org>; CLARK, TRACI <TCLARK@cdaid.org>  
**Subject:** RE: 602 & 612 E. Sherman Avenue

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Chris:

Thank you for this information, it was very helpful.

I have another question, as it wasn't covered in the applicant's submittal and only alluded to in the staff report. Regarding 6<sup>th</sup> street, the current sidewalk depth for this property is approximately 7 feet. This doesn't match the depth of the sidewalk between this property and Front Street or the depth of the sidewalk across 6<sup>th</sup> street beside the old Masonic building. A more consistent depth could be achieved by moving the current curb out into 6<sup>th</sup> street, similar to the situation at Parkside. Is this what you mean by stating that the curb cuts will be removed, and the sidewalk will be made ADA compliant? What would be the resulting width of the 6<sup>th</sup> street sidewalk? Would this still allow for cars to park on the 6<sup>th</sup> street heading north, as they do now?

Also, there is nothing to indicate whether additional street trees and street lighting will be required to match what was required for Parkside and what exists along the Masonic building. These measures would help with the "pedestrian friendly" appearance of 6<sup>th</sup> and soften the impact of the wall of the new hotel.

Thank you.

Joan C. Woodard  
707-479-5090

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**From:** BOSLEY, CHRIS <[CBOSLEY@cdaid.org](mailto:CBOSLEY@cdaid.org)>  
**Sent:** Thursday, February 1, 2024 8:10 AM  
**To:** Joan Woodard <[joancwoodard@outlook.com](mailto:joancwoodard@outlook.com)>; CLARK, TRACI <[TCLARK@cdaid.org](mailto:TCLARK@cdaid.org)>  
**Subject:** RE: 602 & 612 E. Sherman Avenue

Joan,

Thank you for reaching out to the City with your concerns about the proposed development on Sherman Avenue. We try to treat proposed high-density developments equally and fairly and therefore are requiring this developer to provide a similar traffic study to that which was completed for the Thomas George 18-story development at 3<sup>rd</sup> Street and Front Avenue. The traffic study will not only study impacts and potential mitigation measures for traffic generated by the proposed development, but will also consider a pedestrian safety analysis and propose mitigation measures if warranted. It is worth mentioning that approval of their building permit is not contingent on the traffic study as the current zoning allows for the proposed use up to a maximum of 220 feet in height. However, we require traffic studies on high-density developments to identify any traffic signal timing changes or other improvements/restrictions warranted as a result of the studies. The City generally requires traffic studies to meet the Associated Highway District standards, which begin with a Trip Generation and Distribution Letter to assess potential impacts and any further investigation needed. With this project, as with the Thomas George development, we are also requiring a pedestrian safety study to assess potential conflicts between vehicle ingress/egress and pedestrian traffic on Sherman Avenue and the alley. Additionally, we have also asked the developer to study the impacts to the alley. It is very likely that the alley will become a one-way eastbound alley like many others downtown. To address your concerns about seasonal fluctuations in traffic, we have access to historical traffic counts on the Sherman Avenue corridor. In addition, we have access to the Kootenai Metropolitan Planning Organization traffic model that uses current traffic counts to predict increases 20 years into the future, based on land uses and future developments. We generally do not present the findings of the traffic study to the public, but the information is provided to City Council and the Planning Commission for decision making. This project could go back to the Design Review Committee if changes as a result of necessary mitigation measures are significant. We hope you find this information helpful and answers any questions you have.

Thanks,

**Chris Bosley, PE** | City Engineer | [City of Coeur d'Alene](http://CityofCoeurdAlene.com)  
[cbosley@cdaid.org](mailto:cbosley@cdaid.org) | T 208-769-2216 | F 208-769-2284

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**From:** Joan Woodard <[joancwoodard@outlook.com](mailto:joancwoodard@outlook.com)>  
**Sent:** Friday, January 26, 2024 2:00 PM  
**To:** BOSLEY, CHRIS <[cbosley@cdaid.org](mailto:cbosley@cdaid.org)>; CLARK, TRACI <[tclark@cdaid.org](mailto:tclark@cdaid.org)>

**Subject:** 602 & 612 E. Sherman Avenue

**CAUTION:** This email originated from outside your organization. Exercise caution when opening attachments or clicking links, especially from unknown senders.

Chris and Traci:

I wasn't able to attend yesterday's meeting, but I've attached a letter with my concerns about the Traffic Study that I believe you all made a condition of approval for the hotel.

I look forward to your response.

Joan C. Woodard  
707-479-5090



**From:** C LEININGER  
**To:** [CLARK, TRACI](#)  
**Cc:** [C LEININGER](#)  
**Subject:** Objection: Proposed Marriott Hotel  
**Date:** Monday, January 29, 2024 2:08:22 PM

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To whom it may concern:

I apologize for my late response but I have been working out of county and just recently returned and received the notice. This is the FIRST time I have been made aware of the intention to build a hotel at this location. I also noted the notice was dated only two weeks before the hearing date not allowing sufficient time to respond or research the situation. This appears to be a design review but, this is the FIRST time I have been given notice of the intent to build this hotel or notification of public hearings before this project was approved.

I am objecting to the building of the Hotel at this location for numerous reasons.

I own a condo in Parkside on the side that would be next to this hotel. This will significantly impact me personally. I purchased my condo for the location, the views, the privacy, the beautiful and calming qualities of Parkside, the investment considerations, and the area.

You are taking a community known for its peaceful, tranquil, and beautiful setting and turning it into something quite opposite.

1. the notice said there would be 3 story underground parking: This hotel and parking is immediately adjacent to the Parkside building/condo's at 601 E. Front Ave.. The Hotel as well as the underground parking I believe would compromise the structure and foundation of 601 E. Front Ave. I question how this could be done safely without damage to 601 E. Front Ave.

2. Significantly This Hotel will DECREASE the property value of the residence at 601 E. Front Ave.

3. It will destroy the QUIET ENJOYMENT of my/our homes. It will block the beautiful views the residents of the Condos at 601 E. Front Ave. enjoy. One of the main reasons residents purchased the condo's. Of course, it will be a nightmare during the construction, which presumable will take over a year. The noise, dirt, vibration, etc. will make living in our homes untenable.

4. If the hotel is built, there will be substantial negative impact to the residence of the 601 E. front Ave and surrounding residence, the environment, the community, the aesthetic quality of the area.

a. The noise that accompanies a Hotel is well known. First there will be the guest of the hotel. This is supposed to be a 6 story hotel with 3 stories of underground parking. Obviously, this hotel will be for many many people. Just normal noise from guest (adults and children) will be ongoing. Add pets and the situation is even worse. Everyone who has stayed in a hotel or motel understands how disturbing and noisy a Hotel's air conditioning units, garbage collection, heating units, pools, parties, events, etc. will be. All of which will detrimentally impact the property owners at 601 E. Front Ave., and surrounding area, quiet enjoyment of their property as well as their property value.

5. I believe adding a hotel of this size will significantly impact the environment as well, with the added traffic and noise, among many other negative consequences.

I strongly oppose the building the Marriott Hotel at this location. If this is not who I should be lodging my objection with, please tell me who I should be contacting.

Thank you,  
Cheryl L. Leininger  
Owner of 601 E. Front Ave. Unit # 1601.

**Joan C Woodard  
609 E. Sherman Ave Unit 401  
Coeur d'Alene, ID 83814  
707-479-5090**

January 26, 2024

Subject: 6 Story Marriott Hotel at 602 & 612 Sherman

Dear City Staff:

While I was unable to attend yesterday's Design Review meeting, I was advised that the members of the commission voted in favor of the application, subject to some conditions that City Staff included in the staff analysis. One was a traffic study including pedestrian safety.

I have some experience with traffic studies, having been required to provide them on many of the projects I developed around the country over the past 40 years. I'm interested in knowing if the city has standards for these studies. I see where the developers of this project have indicated to the press that they plan to be under construction this summer. I find that hard to believe if a thorough traffic study is to be required.

In this situation, some of the elements I'd expect to see in a traffic study would include an analysis of existing congestion, the inclusion of forecasted traffic from other projects previously achieving entitlements but not yet built, expected traffic growth with these things as given....only then adding in the impact of this proposed development. I looked at the existing traffic counts on the City's website and saw that there don't appear to be any recent traffic counts in the downtown area. I would expect traffic counts would need to be brought current, with estimates for weekday morning, afternoon and evening, and then seasonal and weekend counts. As Coeur d'Alene is like two different places in the summer versus the winter, this would indicate that new traffic counts need to be done in August, which I suspect is peak seasonal traffic generation. New winter counts would also need to be done.

Analysis of the design elements of the hotel would also be included in the study. This would include turning movements, traffic controls, and facility geometry (in particular the use of the alley as an exit for trucks making delivery to the hotel after entering on Sherman, which I understand is the method the project architect indicated will work for all deliveries which is highly suspect). All of the portions of the hotel would also be studied for impact, not just the rooms, but the hotel staff count, seating counts for the lobby level bar, restaurant and social spaces, seating counts for the rooftop bar, social spaces and patio, and some estimates for visitors coming to spend time with hotel guests in guest rooms.

All of this data is important before an adequate trip generation study and loss of service analysis can be completed. Once presented to staff, if this study shows, as I suspect it will, that the level of service in the downtown area will be greatly impacted, what are the staff's requirements for providing this information

to the public and to our elected officials? Traffic studies can also propose appropriate mitigation measures, such as changes to the design of the project to reduce traffic and pedestrian impacts. If such changes were required for mitigation, would the project come back to Design Review or is the staff the only arbiter?

I trust that the city will require a thorough, professional traffic study with updated traffic counts that will clearly define the impacts on traffic and parking that will be experienced by Sherman, 6<sup>th</sup> Street, 7<sup>th</sup> Street, Front Street, and the neighborhoods surrounding the downtown.

Thank you.

Sincerely,

Joan Woodard

**From:** Greg Lapin  
**To:** [CLARK, TRACI](#)  
**Subject:** Hotel feedback  
**Date:** Friday, January 26, 2024 2:51:49 PM  
**Attachments:** [image001.png](#)  
[image001.png](#)

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Hello, you're welcome & thank you for reading my feedback. I just don't see the need for a Marriott Hotel Downtown when there's already a Hotel (The CDA Resort) in Downtown CDA & when there's already 2 Marriott Hotels (The Marriott Spring Hill Suites Hotel just off Northwest Blvd & the new Marriott Hotel where The Outback Steakhouse used to be on Nothwest Blvd) in CDA.

On Fri, Jan 26, 2024, 10:46 AM CLARK, TRACI <[TCLARK@cdaid.org](mailto:TCLARK@cdaid.org)> wrote:

Greg,

I have received your comment.

Thank you,

*Traci Clark*

*Planning Department, City of Coeur d'Alene*

*Administrative Assistant*

208.769-2240

[tclark@cdaid.org](mailto:tclark@cdaid.org)



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**From:** Greg Lapin <[greg200121@gmail.com](mailto:greg200121@gmail.com)>  
**Sent:** Friday, January 26, 2024 10:42 AM  
**To:** CLARK, TRACI <[tclark@cdaid.org](mailto:tclark@cdaid.org)>

**Subject:** Hotel feedback

CAUTION: This email originated from outside your organization. Exercise caution when opening attachments or clicking links, especially from unknown senders.

Hello, the developer from Bozo (Bozeman), Montana that's wanting to build the 6-story Marriott Hotel at 6th St & Sherman Ave in Downtown CDA is just being an idiot. This is stupid, not necessary & just a waste of money that could be put towards better use as there's already a Hotel (The CDA Resort) in Downtown CDA at 2nd St (Just 4 blocks from the proposed building of The Marriott Hotel) & Front. The developer is just being an idiot as they're (the developer) going to wind up making CDA look like Spokane or NYC if they're going at that rate. Thank you for reading.

**Cyndy Donato**  
**609 E. Sherman Ave Unit 202**  
**Coeur d'Alene, ID 83814**  
**314-277-3027**

January 25, 2024

Subject: 602 & 612 Sherman Development

Dear City Staff and Members of the Design Review Commission:

I am writing because I want to share my comments regarding the contemplated development of 602 & 612 Sherman. I am new to the area, being recruited from the Midwest to Coeur d'Alene. In August of 2023, I purchased my unit in 609 Sherman. I am a member of the CdAEDC (Jobs Plus) and the Woman's Gift Alliance and an executive at Kootenai Health. I intend to make my home here for years to come and as such quickly invested in being a downtown resident.

The segment of Sherman I choose is not in the "hubbub" of downtown and yet is walkable to everywhere. I have views of Tubbs Hill and the Lake, am on the parade route and loved the green space now being contemplated for construction. Had I known that this project was contemplated, I would have NEVER bought in 609. I don't know that you care about how this project will affect a relative "outsider" much less one resident as you contemplate the broader revenue this construction will potentially bring to CdA. Still, the approach contemplated does not support several design standards the city had adopted and for good reason!

1. The massing and scale of this project violate the intent of the Coeur d'Alene Downtown Development Regulations and Design Standards adopted as of July 5, 2006.
  - a. "To encourage private and public investment, attract shoppers and visitors, and appeal to existing and new residents". This project will not appeal to the existing residents across the street or within the same block. The architect and developer for this project are both from out of state, so perhaps they didn't think it would help to understand the needs of the neighborhood before they began planning this project. To my knowledge, there has been no attempt to meet with any local residents to discuss these factors before beginning their work.
  - b. "Preserve views of Tubbs Hill and other distant landforms, view corridors through upper-level setbacks along with tower size and spacing". Concerning 609 Sherman, directly across the street, all of our existing views to McKuen Park, Tubbs Hill, hills to the east, and much of Lake Coeur d'Alene are obliterated by the massing and scale of this building. While the minimal setback above 45 feet is being observed that accomplishes very little in terms of this view standard. It seems the Parkside project had a similar issue when it was proposed, which is why the setbacks from 6<sup>th</sup> and 7<sup>th</sup> Street are so large. By making the building taller and narrower, they were able to preserve views for the surrounding area and create more visual interest at the lower elevations. Additionally,



for many months of the year when the sun is low, this proposed building will block the sun from reaching the front of our building and specifically my unit far beyond the amount of time the existing Parkside project does. As others have suggested, a sun-hours and shadow study would normally be required to analyze the impact of a proposed structure on existing buildings and neighborhoods.

- c. “Respect the small-town scale and character”. Because this site is so large and has the longest frontage on Sherman as any other property on Sherman in the downtown area, building from property line to property line creates a structure that is out of scale with “small town” and is not in keeping with the existing structures on Sherman. While I am not opposed to the project, the location needs to be reconsidered in order to preserve the continuity of our downtown. If I have wanted this kind of a neighborhood, I could have purchased in Riverstone. The charm of downtown, with views and access to local shops, restaurants and museums is what makes downtown special. This project is not in keeping with this standard.
  - d. There has been no attempt by the applicant to show their project as it more directly relates to 609 Sherman other than a height comparison. The approval of this hotel will most significantly impact our building, our real estate value, (and therefore our assessed value for property tax) and our enjoyment in living downtown as it turns into a smaller version of Portland or Seattle.
2. The practical and operational issues must have much more focus to reduce the impacts to neighboring buildings, pedestrians, and functions that are important to downtown Coeur d’Alene.
    - a. The location of the driveway entrance on Sherman will create backup issues in both directions as there is no left-turn lane from Sherman into the parking driveway and it is just a short distance away from the stop light at 7<sup>th</sup>. Cars routinely stack up on eastbound Sherman for the stoplight at 7<sup>th</sup>, blocking access. During the winter, berms are frequently blocking the middle of Sherman so there would be no ability to safely access the entrance for cars entering from the east.
    - b. During the many parades and other events that occur on Sherman, access is typically blocked at 7<sup>th</sup> Street (and further west) to allow Sherman to be a pedestrian street with no cars. Access will have to occur from the alley between 7<sup>th</sup> and 6<sup>th</sup> during these times. However, the alley is basically a single-lane alley with no available increase in width to accommodate two-way traffic. The narrowness of this alley will also make it very difficult for passenger vehicles to manage a turn in or out without pulling into the alley, creating an unsafe situation for all.
    - c. Does the City contemplate no longer closing this section of Sherman for the many cultural events and parades that make Coeur d’Alene attractive to residents and visitors? This would be such a tragedy and surely impact business and residents in the immediate area.
    - d. This alley is also the only location for at least 4 large dumpsters associated with Parkside that are emptied multiple times per week. It is also the location for delivery vehicles (Fedex, UPS, Amazon, etc.) as well as delivery trucks making deliveries of food, liquor, etc. to the restaurant in Parkside. If the subject property intends to use the alley as well for garbage and deliveries, this could/will create a bottleneck for the smooth operation

of all. It also will make it nearly impossible for hotel guests to utilize the alley entrance and exit.

- e. Another concern will be firefighting. The heavy utilization of this alley will require the fire department to fight a Parkside fire predominantly from Front, 7<sup>th</sup>, and 6<sup>th</sup>, as the alley will be too greatly impacted by utilization and the mass at the western edge of the proposed building and the alley to enable a large truck to make the turn eastbound. It will also mean that the proposed hotel will be addressed by the fire department only from Sherman and 6<sup>th</sup> Street. Does the Fire Department have the equipment to deal with these distances and heights?
- f. Finally, as the building is proposed, there is no acceptable construction staging area. From an operational standpoint, the alley cannot be used as it will impact the permitted use of the alley by Parkside. Staging cannot close off Sherman either, which leaves 6<sup>th</sup> Street, which will severely impact Parkside and the pedestrian focus of 6<sup>th</sup> Street. I just don't see how this project can be built as conceived based on these logistical factors.
- g. The project contemplates that Coeur d'Alene can economically support an additional hotel of such scale. An economic study should be conducted to determine the impact on the Coeur d'Alene Resort which is the heartbeat of our city and downtown.

### 3. Specific Design Questions.

- a. In the applicants' materials, they describe the signage for the property, which seems fine based on the narrative, however, the renderings they presented show two additional signs on the top of the building. One on Sherman and the other on the East side of the building. If these signs are to be considered, they should be included in the calculations for allowable signage and the question of whether they are to be lighted or not should be addressed. A sign at the top of the building on the Sherman side, if lighted, will be a definite impingement on the right to quiet enjoyment for those of us living in 609 Sherman. This signage will also increase the height of the project further impinging on the natural views for both Parkside and 609 residents.
- b. The rooftop bar and lounge which is to be open to the public needs to be considered with more sensitivity to existing residents. Although they say it will not require additional parking because it is under 3,000 square feet, consider that the drawings reflect more than 125 seats. Where will these people park? This would be in addition to any individuals who are enjoying a meal or a drink on the lobby floor, where there is seating for well over 100. The noise generated by a rooftop bar and lounge will also have a serious impact on those who live in the area. At a minimum, hours of operation should be restricted so that the living environment for residents does not deteriorate.
- c. Noise levels in general should be considered before approving this building. Given the amount of hard surface in terms of length and height, the objectionable and illegal noise levels generated by cars with altered mufflers and revving motorcycles will be exacerbated. There is already little ability (or will) for the City to control this problem. The increase in noise pollution and reverberation will make downtown less desirable for residents and visitors alike, as during the summer outdoor dining is nearly impossible as well as normal conversation for those on the sidewalks. I specifically chose a residence

at this end of Sherman to avoid the noise levels found at the Northwest Boulevard and Sherman Avenue.

I appreciate that the City and our elected officials are interested in providing opportunities for greater economic activity in the downtown area, but it shouldn't be at the expense of those of us who make our home in the downtown. Existing residents and businesses should be confident that the city and its officials are protecting their rights and contributions to the economic base. This project deserves much greater scrutiny for operational issues and the negative impacts to 609 Sherman, Parkside and other neighboring properties. I believe that greater setbacks above the first floor should be required to preserve views, lessen impact of shade and loss of sun, and reduce the noise. Without much greater setbacks, this building and project are not a fit for this location in our downtown.

Thank you for your consideration.

Sincerely,

Cyndy Donato

**From:** Jan Gibson  
**To:** [CLARK, TRACI](#)  
**Subject:** Marriott Hotel on Sherman  
**Date:** Thursday, January 25, 2024 8:19:01 AM

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What impact will the construction have on 6th St. between Sherman and Front?  
Will the alley between 6th and 7th be impacted?  
Will the alley become one way?

Charles Gibson

**From:** S Moore  
**To:** [CLARK, TRACI](#)  
**Subject:** 6 Story Marriott Hotel at 602 and 612 Sherman  
**Date:** Wednesday, January 24, 2024 4:00:06 PM

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**CAUTION:** This email originated from outside your organization. Exercise caution when opening attachments or clicking links, especially from unknown senders.

Dear City Staff and Members of the Design Review Commission,

I am a year-round owner/resident of a condo at Parkside, 601 E. Front Avenue, directly across a single-lane alley from the proposed Marriott Project.

I am opposed to the proposed Project and urge you as individuals pursuing the best interests of citizens of Coeur d'Alene to NOT APPROVE this Project.

First, let me say that I continue to feel as if Coeur d'Alene is fast-becoming the Seattle and Portland of Idaho. Each time I learn about a newly approved project, the current U.S. southern border policy comes to mind. Coeur d'Alene seems to have lost sight of its residents and taxpayers who are already here, who have already invested heavily by purchasing homes in Coeur d'Alene and contributing to the tax base. Instead, each new approval is an appeal for more people who are not already here to come to Coeur d'Alene and negatively impact our ill-equipped infrastructure, our natural resources, our small-town atmosphere, and our majestic views. Those in power communicate to us that you care more about the out-of-towners than the existing residents and citizens.

Case in point: this 6-plus story Marriott Project.

There are 53 residential homes in the Parkside tower and 8 more in the 609 E. Sherman Ave. condominiums immediately across Sherman. Our property values have plummeted since the Marriott Project was little more than a rumor. A year ago these properties were highly-sought-after, as evidenced by my jaw-dropping property taxes. In recent months, an unprecedented number of homes have been put on the market as a reaction to the possibility of a 6-plus-story building being built literally 15 feet from Parkside living room windows. However, the homes are either NOT selling or the prices are being drastically dropped to lock in a sale. Once a three-story below-ground garage starts to be dug, I'm asking you in all seriousness, will my property value go up or will it go down?

It will go down, and not because the overall area market is deflated. No, our homes and our greatest investments will lose significant value because of *your decision and vote on this one Project*.

Please, please fight for us — the *current* citizens of Coeur d'Alene.

Several of the residential homes and all of the commercial businesses on Parkside's north side will go from a view of a church steeple rising amidst the beauty of fall foliage, snow-covered winter mountains, trees filling up with spring buds and new leaves, and summer fireworks and parades... to a view of another building's windows and balconies and the activities of strangers 15 feet across a narrow alley. How is that change good for the small-town, neighborly atmosphere that drew us to buy our homes in Coeur d'Alene?

Who will buy *that view* in the future? And for what price?

The Marriott Project is disastrous for downtown homeowners.

It will also be disastrous for the small hotels and inns along Sherman. Think about these *current* Coeur d'Alene business owners and employees as you consider the impact of this proposed Project. If one, two, or all of these smaller inns close because their clientele re-routed to the Marriott, it will be your decision

that caused the closures.

Also there is the matter of traffic and parking. Parkside has only one entrance/exit from its garage— not the usual two. Parkside's garage door is on 6th Street. Where will the staging be as the proposed Project is being built over the course of years? It should not be on 6th Street. What happens in case of a fire or medical emergency in my building? Can you assure the homeowners and business owners/employees that the Project's staging area, fences, equipment, and construction workers' vehicles will in no way hinder first responders from being where they need to be to save lives and property?

I see that the Project calls for the same number of parking spaces as hotel rooms. What is the plan for parking for hotel, restaurant, and bar employees and restaurant/bar guests? Downtown Coeur d'Alene already has a parking shortage that is so well-known, residents from Hayden, Post Falls, Rathdrum, and surrounding communities no longer come to our downtown. Ask them why. It's the congested traffic and lack of parking. Long waits at traffic lights and driving around and around on parking space quests are not reasons anyone moved to Kootenai County.

Your we-need-to-grow-bigger-denser-and-higher decisions are literally driving regular customers away from our downtown businesses. If there is a study that can be done to prove or refute this, please include both the tourist season and the non-tourist season. Full-time residents are year-round customers, but once they learn to buy elsewhere, away from Coeur d'Alene's downtown gridlock, that's what they do—they take their money elsewhere— permanently. The Farmer's Market is no longer downtown. It has moved to Riverstone where parking is easier and there is less traffic congestion. We miss it.

How is adding to the congestion and parking shortage a good idea for downtown residents and businesses?

The Project's plan shows a parking garage entrance on Sherman between 6th and 7th. I want to know how this will affect the parades which, in my opinion, are one of Coeur d'Alene's best assets. Talk about small-town charm! In the renderings of the Project, I only see the one garage entrance/exit on Sherman. Will this Project send our parades and street fairs and their revenue streams elsewhere because it's illegal to block that garage? Will these crowd draws also move to Riverstone or a friendlier, less problematic location?

I also have great concern about the single-lane alley traffic between Parkside and the proposed Project. When the huge Sysco truck or the U.S. Post Office, Fed-Ex, or UPS truck, or any delivery truck is loading and unloading, and during that time blocking the alley, where do the next-in-line trucks wait? 6th and 7th cannot handle a line of trucks, covering the needs of *two large buildings*, waiting to squeeze into a single-lane alley.

Now imagine that back-up during snow conditions, or when a large moving van is parked on 6th Street to move in a new Parkside owner or, more likely, to move out a fleeing Parkside owner.

What about the increased noise and pedestrian hazards caused by doubling the large truck traffic around Parkside's perimeter? Will that increased traffic noise and congestion raise downtown homeowners' property values or permanently depreciate our property value?

Again I ask you, are you sacrificing the many, many existing downtown corridor homeowners and business owners in order to make visitors passing through happy?

Speaking of winter conditions, when there is a snow berm down Sherman, will adding in the traffic entering and exiting the Project's Sherman Avenue garage have a positive effect on the small-town charm of Coeur d'Alene? No, it will increase the traffic-flow problems, especially since these vehicles will be driven by out-of-towners whose phones are telling them to make an impossible left-hand turn through a snow berm.

I have too many concerns to cover in this letter. I will close with my concern regarding the effect the noise and light from a hotel bar, which is partly outside, will have on Parkside homeowners 15 feet across



the alley. The median age at Parkside is over 70. We bought our properties because we appreciate the advantages of being in a vibrant downtown community, but we did not do so in anticipation that a bar would be built literally 15 feet away from our living room windows. Also, we purchased our homes thinking our civic leaders will look out for our best interests. Not only will there be the noise from music and TVs; there will also be lights from the bar and the hotel rooms, on and off and on and off and (you get the idea) — 15 feet outside our homes' north and west massive windows. The rendering does not make clear if the multiple large signs on the Project will be lit, but I assume they will be.

Please, please, please, dear Planners, as you make your decision, consider the adverse effect this Project will have on the property value and lifestyle joy for homeowners, not just downtown but in the surrounding quaint, history-rich neighborhoods, the adverse effects on the small inns and hotels, nearby Bed and Breakfasts, and the businesses, many of which are in historical buildings— all the things that make Coeur d'Alene special and magical. As Sherman, 6th, and 7th become congestion nightmares, please consider where the overflow parking and adjusted driving routes will relocate. Answer: to the surrounding streets and neighborhoods.

This proposed Project will only make our beloved city a lot more like Seattle and Portland. *This Project is not in our best interests.*

I accept that something will be built on this vacant lot at some point, but I want it to reflect northern Idaho's history in its design and architecture. I want it to increase my property's value, my comfort and safety, and the joy that has come from the small-town feel of our city corridor. Please don't take Coeur d'Alene one step closer to losing the very attributes that make our community special. Don't sacrifice the charm that sets us apart from other booming cities whose infrastructures are crumbling under the weight of growth-focused decisions. Those cities will never be able to turn back the clock and fix all the wrongs that came out of each bad decision. We don't want to be one of those cities.

Please use your vote to stand up for Coeur d'Alene's existing residents. Please vote to NOT approve the proposed Marriott Project.

Thank you for reading my entire letter. I truly appreciate that.

With respect,

Shelly Moore  
601 E. Front Ave. #1402  
Coeur d'Alene ID 83814

**Joan C Woodard**  
**609 E. Sherman Ave Unit 401**  
**Coeur d'Alene, ID 83814**  
**707-479-5090**

January 23, 2024

Subject: 6 Story Marriott Hotel at 602 & 612 Sherman

Dear City Staff and Members of the Design Review Commission:

I am writing because I will be unable to attend your meeting on January 25, 2024 due to a prior commitment. I also spent over 40 years in the commercial development business around the country and have numerous concerns about the proposed project.

1. Let me start with a positive. I believe the architect's choice of materials and basic design are quite admirable and it is clear they have experience in designing multi-story hotels.
2. However, the massing and scale of this project violate the intent of the Coeur d'Alene Downtown Development Regulations and Design Standards adopted as of July 5, 2006.
  - a. "To encourage private and public investment, attract shoppers and visitors, and appeal to existing and new residents". This project will not appeal to the existing residents across the street or within the same block. The architect and developer for this project are both from out of state, so perhaps they didn't think it would help to understand the needs of the neighborhood before they began planning this project. To my knowledge, there was no attempt by them to meet with any of us to discuss these factors before beginning their work. In my work in various cities, I would never have been able to schedule a Design Review meeting until I had done this!
  - b. "Preserve views of Tubbs Hill and other distant landforms, view corridors through upper-level setbacks along with tower size and spacing". Concerning 609 Sherman, directly across the street, all of our existing views to McKuen Park, Tubbs Hill, hills to the east, and much of Lake Coeur d'Alene are obliterated by the massing and scale of this building. Yes, they have provided the minimal setback above 45 feet, but that accomplishes very little in terms of this view standard. As I understand civic history, the Parkside project had a similar issue when it was proposed, which is why the setbacks from 6<sup>th</sup> and 7<sup>th</sup> Street are so large. By making the building taller and narrower, they were able to preserve views for the surrounding area and create more visual interest at the lower elevations. Additionally, for many months of the year when the sun is low, this proposed building will block the sun from reaching the front of our building far beyond the amount of time the existing Parkside project does. A sun-hours and shadow study would normally be required to analyze the impact of a proposed structure on existing buildings and neighborhoods.

- c. “Respect the small-town scale and character”. Because this site is so large and has the longest frontage on Sherman as any other property on Sherman in the downtown area, building from property line to property line creates a structure that is out of scale with “small town” and is not in keeping with the existing structures on Sherman.
  - d. I found it very interesting that there was no attempt by the applicant to show their project as it more directly relates to 609 Sherman other than a height comparison. The approval of this hotel will most significantly impact our building, our real estate value, (and therefore our assessed value for property tax) and our enjoyment in living downtown as it turns into a smaller version of Portland or Seattle
3. Operational Issues must have much more focus to reduce the impacts to neighboring buildings, pedestrians, and functions that are important to downtown Coeur d’Alene.
- a. The location of the driveway entrance on Sherman will create backup issues in both directions as there is no left-turn lane from Sherman into the parking driveway and it is just a short distance away from the stop light at 7<sup>th</sup>. Cars routinely stack up on eastbound Sherman for the stoplight at 7<sup>th</sup>, blocking access. During the winter, berms are frequently blocking the middle of Sherman so there would be no ability to safely access the entrance for cars entering from the east.
  - b. During the many parades and other events that occur on Sherman, access is typically blocked at 7<sup>th</sup> Street (and further west) to allow Sherman to be a pedestrian street with no cars. Access will have to occur from the alley between 7<sup>th</sup> and 6<sup>th</sup> during these times. However, the alley is basically a single-lane alley with no available increase in width to accommodate two-way traffic. The narrowness of this alley will also make it very difficult for passenger vehicles to manage a turn in or out without pulling into the alley, creating an unsafe situation for all.
  - c. This alley is also the only location for at least 4 large dumpsters associated with Parkside that are emptied multiple times per week. It is also the location for delivery vehicles (Fedex, UPS, Amazon, etc.) as well as delivery trucks making deliveries of food, liquor, etc. to the restaurant in Parkside. If the subject property intends to use the alley as well for garbage and deliveries, this could/will create a bottleneck for the smooth operation of all. It also will make it nearly impossible for hotel guests to utilize the alley entrance and exit.
  - d. Another concern will be firefighting. The heavy utilization of this alley will require the fire department to fight a Parkside fire predominantly from Front, 7<sup>th</sup>, and 6<sup>th</sup>, as the alley will be too greatly impacted by utilization and the mass at the western edge of the proposed building and the alley to enable a large truck to make the turn eastbound. It will also mean that the proposed hotel will be addressed by the fire department only from Sherman and 6<sup>th</sup> Street. Does the Fire Department have the equipment to deal with these distances and heights?
  - e. Finally, as the building is proposed, there is no acceptable construction staging area. From an operational standpoint, the alley cannot be used as it will impact the permitted use of the alley by Parkside. Staging cannot close off Sherman either, which leaves 6<sup>th</sup> Street, which will severely impact Parkside and the pedestrian focus of 6<sup>th</sup> Street. I just don’t see how this project can be built as conceived based on these logistical factors.

4. Specific Design Questions.

- a. In the applicants' materials, they describe the signage for the property, which seems fine based on the narrative, however, the renderings they presented show two additional signs on the top of the building. One on Sherman and the other on the East side of the building. If these signs are to be considered, they should be included in the calculations for allowable signage and the question of whether they are to be lighted or not should be addressed. A sign at the top of the building on the Sherman side, if lighted, will be a definite impingement on the right to quiet enjoyment for those of us living in 609 Sherman.
- b. The rooftop bar and lounge which is to be open to the public needs to be considered with more sensitivity to existing residents. Although they say it will not require additional parking because it is under 3,000 square feet, consider that the drawings reflect more than 125 seats. Where will these people park? This would be in addition to any individuals who are enjoying a meal or a drink on the lobby floor, where there is seating for well over 100. The noise generated by a rooftop bar and lounge will also have a serious impact on those who live in the area. At a minimum, hours of operation should be restricted so that the living environment for residents does not deteriorate.
- c. Noise levels in general should be considered before approving this building. Given the amount of hard surface in terms of length and height, the objectionable and illegal noise levels generated by cars with altered mufflers and revving motorcycles will be exacerbated. There is already little ability (or will) for the City to control this problem. The increase in noise pollution and reverberation will make downtown less desirable for residents and visitors alike, as during the summer outdoor dining is nearly impossible as well as normal conversation for those on the sidewalks.

I appreciate that the City is interested in providing opportunities for greater economic activity in the downtown area, but it shouldn't be at the expense of those of us who make our home in the downtown. Existing residents and businesses should be confident that the city is protecting their rights and contributions to the economic base. This project deserves much greater scrutiny for operational issues and the negative impacts to 609 Sherman and other neighboring properties. I believe that greater setbacks above the first floor should be required to preserve views, lessen impact of shade and loss of sun, and reduce the noise. With greater setbacks, more building height would be acceptable.

Sincerely,

Joan Woodard

**From:** Karen Botker  
**To:** [CLARK, TRACI](#)  
**Subject:** Public Hearing Comment  
**Date:** Wednesday, January 24, 2024 7:02:04 AM

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While I strongly oppose anything being built downtown that detracts from the lovely, small town, downtown feel, whomever makes those final decisions already ruined it by allowing another 13 story building in the downtown corridor.

I hope the deciding body will make a decision now, about how much they will and will not allow in the future. I would hate to see downtown CDA become a downtown that never feels sunshine on the sidewalks because of all of the large buildings that are being built there.

Karen Botker  
Kootenai Co. Resident

**From:** Tom Prohaska  
**To:** [CLARK, TRACI](#)  
**Subject:** Design Review Commission ITEM DR-1-24 Comments  
**Date:** Monday, January 22, 2024 5:22:39 PM  
**Attachments:** [image003.png](#)

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I am writing to provide comments about the above referenced item being presented on Thursday, January 25, 2024 at 12:00 pm at City Hall, 710 E. Mullan Ave., Conference Room #6, Coeur d'Alene, Idaho. In particular, CDA Hotel, LLC is proposing a six (6) story Marriot Hotel with a parking structure three stories underground for guest parking.

I am the co-Founder, Chairman, CEO and President of Idaho Trust Bank. Idaho Trust Bank was founded in 1994 in Coeur d'Alene, Idaho by myself and my brother, Daniel Prohaska. We have maintained a continuous presence in Coeur d'Alene for the last 30 years. In 2012, Idaho Trust Bank purchased 622 East Sherman Ave in downtown Coeur d'Alene. Originally built as a bank building, over the prior 40 plus years it had changed ownerships and uses. When acquired by Idaho Trust Bank, the building was very run down and somewhat of an eye-sore. We immediately spent hundreds of thousands of dollars to remodel and refresh the building. Over the last twelve (12) years it has anchored the southwest corner of Sherman Ave. and N. 7<sup>th</sup> Street in Coeur d'Alene's beautiful downtown. This location is home to seven (7) full time employees where we serve customers and businesses from all over downtown, Coeur d'Alene and Kootenai County with banking, lending and wealth management services. Idaho Trust Bank is one of only nine (9) banks headquartered in Idaho.

Idaho Trust Bank is supportive of a vibrant and growing Downtown Core. We also recognize that four (4) lots comprising 602 and 612 E. Sherman Ave. are desirable locations for development. However, we have concerns about the proposed plan offered by CDA Hotel, LLC. These concerns include (and may be further augmented when additional information or analysis is available):

- the scale of the hotel structure relative to the rest of the character and buildings in the Downtown Core (see, Downtown Core zoning district Design Regulations);
- the scale of the hotel structure relative to the Idaho Trust Bank building at 622 East Sherman Ave. (see, Downtown Core zoning district Design Regulations);
- the impact of the construction of an underground structure of the scale proposed on surrounding buildings in the Downtown Core;
- the impact of the construction of an underground structure of the scale proposed on the Idaho Trust Bank building at 622 East Sherman Ave.;
- the impact of the construction of a building on the scale proposed on surrounding buildings in the Downtown Core;
- the impact of the construction of a building on the scale proposed on the Idaho Trust Bank building at 622 East Sherman Ave.;
- the impact of the construction of the building and underground structure on the retaining wall surrounding 622 East Sherman Ave.;
- the negative impact of the proposed project on the streetscape of the Downtown Core (see,



Downtown Core zoning district Design Regulations);  
-the negative impact of removal of Sherman Ave parking spaces in the Downtown Core (see, Downtown Core zoning district Design Regulations);  
-the negative impact of removal of S. 6<sup>th</sup> Street parking spaces in the Downtown Core (see, Downtown Core zoning district Design Regulations);  
-the negative impact of a significant increase in traffic on Sherman Ave in the Downtown Core;  
-the lack of a “blank wall treatment” on the wall facing 622 East Sherman Ave. as required by the Downtown Core zoning district Design Regulations;  
-the lighting intensity of the proposed structure on the character of Sherman Ave in the Downtown Core (see, Downtown Core zoning district Design Regulations);  
-the lack of screening of the parking structure (see, Downtown Core zoning district Design Regulations);  
-the lack of a parking impact study in support of the proposed project;  
-the lack of a traffic study in support of the proposed project;  
-the lack of an engineering study identifying the impacts of the construction on surrounding buildings in the Downtown Core; and,  
-the lack of an engineering study identifying the risks and mitigation to the retaining wall supporting 622 E. Sherman Ave.

Idaho Trust Bank would also like to point out that the purpose of the Downtown Core zoning district states that, “Shops and restaurants would be located along key streets.” This proposed development violates that statement of purpose as there is no more key street than Sherman Avenue and this proposed use is not a shop or a restaurant. This underscores our concern that the proposed development is out of character for the buildings and uses in the Downtown Core.

Again, Idaho Trust Bank is very supportive of the Purpose set forth in the Downtown Core zoning district. But, we have many concerns about the proposed project. As a result, at this time, we would ask that the Design Review Commission reject the proposed project.



**Thomas Prohaska**  
Chairman, CEO & President

888 W. Broad Street, Boise, Idaho 83702 | [tfp@idahotrust.com](mailto:tfp@idahotrust.com)  
Desk 208.350.2735 | Main 208.373.6500

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**From:** Sarah (Nichols) Jarvis  
**To:** [CLARK, TRACI](#)  
**Subject:** Upcoming CDA Marriot Hotel  
**Date:** Monday, January 22, 2024 12:03:30 PM

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Hi there,

I recently saw the article discussing the proposed Marriot hotel in downtown CDA. As a local CDA resident, I wanted to voice my support for this proposed opportunity. Our local economy could greatly benefit from more options where people can stay, especially in downtown. The CDA resort, albeit a wonderful place to spend time and visit, has monopolized the downtown market and therefore controlled prices, making it difficult for more people to come visit and stay in the downtown area. Additionally, every summer the resort is fully booked.

Looking forward to the approval of this hotel.

Thank you!

--

Sarah Jarvis  
208 – 661 – 8208

**From:** John S. Nichols  
**To:** [CLARK, TRACI](#)  
**Subject:** Marriott Hotel Downtown CDA  
**Date:** Monday, January 22, 2024 10:45:16 AM

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Hello,

I recently heard about the proposed Marriot Hotel for downtown CDA. I simply wanted to voice my support for this hotel. As someone that owns and develops properties in North Idaho, having a new hotel in downtown would be great for the local economy.

I look forward to hearing about this projects approval for our downtown area.

Sincerely,

John Stephen Nichols  
Cornerstone Commercial Investments, Chief Operating Officer  
(208) 916-0212  
Coeur d'Alene, Idaho  
<http://www.cornerstonecommercialinvestments.com>

**From:** Ed Hatter  
**To:** [CLARK, TRACI](#)  
**Subject:** CDAHOTEL, LLC  
**Date:** Monday, January 22, 2024 7:38:04 AM

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My wife and I live downtown and strongly feel this is not the proper location for a busy hotel because...

..Downtown along Sherman is already much too busy and loud.

.. Egress and ingress would create traffic pressure on Sherman, 6th, and 7th which is already way too congested.

.. increased noise which is already a major problem downtown and is a problem everyone has already been attempting to solve.

.. Impact of a hotel is totally different than condo living as it is totally transitory and occupied by short term occupants with no real regard for the community.

Sincerely,  
Ed and Michael Hatter  
701 E Front Ave  
Apt 701  
Coeur d Alene  
208-755-8679

**From:** strategic.scientific@verizon.net  
**To:** [CLARK, TRACI](#)  
**Subject:** Item DR-1-24  
**Date:** Sunday, January 21, 2024 4:33:09 PM

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In response to the planned Marriot Hotel, I STRONGLY object to this development.

It will increase the traffic on Sherman and put further strain on the downtown infrastructure.

John Wieser  
601 East Front Ave.  
Coeur D'Alene, ID 83814



**From:** kaquilter1@gmail.com  
**To:** [CLARK, TRACI](#)  
**Subject:** Marriott Hotel  
**Date:** Sunday, January 21, 2024 11:17:39 AM

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I am totally against the building a Marriott Hotel in Coeur d'Alene!! It is way too large and imposing for our downtown area. It would greatly change the character of our charming city with its many shops and restaurants that attract tourists and residents alike. The CdA resort is set back from Sherman, and with its beautiful landscaping, doesn't see so obnoxious as the proposed Marriott would. I am shocked that our city mayor and others in city management would entertain such a disgusting idea.

Sent from my iPhone  
Karen Anderson

**From:** kaquilter1@gmail.com  
**To:** [CLARK, TRACI](#)  
**Subject:** Marriott Hotel  
**Date:** Sunday, January 21, 2024 11:40:41 AM

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I am totally against the building of a Marriott hotel in downtown Coeur'Alene!! It would greatly change the character of the whole downtown area, with its charming shops and restaurants , which attract tourists and residents alike. I'm shocked that our mayor/ city leaders would entertain such a proposal! Coeur'Alene has a lovely, cozy small town feel, yet with lots to offer. Why destroy that so some developer can make a fortune??!! Outrageous!

Sent from my iPhone

**From:** Melita Clary  
**To:** [CLARK, TRACI](#)  
**Subject:** Marriot Hotel on 602 and 612 Sherman, Courd'Alene  
**Date:** Saturday, January 20, 2024 7:32:07 PM

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To whom it may concern,

We have lost our hometown feeling, and although I live in Post Falls, I am very much a part of the community.

I understand free commerce and capitalism . If I invested money in land, I would want to do with it what I wanted to also. The difference is that I would not infringe on my neighbors, nor their ability to enjoy their adjoining land.

We already have five high-rise buildings downtown with another larger one coming on East Front Street that muddies the area. This completely ruins the historic beauty of our downtown . Marriot already has developed hotels in our area. Please forbid them to put a hotel in . I understand this is a grassy lot in front of one of the towers. If they were to go ahead with this, could it be a maximum of three floors(stories) and be regulated to have the esthetic (look like) our older buildings that line Sherman Avenue?

Thank you,

Melita Clary

Sent from my iPhone

**From:** Tom Pehlke  
**To:** [CLARK, TRACI](#)  
**Subject:** new marriott  
**Date:** Saturday, January 20, 2024 12:42:35 PM

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I can not attend the meeting about the proposed new Marriott but I have a concern. 130 parking spaces for a hotel with 131 rooms will not work. The project proposes a bar and restaurant as well. This project will destroy residential parking in the areas to the north of the proposed Hotel. Where do they think the customers and employees will park their cars?

Tom Pehlke

Design Build Group  
208 651-2520

**From:** Sara Klumpe  
**To:** [CLARK, TRACI](#)  
**Subject:** Proposed Marriott Hotel at 601-612 E. Sherman, Coeur d'Alene, ID  
**Date:** Saturday, January 20, 2024 9:05:46 AM

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**PLEASE - can't we just have some green space downtown?  
My input probably means nothing to "the powers that be" but  
I am totally against adding (yet another) tall building  
downtown.**

BTW, I don't live in the structure behind the proposed building, but I would imagine the people facing north are already bummed they have a so-so view of CDA, much less than backside of another hotel.

**NO - NO - NO - NO!! PLEASE RESERVE SOME SPACE FOR  
HUMANITY, NOT SECOND HOME OWNERS!**

S. Klumpe  
sklumpe@hotmail.com

*"If we couldn't laugh we would all go insane." ~ Jimmy Buffett*

**From:** william canevari  
**To:** [CLARK, TRACI](#)  
**Subject:** Marriot Hotel on Sherman to be built  
**Date:** Saturday, January 20, 2024 6:12:17 AM

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As a CDA resident I am against the construction of more huge buildings in the downtown Sherman corridor. The continued construction of more and more high rise buildings is turning the town into Bellevue.

I grew up in Bellevue, Washington in the 60s. It was a quaint little suburban town, with low rise buildings and a hometown feel. That changed in the 90s and 2000s, and now it so metro, that you can barely see the sky anymore.

I dont want that to happen to this town. I have lived here for 30 years, having moved here for the "hometown feel" of CDA. I see that has eroded somewhat. If there is a need for more Hotels and high rise condo/apartments in CDA, they should be built away from the downtown Sherman corridor. The east Sherman area needs rehabilitation more than downtown needs a new high rise hotel.

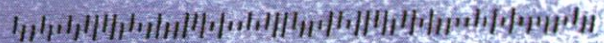
William Canevari  
wcanevari@yahoo.com



604 Sherman  
CdA, ID 83814



Coeur d'Alene Planning Department  
710 E. Mullan Avenue  
Coeur d'Alene, ID  
83814



Please cut here

Coeur d'Alene Planning Department  
710 E. Mullan Avenue  
Coeur d'Alene, Idaho 83814

Comments:

I do not support this project. We have plenty of housing/hotel options in CdA. We don't need more. We have enough traffic on Sherman. We don't need more. We have too few green spaces in the downtown grid. We need more of these. We do not need more lights downtown taking away from our starry nights. We do not need more noise downtown. We have already too much. We don't need anymore buildings that are more than 2-3 stories cluttering up our lake views. This takes away the charm of CdA. Pretty soon, we'll be so crowded that our tourism is going to suffer - too over crowded!

- &/or 2. Phone or visit our office (769-2240) with your concerns or questions
- &/or 3. Email your comments to: [tclark@cdaid.org](mailto:tclark@cdaid.org)
- &/or 4. Come to the public hearing.

ITEM: DR-1-24



Of course, no one adjacent to this property wants their view restricted to the back of a building. I get property rights. But I also get our and our neighbors' investments in Parkside and feel it should be protected.

Some have paid over one million dollars, or close to it, to live in the downtown core. The height of the proposed building would severely impact those on the north side of Parkside. Thankfully, we are on the south side.

Going ahead with the proposed building would severely impact our property value in a negative way. Not temporarily, but permanently.

Marriott Hotel probably needs the building to be that high to recoup their expenses and make it pencil out as a good business investment – for them. A shorter building, with a maximum of 3 stories above ground, would not be as negatively impactful as the proposed building.

The easy response is “if you wanted control over what goes there, you should have bought it.” We are beyond that now, and want what is best for the 53 residential condos, not one investor.

Tina Johnson  
601 E. Front #1103  
whatagreatson@gmail.com

**From:** Ed Reinhart  
**To:** [CLARK, TRACI](#)  
**Subject:** Opposed to CDA Hotel, LLC / Public Hearing Feedback  
**Date:** Tuesday, January 16, 2024 3:13:51 PM

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Good Afternoon,

First, I want to thank the board for allowing the public and property owners to voice their opinion. It's shows a good faith effort to be transparent in the permit process.

We are property owners at Parkside Condos located at 601 East Front Avenue. Condo #703

The proposed 6 story Marriott Hotel will have a profound impact on both traffic and noise in the area. Many residence are year round and the increased noise will absolutely increase.

In addition, the nature of the hotel industry is high turnover. That means increased cars/trucks/delivery vehicles turning into 6th avenue. CDA promotes it's self to a be safe bike and walking city...this is exactly the opposite effect.

We are strongly opposed and urge the Board to decline the permit. This is not the type of project for that property.

Thank you,

Ed & Deb Reinhart

#703  
601 E. Front Ave

**From:** James Sawhill  
**To:** [GOOKIN, DAN](#); [EVANS, AMY](#); [WOOD, CHRISTIE](#); [MILLER, KIKI](#); [ENGLISH, DAN](#); [MCEVERS, WOODY](#); [CLARK, TRACI](#)  
**Subject:** CDA Hotel Appeal Marriott, DR-1-24AA  
**Date:** Wednesday, May 29, 2024 12:28:35 PM  
**Attachments:** [Appeal Marriott Letter Sawhill.pdf](#)  
[Appeal Marriott Narrative Sawhill.pdf](#)

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Council Members

I support the appeal of the Marriott hotel Design Review Committee approval. I do not believe the projects meets all the standards in the Downtown Design Guidelines. Please see the attached letter and narrative. I request the Council uphold the appeal and return the project for redesign and modification to address the deficiencies.

Thank you.

Jim Sawhill

Jim Sawhill  
1700 Tower Pointe Drive 501  
Coeur d'Alene ID 83814  
907-230-7339 [jimsawhillak@gmail.com](mailto:jimsawhillak@gmail.com)

City of Coeur d'Alene  
710 E. Mullan Ave  
Coeur d'Alene, ID 83814

Attention: City Council

Regarding: CDA Hotel LLC (Marriott Hotel)  
Appeal of Design Review Committee Approval

Dear Council Members

I support the appeal of the Marriott hotel Design Review Committee approval. I do not believe the projects meets all the standards in the Downtown Design Guidelines. Please see the attached narrative. I request the Council uphold the appeal and return the project for redesign and modification to address the deficiencies.

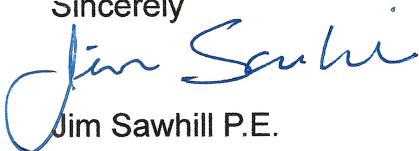
I'm a civil engineer with 35 years' experience completing commercial, retail, hotel, and residential projects. I also have significant planning experience completing rezones, conditional use, subdivisions, traffic studies, and design approvals.

The Downtown Design Guidelines sets clear expectations for development in the core area. This project fails to meet the guidelines and, in my opinion, didn't even try. I've worked with a lot of hotel chains. They typically have prototype designs that they adapt to a site. That's what this looks like to me. After the fact they tried to justify their design to the guidelines.

The core area has many examples of buildings that conform to the guidelines and blend into the character of downtown. This design could easily be enhanced with colors, materials, and details to meet the Design Guidelines and blend into the downtown core area.

*Please uphold the appeal and return the project for redesign and modification to meet the Downtown Design Guidelines.*

Sincerely



Jim Sawhill P.E.

## Request

***The CDA Hotel LLC design review appeal must be upheld and the project returned for redesign and modification.*** The application does not meet the requirements of the Downtown Design Guidelines. The application is also flawed because it did not include a traffic impact analysis (TIA). Significant traffic issues are likely to come to light that will require design modifications which will change many elements of the current design and effect the approval. The project also did not make any effort to engage the community to understand local concerns and obtain input.

## Application

The CDA Hotel LLC application is very minimal. The two-page narrative is very telling in its lack of information. The application provided no analysis of the Downtown Design Guidelines and how the project conforms to each important standard. The applicant did provide an after the fact analysis only after requested by staff. It is obvious that the hotel was designed with no regard for the design guidelines and the core area colors and materials.

## Coeur d'Alene Downtown Design Guidelines

The Downtown Design Guidelines is an important document to maintain the character of the core area. Within the core area is the Sherman Avenue historic district that runs from 1<sup>st</sup> Street to 6<sup>th</sup> Street. This project has the opportunity to extend the historic district's character or define its end.

The project fails to meet several design standards in the guidelines. ***Most important it ignored the color palette and materials common to downtown.*** The project as designed will detract from the character of downtown.

***The application fails to meet the following standards.***

### Sidewalk Uses

*This standard is not met.*

The application provided information on compliance to this standard for Sherman Avenue.

No information or analysis was provided for 6<sup>th</sup> Street, a pedestrian oriented street. A 7-foot clear dimension for pedestrian travel must be provided, as well as appropriate setbacks from the curb as provided by code. Based on the application, there is no way to know if this standard is met.



## Massing

*This standard is not met.*

The Design Guidelines provide written standards and pictures and graphics to convey the requirements of the core area. The pictures are important (a picture is worth a thousand words) because it better communicates expectations on color and materials.

The base of the proposed hotel does not meet the guidelines for materials and color.

The grey brick is out of character of downtown and the immediate area. Parkside, McEuen Tower, and 609 Sherman condos have redbrick. The Masonic Lodge building has a red and tan base. The style of the proposed architecture does not adopt any treatment or details common to downtown and listed in the Design Guidelines.



The proposed hotel is a stark contrast to the surrounding area. The project has made no effort to have colors and materials complimentary to the surrounding area, particularly the Masonic Temple Building across the street.

### Ground Level Details

*This standard is not met.*

The Design Guidelines provides a list of 13 design elements that may be incorporated into a project. Five of the elements are required.

The Sherman Avenue elevation has three of the 13 design details that are listed in the guide: seasonal planters, canopies, and concrete plinth. They claim a nonexistent pedestrian scale sign and accent sconces which is not a listed element.

The 6<sup>th</sup> Street elevation has two listed details, a concrete plinth and canopies. A significant area on 6<sup>th</sup> Street is a blank wall with no treatment to mitigate its impact.

The list of elements and pictures in the Design Guidelines convey the intent of the guidelines for projects to embrace the historic character of downtown. This project fails to do this.

### Ground Floor Windows

*This standard is partially met.*

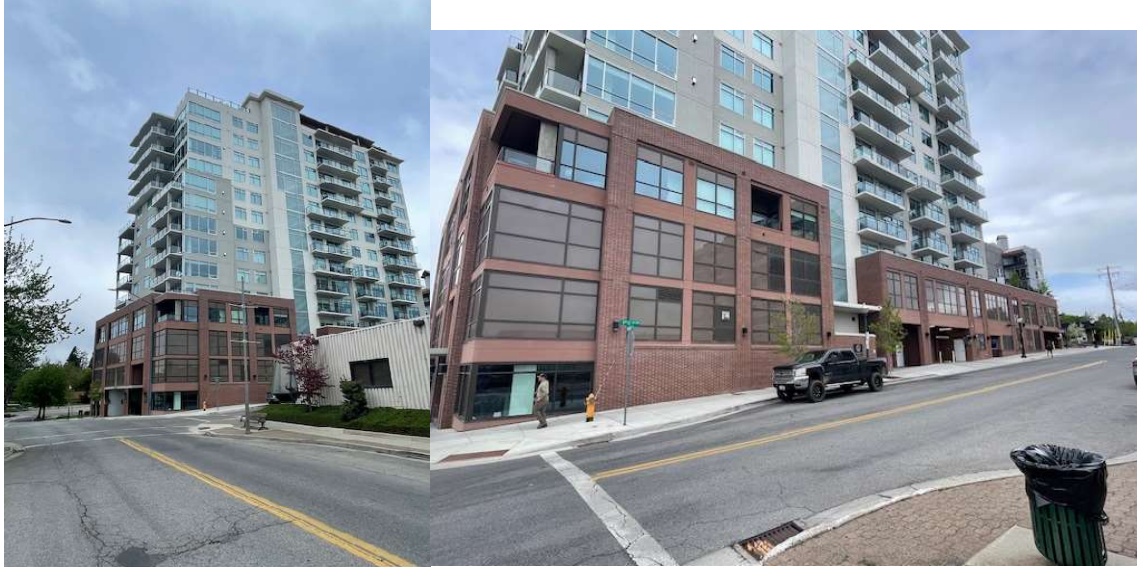
The Sherman Avenue elevation meets the 40% transparency standard for vehicle-oriented streets with 45% window area.

The 6<sup>th</sup> Street elevation only has 26% transparency. 60% is required for pedestrian oriented streets.

### Unique Historical Features

*This standard is not met*

This building was designed as a contemporary structure and does not relate to the context of downtown or the immediate area. The pictures in the section of the guidelines clearly communicate the expectations of the community and has been successfully employed by other buildings in the core area. This project has failed to incorporate historic features or incorporate the color palette and materials of downtown.



One Lakeside building used materials, color, windows, and guideline elements to meet standards for the base.



The Great Floors building conforms to Downtown Design Guidelines for material, color, and details.





The 3rd Street Parking Garage used materials, color, false windows, and details to blend a typically bland structure into the fabric of downtown.



The Architects West and Washington Trust Bank buildings blended modern materials into their building and embraced the intent of the guidelines.



The Former Wells Fargo Bank Building on Sherman Avenue used materials, color and details in accordance with the design standards.

***There are numerous buildings in the core area that have adapted their design to blend into downtown.***

### Traffic Impacts

The site is over developed given the restrictions on Sherman Avenue and 6<sup>th</sup> Street. Sherman Avenue is a Vehicular-Oriented Street. Driveways are not prohibited but should be discouraged. 6<sup>th</sup> Street is a Pedestrian-oriented Street. No curb cuts are allowed on pedestrian oriented streets per the Guidelines, but many exceptions have been made over the last 20 years. This leaves the alley as the primary access. The alley as access would work for a residential development but would be insufficient for a hotel.

A driveway onto Sherman will cause congestion and interrupt the smooth flow of traffic with turning vehicles which will be discussed in detail below. A driveway onto 6<sup>th</sup> Street would be better and of far less impact to the area traffic flow. The City should provide a waiver of the 6<sup>th</sup> Street curb cut prohibition.

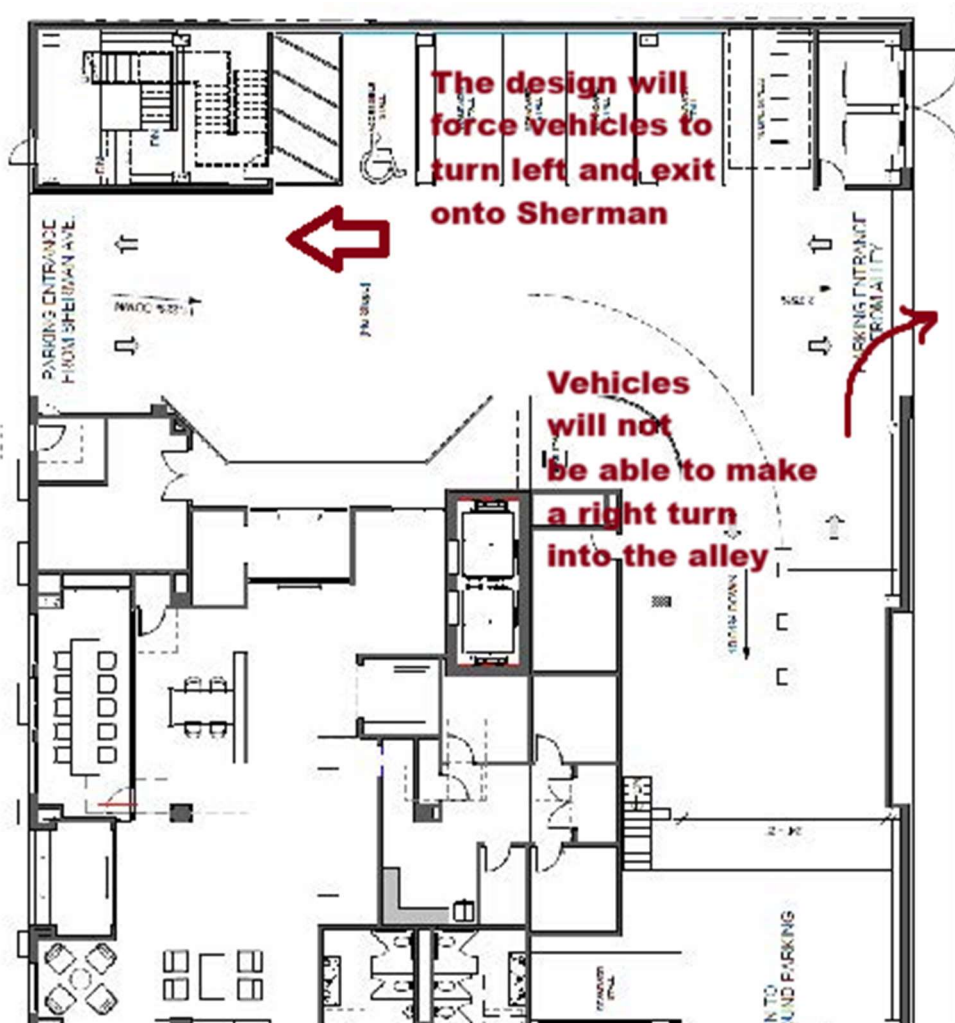
The Institute of Transportation Engineers, Trip Generation Handbook has a trip generation rate for hotels of 8.17 trips per room and a pm peak hour rate of .6 trips per room. The proposed 131 room hotel will generate 1070 trips per day and 79 trips during the pm (5:00 pm) peak hour. The projected traffic will reduce the level of service of the area roadways.



An unsignalized intersection analysis and queuing analysis is needed of the driveway on Sherman to understand the impact and congestion likely to occur. Vehicles turning left into the hotel are likely to block the 7<sup>th</sup> Street left turn pocket.



The underground parking has the exit lane coming up from the lower levels against the south wall (see below). This will make it impossible to turn right and enter the alley. There is simply not sufficient maneuvering room. Turning templates should be used to verify this. This design issue will force additional vehicles onto Sherman Avenue further impacting this roadway.



Level 1 floor plan

A Traffic Impact Analysis is needed to understand the impacts of the proposed hotel on the transportation system of the area. A driveway onto 6<sup>th</sup> Street should be considered as a less impactful option.

## **Appeal**

***The appeal must be upheld.***

*The project must be returned for redesign and modification to address the following Downtown Design Guidelines Standards:*

- A Traffic Impact Analysis must be completed before the review, and then the Applicant should provide revisions to the plan to address the following standards that have not been met
  - Sidewalk use
  - Massing
  - Ground Level Details
  - Ground Floor Windows
  - Unique Historical Features
  - Downtown color palette and materials

# APPELLANT PRESENTATION

# A Request to Appeal the Design Review Commission's approval of the CDA Hotel, LLC design for a six (6) story Marriott Hotel

Appellant: Joan Woodard, 609 E. Sherman Ave.

Date: April 16, 2024

- Traffic, Circulation and Pedestrian Studies of the intended use and design are typically done before the design of the project is approved and construction commences
- The entrance and exit to the hotel should occur on 6th Street rather than Sherman Avenue
  - Curb cuts were to be categorically denied on Sherman
  - Stacking problem on Sherman at stop light on 7<sup>th</sup>, double yellow, no turn lane
  - Proximity to Idaho Trust grandfathered driveway on Sherman
  - Interference with downtown events and Sherman closures
  - Potential interference with I-90 emergency by-pass
  - No "of right" to curb cuts if moved
  - 6<sup>th</sup> is a pedestrian street, but drives exist on pedestrian streets throughout downtown with approved Design Departure
  - Entrance and exit to a high-use service alley is not an acceptable option

## Some Goals and Objectives of the Comprehensive Plan that should have been addressed by the Applicant in their submittal

- Goal CI 1: **Coeur d'Alene citizens are well-informed, responsive, and involved in community discussions.**
- OBJECTIVE CI 1.1: Foster broad-based and inclusive community involvement for actions affecting businesses and residents to promote community unity and involvement.
- OBJECTIVE CI 2.1: Maintain the community's friendly, welcoming atmosphere and its small-town feel.
- OBJECTIVE CI 2.2: **Support programs that preserve historical collections, key community features, cultural heritage, and traditions.**
- OBJECTIVE ER 3.1: Preserve and expand the number of street trees within city rights-of-way.
- OBJECTIVE ER 3.2: Protect and enhance the urban forest, including wooded areas, street trees, and "heritage" trees that beautify neighborhoods and integrate nature with the city.
- OBJECTIVE GD 1.7: **Increase** physical and visual access to the lakes and rivers
- Goal GD 5: Implement principles of environmental design in planning projects.
- OBJECTIVE GD 5.1: **Minimize glare, light trespass, and skyglow from outdoor lighting.**

## Per the Adopted Comprehensive Plan, these action items were identified to be addressed within the first five years

- Review and consider changing the Zoning Code to **discourage obstruction of open view corridors of both public and private parks, green spaces and natural areas.**
- **Reevaluate downtown design standards to enhance infrastructure and usability, while preserving the character and historic nature of the corridor**
- **Establish a visual resources inventory in the community and determine if there are specific guidelines that should be defined and established in the City Code for public view corridors in development projects.**
- **Evaluate if building heights in zoning districts adjacent to shorelines should be modified to protect view corridors and limit shadows.**
- Modify the Zoning Code to encourage meaningful public access to shorelines and **preservation of public view corridors** through density bonuses, height incentives, or other means.
- Revise the Zoning Code to include lighting standards for parking lots and new commercial, mixed-use, multifamily residential, and industrial development so as **to avoid light pollution and nuisance complaints.**

## A few of the Comprehensive Plan shortcomings and information missing from the Application

- Did not address the obstruction of views for neighboring property owners or the impact of shade, shadows, or glare
- Did not adapt the project massing to preserve some views for those driving or walking westbound on Sherman or for those living and working on the opposite side of Sherman
- No effort by Staff or Applicant to seek out input from neighboring property owners. This denied the public from having a voice or knowing decisions that have been made that will affect them
- Did not address how lighting and noise that will emanate from the open rooftop lounge/bar/restaurant will impact neighbors, night sky, and potential for nuisance complaints
- Did not consider the historical context for setbacks/massing

## Consider the impact to these views and vistas for property owners across the street





Consider the impact on views and vistas for those driving or walking down Sherman Avenue



## Some specific failures of the application regarding the Site Performance Standards

- Street Trees and Street Lighting
- 6<sup>th</sup> Steet Sidewalk Deficiencies
- Pedestrian-oriented Space and Plazas
- Blank Wall Treatment
- Scale
- Massing
- Ground Level Details
- Unique Historic Features
- Restaurants and shops on key streets...animation of downtown

## Historic Masonic Building and Idaho Trust are set back 20 feet or more



- Project does not address or respond to the requirements of the city's Comprehensive Plan
- Downtown Design Guidelines have not been met
- Design Review Committee did not address site-specific information in evaluating how well the project met guidelines and did not exercise their right to discretion to reconcile these facts
- Residents at 609 Sherman are significantly impacted by this project as approved; reduction in property value and decrease in marketability
- Design Review Approval was given based on incomplete, inaccurate or missing information
- Residents and visitors are impacted by unaddressed traffic and parking impacts as well as view corridors and vistas.
- Unaddressed traffic and parking impacts will hurt downtown businesses

## The City Council is asked to:

- Uphold the Appeal
- Require the project developer to present a complete traffic, circulation and pedestrian impact study
- Address and mitigate the traffic impacts created with a revised design for consideration by Design Review
- Move the project entrance and exit to 6<sup>th</sup> Street and provide a waiver to the Design Guidelines
- The revised design must address the shortcomings identified:
  1. Provide a 20-foot setback on Sherman
  2. Provide a 15-foot set back on 6<sup>th</sup>
  3. Minimize obstruction of view corridors
  4. Choose building materials that better match the existing downtown palette
  5. Provide appropriate constraints for the impacts of the proposed rooftop bar/lounge regarding light, noise and nuisance
  6. Provide design relief and animation of the building mass on Sherman and 6<sup>th</sup>.

# APPLICANT PRESENTATION



# MARRIOTT AC HOTELS COEUR D'ALENE





- **August 1, 2023** – Applicant completed project review with City of CDA.
- **October 30, 2023** – Applicant completed mandatory Initial Meeting with City of CDA.
- **January 25, 2024** - Project received unanimous Design Review Commission Approval
- **February 9, 2024** – Appeal filed by Joan Woodard
- **April 16, 2024** – Hearing date set for Appeal – Meeting tabled to modify the ordinance on this active appeal (allow public testimony)
  - Public testimony was heard and considered by the DRC. That testimony is available to the Council on the record.
- **June 4, 2024** – Tonight’s hearing date is 4+ months after the initial approval was received for the application.
- Application is an approved use by right in the Downtown Core (DC), complies with the zoning ordinance standards (no variances), and received approval from the DRC as required in the DC zone district.
- The delay of the appeal has caused significant hardship to the applicant and unwarranted delays in the process. The applicant desires to move forward tonight to avoid further delays.



- B. Appeal on The Record: The Council's review of the decision of the Commission shall be based on the record developed before the Commission.  
No new evidence or materials shall be allowed by any party in the appeals proceedings.
- C. Hearing: The applicant, City staff, the appellant, and their representatives, and members of the public may participate in the appeal hearing.  
Any participant in the appeal may only provide testimony and argument, **based on the established record** concerning the decision of the Commission. The Mayor may establish time limits for each speaker and shall enforce the rules of procedure set out in Municipal Code § 1.11.010.
- D. Burden Of Proof: **The appellant must establish by a preponderance of evidence that an error was made in the decision or that design standards or guidelines were ignored or incorrectly applied, and further that the appellant was prejudiced thereby.** No testimony shall be taken on matters which cannot be modified by the Commission, including, but not limited to, Zoning Code requirements, FAR, building height, density, use, parking, or traffic impacts are not grounds for redress on appeal because they are not design review criteria. Basic zoning standards and allowances embodied within the Code shall be presumed to be correct and are not subject to the appeal. Factual findings by the Commission will be accepted by the Council if they are supported by substantial evidence.

## Downtown Core Design Guidelines:

1. Location of Parking
2. Screening of Parking Lots (N/A)
3. Parking Lot Landscaping (N/A)
4. Sidewalk Uses
5. Width And Spacing of Curb Cuts
6. Screening of Trash/Service Areas
7. Lighting Intensity
8. Gateways (N/A)
9. Maximum Setback
10. Orientation to the Street
11. Entrances
12. Massing
13. Ground Level Details
14. Ground Floor Windows
15. Weather Protection
16. Treatment of Blank Walls
17. Screening of Parking Structures
18. Roof Edge
19. Screening Of Rooftop Mechanical Equipment
20. Unique Historic Features
21. Integration of Signs with Architecture
22. Creativity/Individuality Of Signs

## M.C 17.05.650 DC DOWNTOWN CORE

A. Purpose: It is the purpose of the downtown core district to:

- Encourage private and public investment, attract shoppers and visitors, and appeal to existing and new residents.
- Produce a concentration and a mixture of commercial, office, retail, residential, and public uses within the downtown.

B. Application And Intent:

- This district is envisioned to have the highest intensity uses, especially retail, office, residences, and hotels contained within low rise, mid rise and **high-rise buildings**. Shops and restaurants would be located along key streets. Major public spaces and buildings would anchor the district. Over time, parking would be increasingly located within structures.
- This district is centered in and around the downtown area surrounding Sherman Avenue and has density and development incentives to encourage the revitalization of the core business area.

## 1. Lack of Public Notice and Opportunity to be Heard

### Public Notice

- The project was properly noticed as required by M.C 17.09.315(A). Mailings were sent out, notice published in the newspaper, and posted on the subject property. The DRC meeting was properly noticed as evidenced by the written comments and public attendance at the meeting.

### Opportunity for Public Input

- Statement by Appellant: *“the community had no notice, and no opportunity to be heard or incorporate the concerns of neighbors and the community, until everything except for items within the limited scope of DRC review had already been decided and approved by the staff.”*
- The subject property is zoned Downtown Core (DC) which allows the proposed project use by right. Staff had not issued any project *approvals* at the time of the DRC decision on 1/25/24. The zoning ordinance is the law that dictates what uses and heights may be built on a property.
- The public was allowed to testify at the DRC meeting via written comment and in person prior to DRC rendering a decision.
- Prior meetings with staff are informal meetings only to benefit the applicant/ project to assist in zoning requirements and city process.

## 2. Inadequate or missing information in the Application

- *Neighborhood context*
  - *Photos and view corridors*
- 
- The subject property is surrounded by an 18-story tower to the south and 6-story building to the north. The proposed hotel is a 6-story building that blends with the existing neighborhood context. The Downtown Core zone district is envisioned for mid to high rise buildings centered around Sherman Ave.
  - View corridors are not one of the 22 Design Guidelines and furthermore, the adjacent buildings block view corridors mentioned in the appellant statements.
  - The following slides were provided by the applicant as part of the Design Review Commission meeting.



NORTHWEST CORNER



NORTHEAST CORNER



SOUTHWEST CORNER





EXISTING PROJECT SITE



NW VIEW OF SHERMAN AVE. AND 6TH STREET



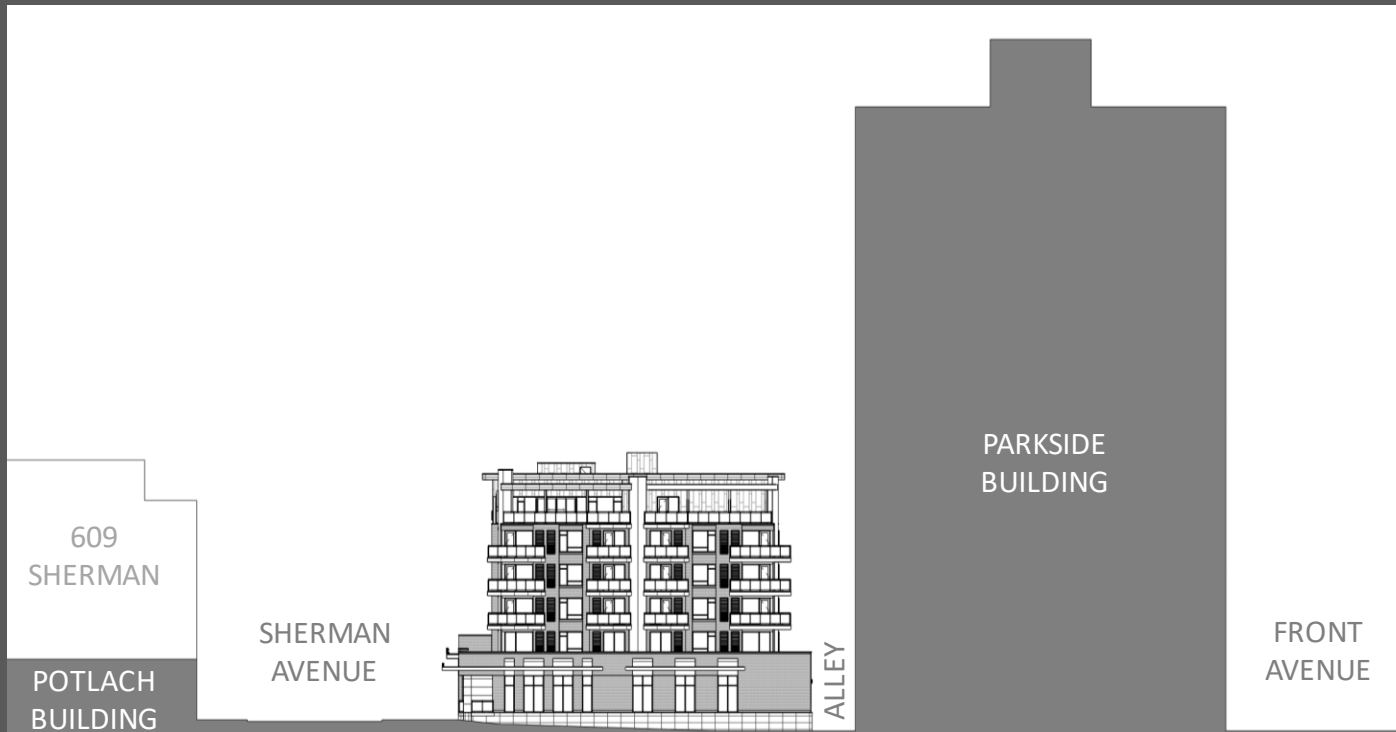
6TH STREET VIEW CORRIDOR TOWARDS LAKE



VICINITY MAP



SHERMAN AVENUE BLOCK MASSING ELEVATION



6TH STREET BLOCK MASSING ELEVATION

CITY BLOCK ELEVATIONS

Sherman Avenue has a mix of low- and mid-rise buildings, which align well with the scale of the plinth of the proposed hotel. The overall mass of the building helps transition from these shorter structures to the high-rise residential buildings on Front Avenue.





1 PHOTO 01  
DR4 12" = 1'-0"



2 PHOTO 02  
DR4 12" = 1'-0"



3 PHOTO 03  
DR4 12" = 1'-0"



4 PHOTO 04  
DR4 12" = 1'-0"



7 PHOTO 07  
DR4 12" = 1'-0"



8 PHOTO 08  
DR4 12" = 1'-0"



9 PHOTO 09  
DR4 12" = 1'-0"



10 PHOTO 10  
DR4 12" = 1'-0"



A PHOTOGRAPHIC STUDY  
DR4 1" = 40'-0"



11 PHOTO 11  
DR4 12" = 1'-0"



12 PHOTO 12  
DR4 12" = 1'-0"



13 PHOTO 13  
DR4 12" = 1'-0"



5 PHOTO 05  
DR4 12" = 1'-0"



6 PHOTO 06  
DR4 12" = 1'-0"



THE  
RICHARDSON  
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PARTNERSHIP,  
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THIS DOCUMENT CONTAINS INFORMATION THAT IS UNCLASSIFIED EXCEPT WHERE SHOWN OTHERWISE. IT IS UNCLASSIFIED DATE 08/07/01 BY 60323 UCBAW/STP. UNCLASSIFIED DATE 08/07/01 BY 60323 UCBAW/STP.

**Providence  
Development**

OWNER

**AC Hotels - Coeur d'Alene**  
602 & 612 E Sherman Ave.  
Coeur d'Alene, ID 83814

PROJECT	
DATE	DESCRIPTION

**Schematic Design**

SITE MAP  
PHOTOGRAPHS

PROJECT #23108  
DATE: 08/07/2019  
CHECKED BY: GWB

DR4  
SHEET #

### 3. Project Approval Decisions Were Made Based on Incomplete Information

- *Design Review Commission should have required a Traffic Study prior to decision.*
- Traffic is not part of the Design Guidelines, and the DRC Approval could not be conditioned a traffic study.
- The Applicant has since completed the traffic study and submitted to the City Engineer for consideration.

#### 4. Ground Level Details not addressed for 6<sup>th</sup> Street

- The ground floor of the building has the most amount of character to provide visual interest to pedestrians including:
  1. Pedestrian-scaled signs to identify the building entry.
  2. Metal canopies above the ground floor storefront windows.
  3. Accent wall sconces on either side of the main entrance.
  4. A decorative concrete plinth to ground the building.
  5. Seasonal plantings along the building to provide visual interest.
  6. The darker brick accents provide a belt course for the building at a pedestrian scale.

The ground level also features an elevated exterior patio at the corner of Sherman and 6<sup>th</sup> Street to further activate this corner.

The Design Review Commission provided a condition of the decision to enhance the concrete plinth along 6<sup>th</sup> Street. The Applicant has consulted with the local art community in CDA for input on enhancements and intends to incorporate a solution to enhance 6<sup>th</sup> Street façade.





GROUND-LEVEL PERSPECTIVE VIEW



GROUND-LEVEL WEST ELEVATION (6TH STREET)



GROUND-LEVEL NORTH ELEVATION (SHERMAN AVE.)

The ground floor of the building has the most amount of character to provide visual interest to pedestrians, including:

- Pedestrian-scaled signs to identify the building entry.
- Seasonal planting in multiple planters against the building along Sherman Avenue.
- Metal canopies above the ground floor storefront windows.
- Accent wall sconces on either side of the main entrance.
- A decorative concrete plinth to ground the building.

The ground level also features an elevated patio at the corner of Sherman and 6th Street to add a further level of detail in this area.





2'-0" TO 10'-0"  
WINDOW ZONE

WEST ELEVATION (6TH STREET)



2'-0" TO 10'-0"  
WINDOW ZONE

NORTH ELEVATION (SHERMAN AVE.)

The building has been designed with many storefront windows along Sherman Avenue and 6th Street that will have clear vision glass into the Lobby, Bar/Lounge, Conference Room, and Corridor spaces (unblocked by shelving).

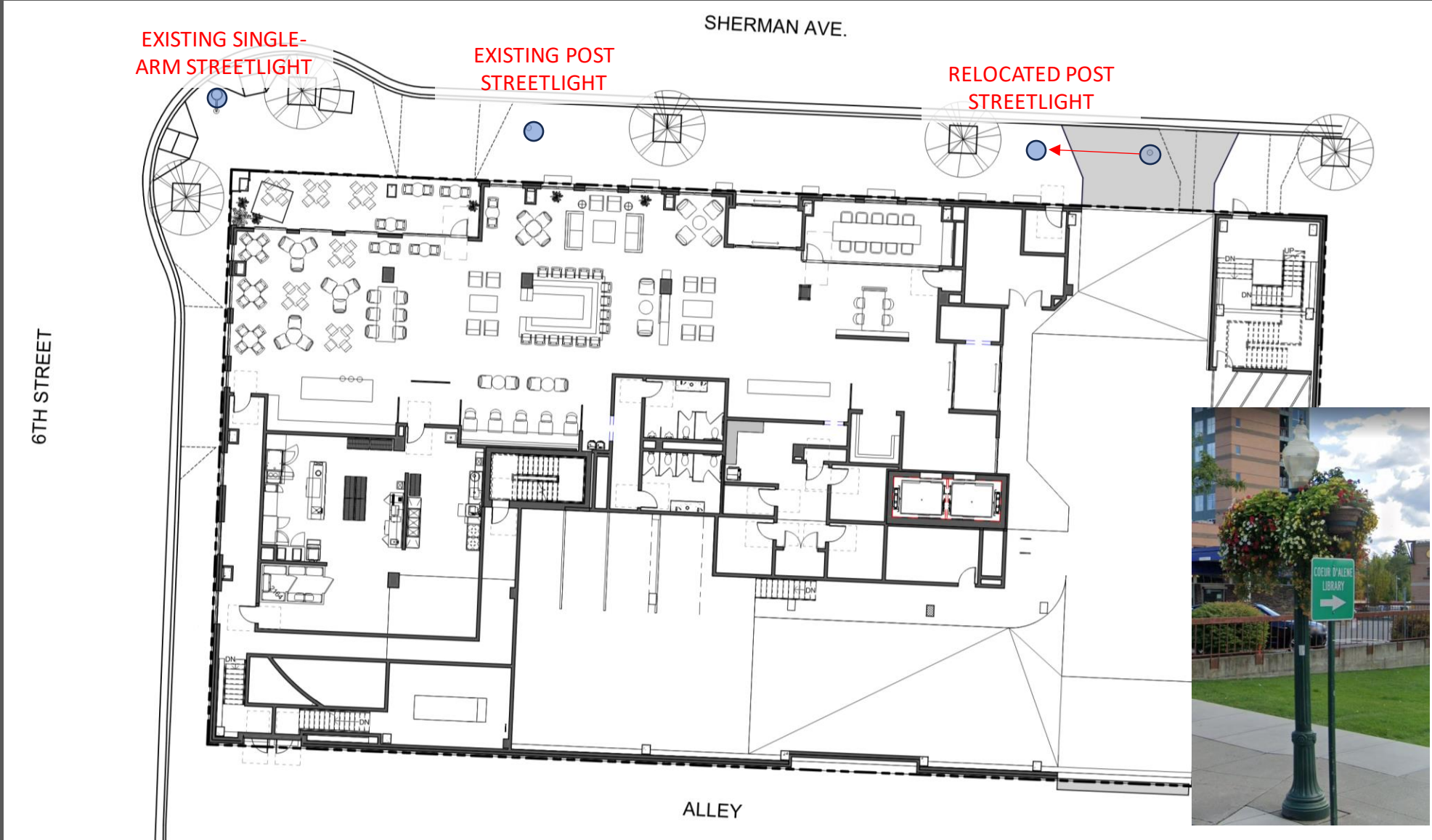
Sherman Avenue has 45% window and glazed door area in the "window zone" of the façade.

6th Street has 26% window area in the "window zone" of the façade.

All ground-floor windows will have a minimum of 60% transparency.

## 5. Unique Historic Features

- Street Trees and Street Lights Cadence
- The proposed development, as evidenced in the materials reviewed at the Design Review Commission, maintains consistency with the street trees and street lighting on Sherman Ave.
  - *The four existing street trees will be replaced with street trees per City standards and will include new 5'x5' tree planting areas around the trees.*
  - *One light will remain in its current location, and the other will be shifted to allow for the new curb cut into the parking structure. There are no existing streetlights along 6th Street.*



STREET LIGHTING

There is one existing single-arm tall streetlight at the corner of Sherman and 6th Street that will remain.

There are two existing post streetlights along Sherman Avenue. One light will remain in its current location, and the other will be shifted to allow for the new curb cut into the parking structure.

There are no existing streetlights along 6th Street.



EXISTING POST STREETLIGHT

## 5. Unique Historic Features (cont.)

- 6<sup>th</sup> Street Conditions – Width Concerns
  - The 6<sup>th</sup> Street sidewalk was a condition on the DRC approval from Engineering.
  - The sidewalk along 6<sup>th</sup> Street will be widened approximately 3 feet at the request of the City Engineer. The width of the sidewalk maintains the necessary clearances and stills allows on-street parking to remain along 6<sup>th</sup> Street.
  - ROW improvements are reviewed during the Site Development Permit process.





EXTERIOR PERSPECTIVE



MASONIC TEMPLE TO THE WEST



609 SHERMAN TO THE NORTH



PARKSIDE BUILDING SOUTH OF EXISTING SITE

The existing site is a lawn-covered dog park, so the project doesn't include any renovation or redevelopment.

As a new construction project, the proposed building relates to the surrounding context through:

- The use of brick as a predominant exterior finish.
- The massing of the building with a base, middle, and top.
- The scale of the building as a steppingstone between the smaller buildings along Sherman Avenue and the high-rise residential Parkside Building.
- The design of the building as a contemporary structure that relates to the primarily modern surrounding architecture.

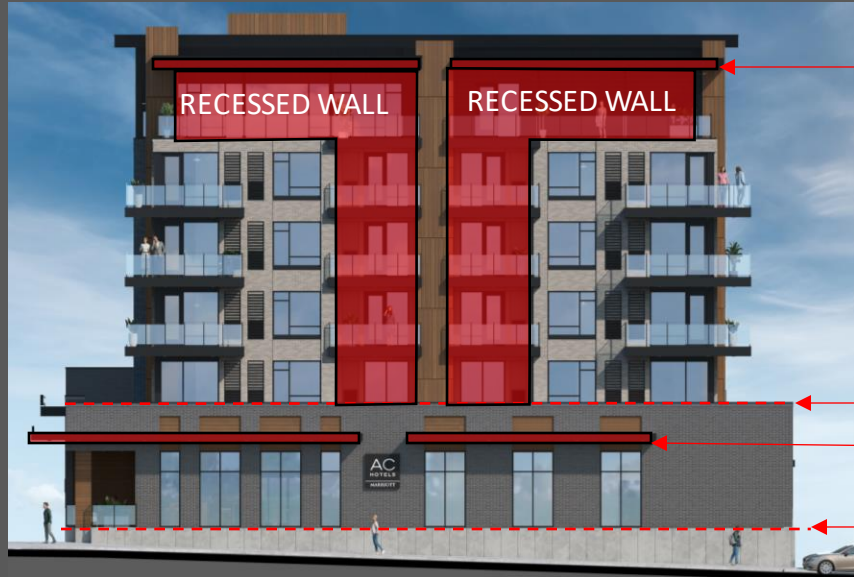
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OVERHANGING ROOF

RECESSED WALL

RECESSED WALL

BRICK BELT COURSE

OVERHANGING ROOF

CONCRETE PLINTH

WEST ELEVATION (6TH STREET)



RECESSED WALL

OVERHANGING ROOF

RECESSED WALL

RECESSED WALL

BRICK BELT COURSE  
OVERHANGING ROOF

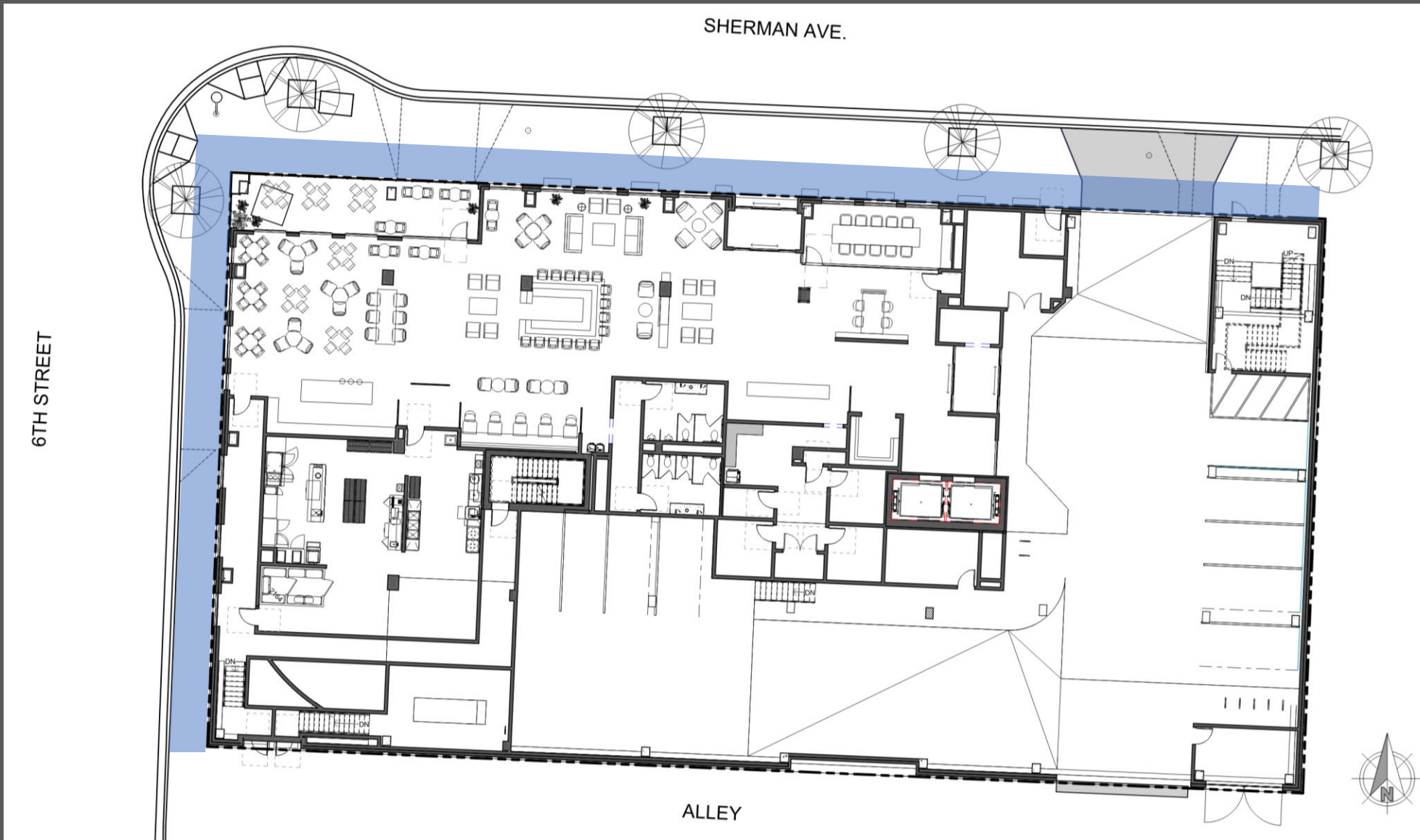
CONCRETE PLINTH

NORTH ELEVATION (SHERMAN AVE.)

The street-facing walls of the building are mostly broken up by windows and doors, but there are additional architectural features that break up the impact of the walls, including:

1. A concrete plinth that varies in height depending on the grade change (from 1'-2" up to 6'-0").
2. A change in brick materials above the ground floor level, acting as a "belt course" for the building.
3. Recesses in the façade at least 2'-0" in depth.
4. Roof overhangs/canopies at the ground floor level and upper roof level that vary from 3'-0" to 5'-0" in depth.

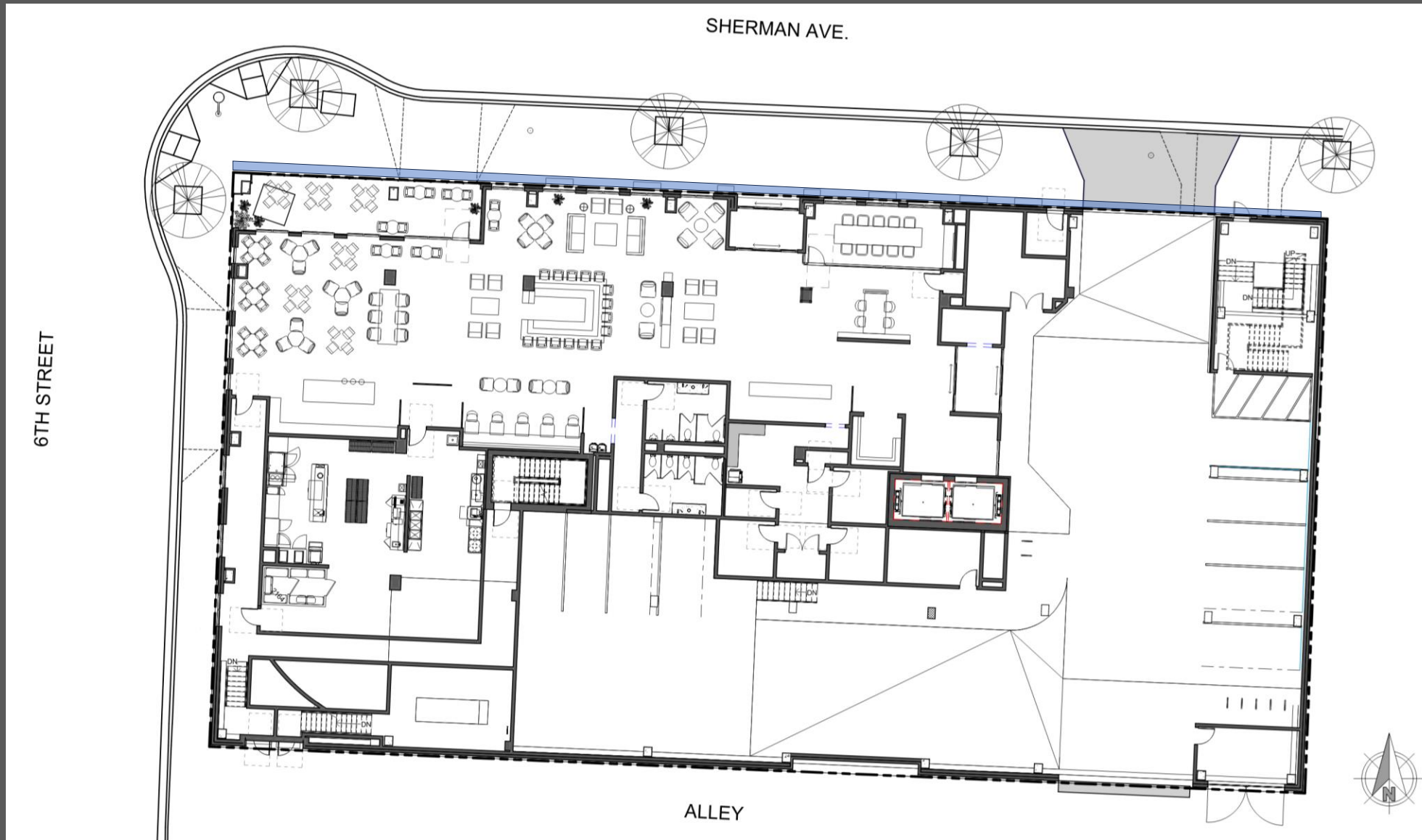
Additional features at the pedestrian level include contrasting wall material and vegetated planter boxes.



CLEAR WALKWAY



The existing sidewalk on Sherman Avenue from the back of curb to the property line is 14.8'. The distance from the new 5'x5' tree planting areas to the property line is approximately 8'-6". A 7'-0" wide clear pedestrian travel area will be maintained.



STOREFRONT AREA  

An 18" wide area between the property line and the pedestrian travel area will be used for planting containers.

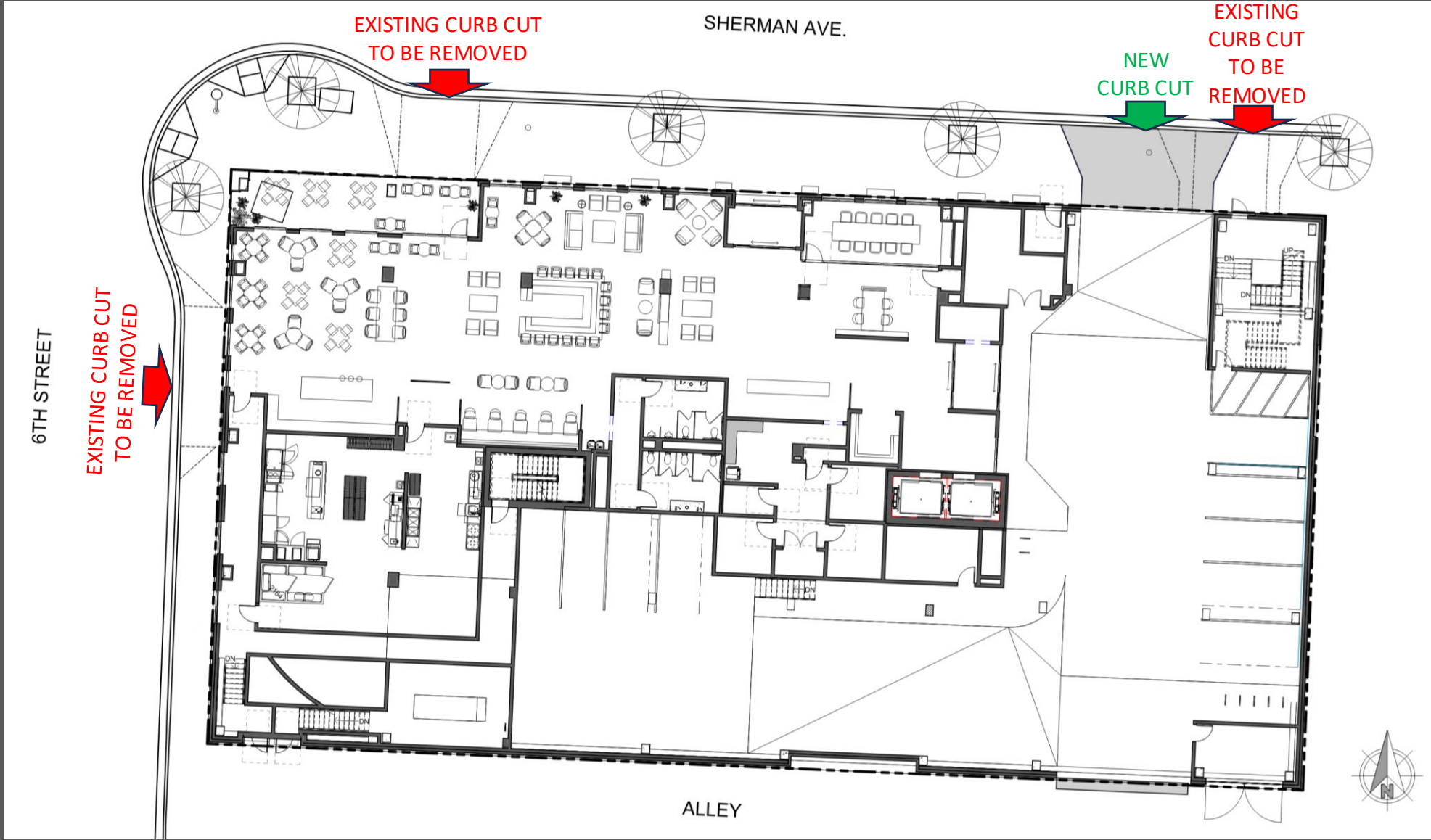
6TH STREET

SHERMAN AVE.

ALLEY







Two curb cuts on Sherman and one curb cut on 6th street currently exist. All three of these existing curb cuts will be removed.

The project only requires one 24' wide curb cut on Sherman.

The sidewalk pattern and material will carry across the driveway.

This project will not be sharing a driveway as it is not feasible.

No Curb cuts are being proposed on the Pedestrian-Oriented 6th Street.

MAIN (STREET) LEVEL PLAN



BUILDING LIGHTING

The majority of the exterior building lighting will be recessed lights in the roof canopies at the ground floor level to provide light to pedestrians, at the guestroom balcony roofs to provide light to the guests, and at the upper roof deck to highlight the building corner. Also, an accent strip of soffit lighting is fully-shielded by the roof overhang.

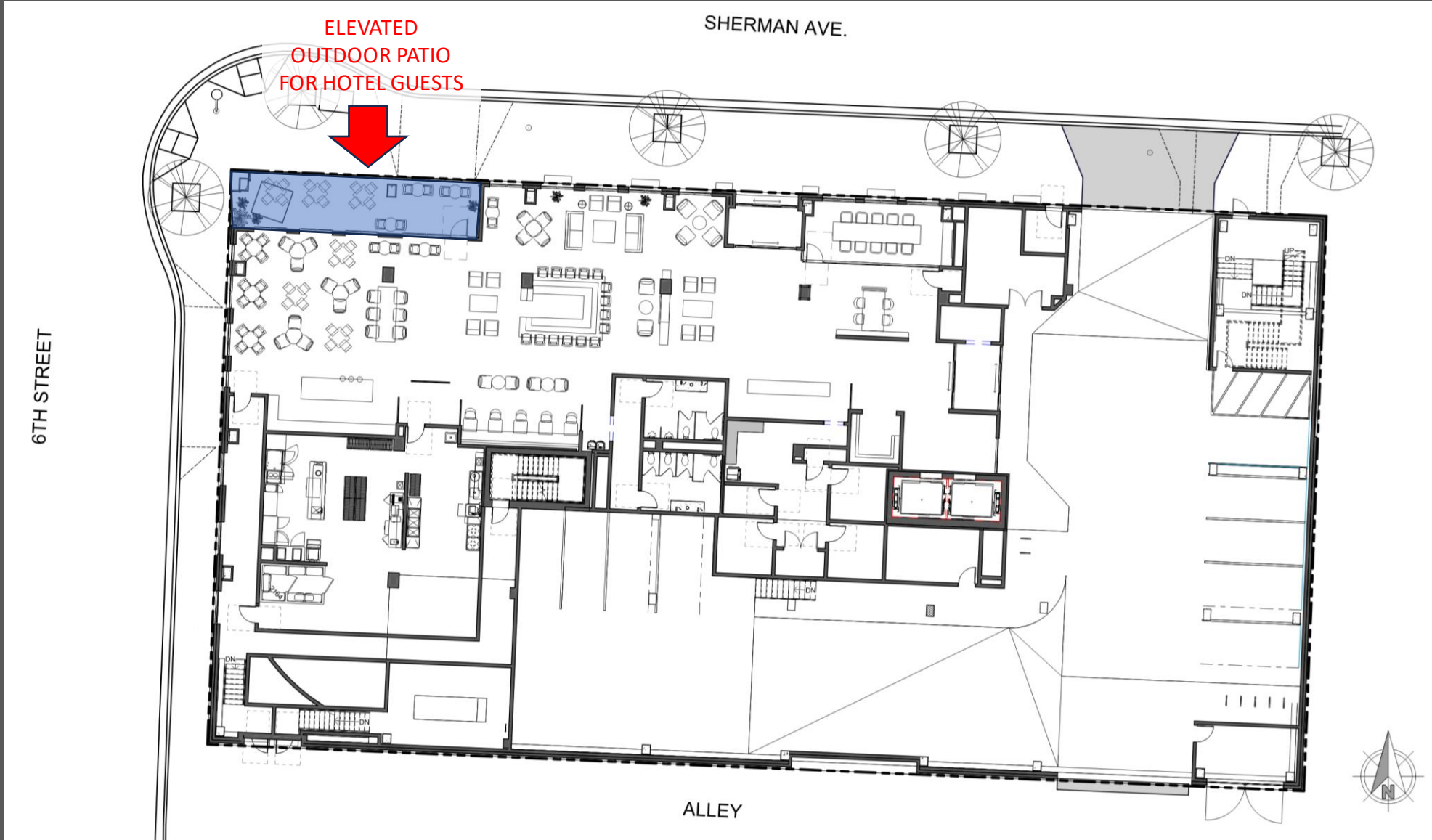
Fully-shielded wall sconces will be added on either side of the main entry doors to highlight the entry.



PROPOSED WALL SCONCE

EXTERIOR RENDERING AT DUSK





The street level façade along the Pedestrian-Oriented 6th Street is set up to the back of the sidewalk along the property line.

A portion of the project on the corner of Sherman and 6th Street has a dining patio for the use of hotel guests but it has a base structure that extends out to the sidewalk.

MAIN (STREET) LEVEL PLAN



MAIN ENTRANCE

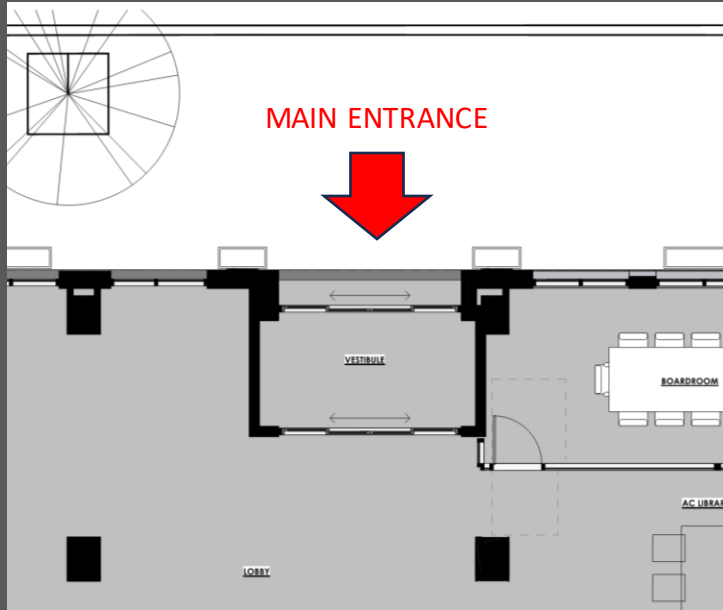
OUTDOOR PATIO

The proposed building is oriented to Sherman Avenue.

The building façade along Sherman incorporates numerous windows as well as an entrance canopy and signage. The façade along 6th Street incorporates windows.

The primary building entrance faces Sherman and is centered in the building façade.

An outdoor patio at the ground level activates the street corner at Sherman Avenue and 6th Street.



MAIN ENTRANCE FLOOR PLAN



MAIN ENTRANCE PERSPECTIVE VIEW

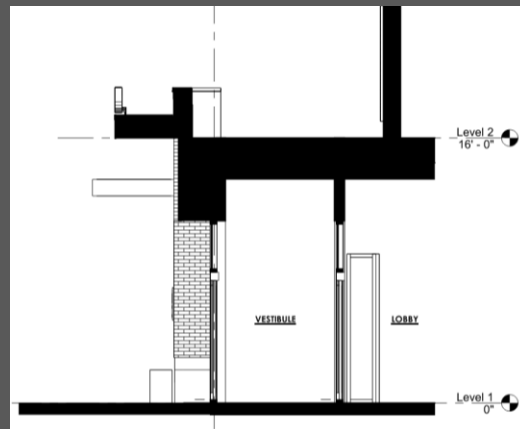
The main building entrance is centered on the façade along Sherman Avenue and welcomes pedestrians with an overhanging canopy as well as a recess in the main building wall.

Both the canopy and the recess provide added weather protection for pedestrians.

These features, along with clear signage, help identify this visually prominent entrance.



MAIN ENTRANCE ELEVATION (SHERMAN AVE.)



MAIN ENTRANCE SECTION





TOP

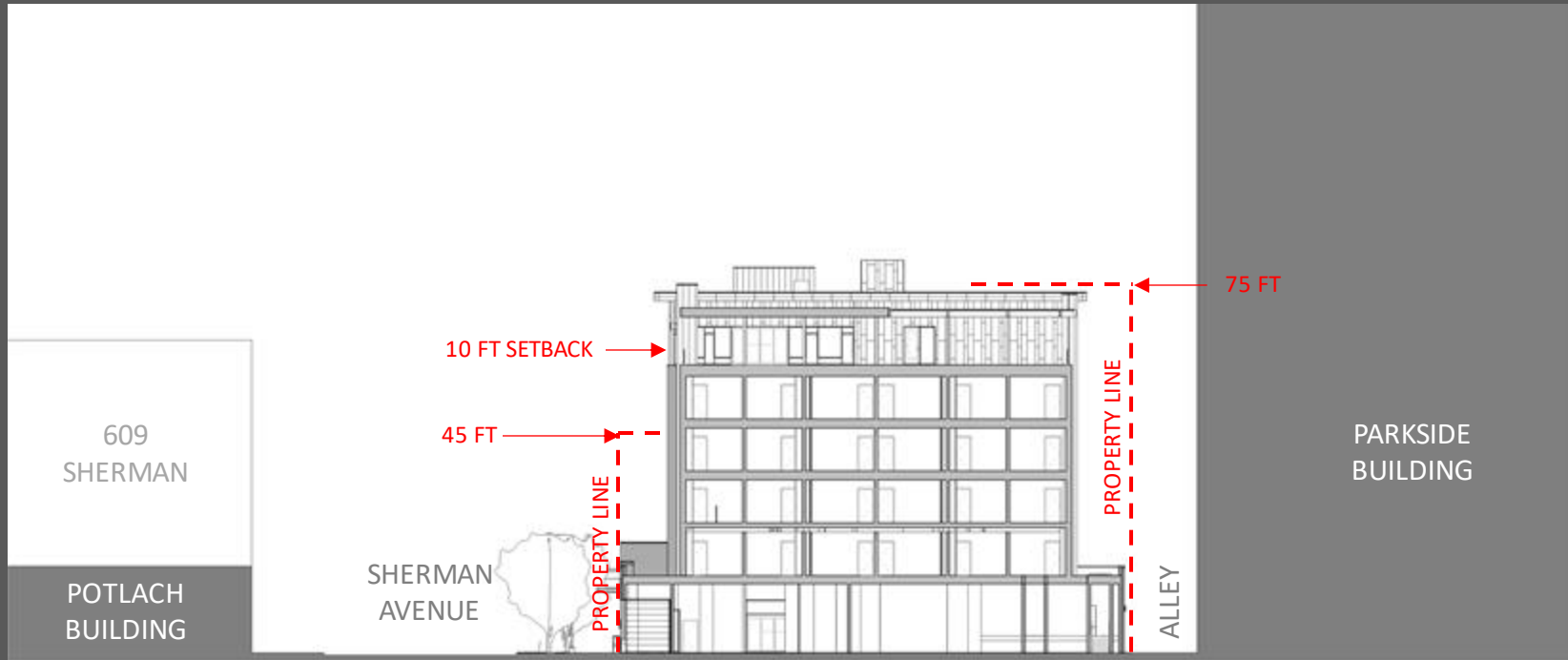
The top section of the building is distinguished by overhanging roofs, an open roof deck with trellis, and additional windows. The main material is a dark metal panel, with vertical wood siding.

MIDDLE

The middle section of the building has a regular pattern of guestroom windows surrounded by dark and light color brick veneer. Also, there are some dark metal panels and vertical wood siding to connect the base to the top.

BASE

The base of the building features a large amount of storefront glazing and canopies to define the ground level. The finish is mainly a dark grey brick veneer accented with horizontal wood siding with a decorative concrete plinth.



SHERMAN AVENUE MASSING SECTION



6TH STREET MASSING SECTION

BUILDING BULK

The base of the building aligns with the property lines of the lot, but steps back above the ground floor level to allow for the required 10-foot setback over 45 feet above grade.

The only parts of the building that extend past these setbacks are roof overhangs and balconies.

The only part of the building that is taller than 75 feet is the stair over-run, the elevator penthouse, and mechanical equipment screening, which does not exceed 15 feet above the roof deck.



PROVIDENCE DEVELOPMENT  
AC HOTELS - COEUR D'ALENE  
602 & 612 E SHERMAN AVE., COEUR D'ALENE, ID 83814



1 WEST ELEVATION - 6TH STREET  
MASSING  
DR12 3/64" = 1'-0"



2 NORTH ELEVATION - SHERMAN  
AVE. MASSING  
DR12 3/64" = 1'-0"



THE RICHARDSON DESIGN PARTNERSHIP, L.L.C.  
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Salt Lake City, Utah 84102  
P: 801.355.6868  
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Providence Development

OWNER:

AC Hotels - Coeur d'Alene  
602 & 612 E Sherman Ave.  
Coeur d'Alene, ID 83814

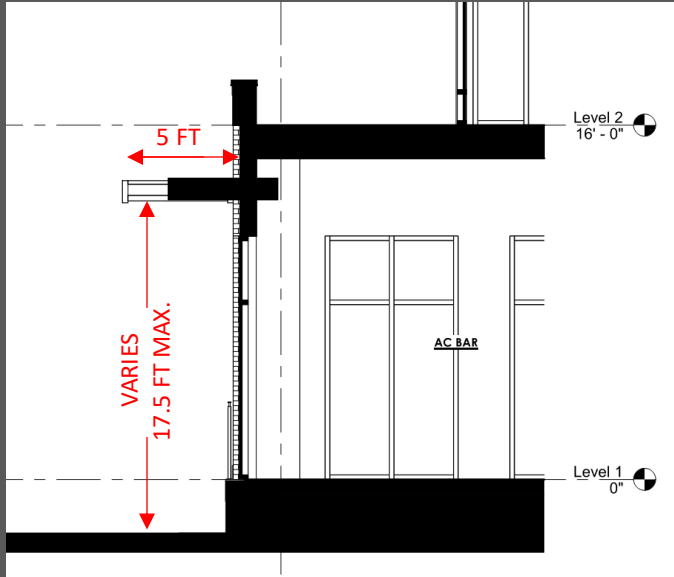
PROJECT:

DATE	DESCRIPTION

Schematic Design

MASSING ELEVATIONS

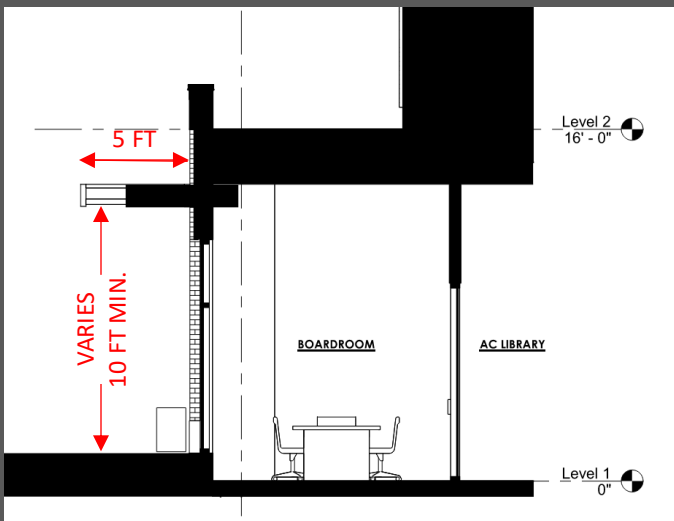
DR12 SHEET 11  
PROJECT #13138  
CHECKED BY: OWNER



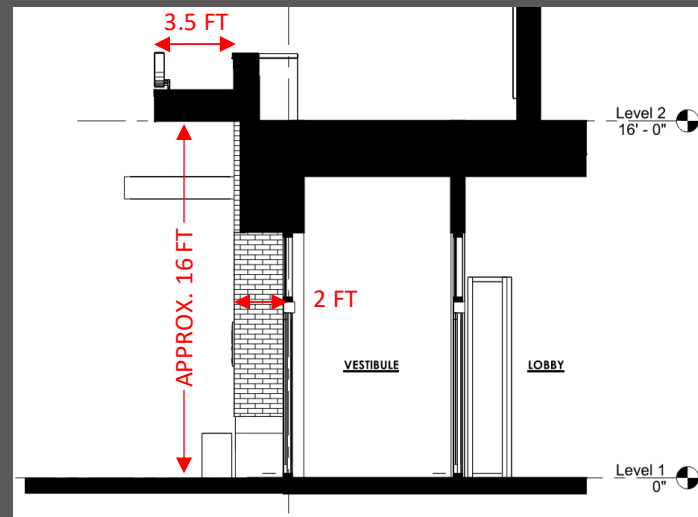
6TH STREET CANOPY SECTION



MAIN ENTRANCE PERSPECTIVE VIEW



SHERMAN AVENUE CANOPY SECTION



MAIN ENTRANCE CANOPY SECTION

The building is designed with 5-foot-deep canopies around nearly the entire length of the Sherman Avenue and 6th Street façades for weather protection.

The exception is at the main entry, which has a shorter canopy at 3.5 feet in depth. However, a recessed entry provides additional protection. This canopy is also 16 feet high, to accentuate the entrance to the building.

The main canopies maintain a consistent level height around the building, but due to the sloping grades, the height of the canopy varies. The minimum height above grade is approximately 10 feet.

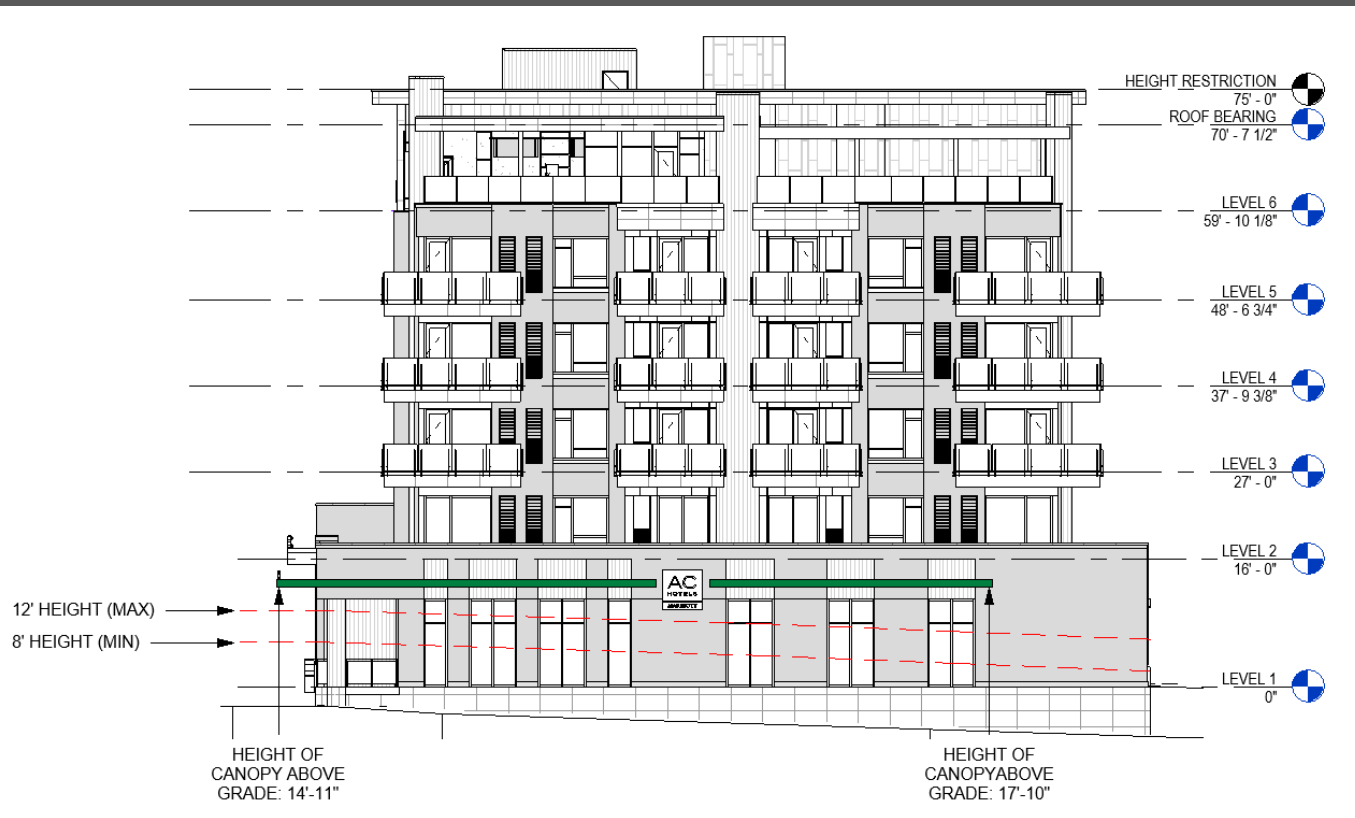
The canopy will have a metal frame finish, with a wood plank soffit. These canopies will also have recessed downlights to provide lighting under the opaque covering.

DESIGN DEPARTURE

Starting near the northeast corner of our project, along Sherman Avenue, the canopy height starts at 9 feet 11 inches above the sidewalk, well within the design guideline range. To maintain clearance for any pedestrian and vehicular safety signage that will need to be suspended from the canopy above the parking garage entrance, the height of 9 feet 11 inches established. As the canopy extends west along Sherman Avenue, the sidewalk slopes down at an average of 2.8%. Due to this slope, the height of the canopy increases to 14 feet 11 inches at the northwest corner of the project, which is 2 feet 11 inches above the maximum allowable height in the guideline.



SHERMAN AVENUE FACADE

6<sup>th</sup> Street Facade

## DESIGN DEPARTURE

1. The canopy as designed meets the intent of the design guideline by providing pedestrians with cover from rainfall and snow.
2. The canopy as designed remains horizontal along the facade and does not change height (except at the hotel's main entrance), aesthetically it does not have a detrimental effect on nearby properties or the city as a whole.
3. The canopy as design covers 94% of the façade along Sherman Avenue and 81% of the façade along 6th Street. The Design Guidelines do not dictate how much of the façade needs to be covered with Canopy. With over 85% of the Sherman and 6th Street facades covered with canopy, this project offers a significant improvement over what otherwise could have been built under minimum standards and guidelines.
4. The Canopy as designed fits aesthetically with the whole of the building. The strong horizontal plane of the canopy helps define the base of the building which is an important aspect of the design guidelines.
5. If the canopy were to step down with the grade along Sherman Avenue and 6th Steet to maintain the maximum and minimum height requirement of the guidelines, the canopy would eventually intersect the exterior windows of the project on the main level and would adversely affect the interior daylight experience from individuals inside the project. From the exterior, the stepping down of the canopy along the slope of the sidewalk would adversely affect the aesthetic quality of the architecture.

# CONSENT CALENDAR



MINUTES OF A REGULAR MEETING OF THE CITY  
COUNCIL OF THE CITY OF COEUR D' ALENE, IDAHO,  
HELD AT THE LIBRARY COMMUNITY ROOM

May 21, 2024

The Mayor and Council of the City of Coeur d' Alene met in a regular session of said Council at the Coeur d' Alene City Library Community Room on May 21, 2024, at 6:00pm., there being present the following members:

James Hammond, Mayor

Woody McEvers                    ) Members of Council Present  
Christie Wood                    )  
Dan Gookin                        )  
Dan English                       )  
Amy Evans                         )  
Kiki Miller                         )

**CALL TO ORDER:** Mayor Hammond called the meeting to order.

**INVOCATION:** Chaplain Steve Slover of the Salvation Army led the Invocation.

**PLEDGE OF ALLEGIANCE:** Councilmember McEvers led the pledge of allegiance.

**PRESENTATIONS:** Mayor Hammond read the Proclamation declaring May 18-24, 2024 as National Safe Boating Week in support of the North American Safe Boating Campaign and the start of the year-round effort to promote safe boating.

**PUBLIC COMMENTS:**

Deb Mitchel, Coeur d' Alene, thanked the Council for bringing the moratorium on demolition to tonight's agenda. She announced that the museum will do a walking tour of Fort Sherman on Saturday, May 25, at 11:00 a.m.

Linda Coppess, President and CEO of the Coeur d' Alene Regional Chamber, thanked the Council for the partnership and support on bringing together the community's 4<sup>th</sup> of July celebration. She thanked the staff for planning meetings and stated that it gets more efficient and smooth each year. Mayor Hammond said that the 4<sup>th</sup> of July Parade is a challenge, and he thanked the Chamber for their efforts as well.

Richard Dance, Hayden, provided a brief introduction on the electric and gas audit for the City that is part of tonight's agenda. He said that the City pays about \$300,000 per month in electricity and gas, with 20 invoices coming from Avista and 15 from Kootenai Electric. Mr. Dance noted that 90% of the City's electricity and gas comes from Avista and 10% from Kootenai Electric. He pointed out that in the Avista invoices, there are items such as area lights and city tax while in

Kootenai Electric invoices, there are items such as service availability, power cost adjustments, demand charges, power factors, franchise fees, minimum charge difference, and a wood pole. He stated that TRI Utility Cost Reductions has 35 years of experience and seeks to get the lowest rate for electricity and gas for the City of Coeur d'Alene based on industry usage and demand.

Joan Woodard, Coeur d'Alene, submitted a document regarding design review procedures which was taken from the city website under Informational Documents. She pointed out that Section 3, titled Submittal Requirements, outlined that there will be three meetings with the Design Review Board. She said that while she recognizes that the code has changed, the public assumed that this city document will be the standard for the proposed Marriott project. She added that most residents will not read the code but rather look at information documents which outline a very clear and logical process. Ms. Woodard explained that in the case of the Marriott Hotel, it was only presented at only one Design Review Board meeting where the public could have assumed that it will be presented three times with an opportunity for the applicant to address the concerns of the residents before the Design Review Board made a final recommendation. She added that there are many factual deficiencies in the application.

Walter Burns, Coeur d'Alene, spoke in support of the moratorium on demolition in the downtown core, downtown overlay, and downtown east overlay. He stated that the Historic Preservation Commission is working on a review of the downtown core development standards and infill overlay standards with a task group composed of city staff, representatives from the Historic Preservation Commission, Planning Commission, Design Review Commission, Downtown Association, and others. He added that the group will have their initial meeting tomorrow. Mr. Burns shared that the Historic Preservation Commission is also studying ways to amend the current demolition permit process to allow review of demolitions of historic structures and to provide a mechanism for discussion with property owners. He pointed out that the demolition moratoria would allow the Commission the time to come up with responsive and effective recommendations to the Council.

#### **DISCUSSION ON DEMOLITION MORATORIA:**

Councilmember Miller noted that there is enough reason that an action item regarding enacting a moratorium on demolition permits be brought to the June 18, 2024 Council meeting. She explained that there are plausible reasons for a moratorium such as: the task group for the downtown core review and design needs enough time to come up with recommendations; a demolition "permit" does not exist because the code only follows the Idaho Building Code process, hence enough time is needed to discuss if change is warranted; and there is also a need to put together answers in order to address public anxiety and their desire for preservation efforts as more buildings are coming down or being threatened. She stated that the Council has to agree that this item will be included in the June 18 meeting agenda. As such, the staff will be directed to create a report that outlines the interim ordinance to include defining the area or buildings that the moratorium will apply to; describing creating a permit review process; consideration of adding a demolition code; describe consideration of voluntary historic overlays; and include requirements of the moratorium that the Council needs to know should they pass it. Councilmember Miller said that if the motion passes on June 18, then the Council will need to state the reason for initiating the moratorium and approve the outline of the ordinance that will require staff to execute the plan

within six months or one year which addresses the issues causing imminent peril to public health, safety, or welfare that will be resolved within the moratorium period. Councilmember English said that he supports the effort to bring the moratoria forward as an agenda item but raised that he is concerned with the timing that it may affect priorities like the NIC overlay district. City Attorney Randy Adams replied that they are working on a draft ordinance with input from Councilmember Gookin. He added that it will be presented to the public and NIC for comments and eventually to the Council for input and direction. Councilmember Miller stated that the demolition moratoria will be a three-phase process and the first step is to have it placed on the agenda. Councilmember Gookin inquired on the ad-committee on downtown overlay, and Planning Director Hilary Patterson explained that it is a working group that will look at the downtown development regulations as well as infill regulations and design guidelines. She said that it is composed of representatives from the Historic Preservation Commission, Design Review Commission, Planning and Zoning Commission, Downtown Association, and two community members. She added that the first meeting will be tomorrow, May 22. Councilmember Gookin stated that he would like to have Councilmembers representation in the working group. Councilmember Wood stated that it is a good idea to put the demolition moratoria as an action item in the June 18 meeting. Councilmember McEvers said that he agrees with Councilmember Miller to bring the demolition moratoria forward; however, he inquired on the definitions of historical structure, imminent peril, and mental health impact. Councilmember Miller clarified that the legal language is an imminent peril to public safety and health. Mayor Hammond explained that the issue for now is whether to put the demolition moratoria on the agenda, but it doesn't necessarily mean that the Council is going to pursue a moratorium, but it only means that further discussion will occur. He added that there is no guarantee that it will be for the June 18 meeting as there might be other priorities.

## **ANNOUNCEMENTS:**

Councilmember Evans thanked the Pedestrian Bicycle Committee members for planning a great bike to work week event last week. She also thanked the citizens who joined the bike to work event last Friday, May 17. She also made several announcements: June 1 is National Trail Day and will be celebrated by Tubbs Hill volunteers building a new trail from the Museum to the main loop trail on Tubbs Hill from May 30 to June 1, 2024; nominations for the Mayor's Awards in the Arts is now open and due on August 30, 2024; and the application process for the ArtCurrents is also open with deadline on July 1, 2024.

Councilmember Wood thanked the Fire Department for the demonstration of the capabilities of the new fire boat with the Tubbs Hill Foundation members.

Councilmember English announced that he will be co-facilitating a six-weeks series on promoting civil engagement within the community which will be every Saturday at the Community Library from 3:00 p.m. to 4:30 p.m. starting June 1.

Councilmember Gookin provided updates on his requests from staff during the April 16 Council meeting: on the budget committee, he met with the Mayor and Councilmember McEvers, and it will be moving forward with some involvement from the public; the billboard amendments will be discussed in the next GS/PW meeting; the parking requirements and height restrictions review will be discussed in the July 16 Council meeting; NIC rezoning will be discussed in the July 2<sup>nd</sup>

Council meeting; the review of the design guidelines, rules and ordinances will take some time and possibly discussed in November; and the amendment on the design review code that would allow for public comment is part of the agenda of tonight's Council meeting.

Councilmember McEvers announced that there will be a big demonstration event, food and fun music at the Skatepark on Saturday, May 25, starting at 11:00a.m.

**CONSENT CALENDAR:**

1. Approval of Council Minutes for the May 7, 2024 Council Meeting.
2. Approval of Minutes from the May 13, 2024 General Services/Public Works Committee Meeting.
3. Setting of General Services/Public Works Committee Meeting for **Tuesday**, May 28, 2024, at 12:00 noon.
4. Approval of Bills as Submitted
5. Approval of Financial Report
6. Setting of an appeal hearing for June 4, 2024: For an appeal made by Joan Woodard of DR-1-24AA; CDA Hotel, LLC (Mariott Hotel) located at 602 & 612 E. Sherman Avenue.
7. **Resolution No. 24-041-** A RESOLUTION OF THE CITY OF COEUR D'ALENE, KOOTENAI COUNTY, IDAHO, APPROVING THE FOLLOWING: TWO UTILITY EASEMENTS FOR WATER LINE AND TANK ACCESS; SIGNATURE AUTHORITY FOR THE NEW FINANCE DIRECTOR/CITY TREASURER; PURCHASE OF REPLACEMENT LAPTOPS FOR POLICE DEPARTMENT; REJECTION OF ALL BIDS FOR THE COMPOST FACILITY MAINTENANCE PROJECT; A MEMORANDUM OF UNDERSTANDING WITH THE COEUR D'ALENE REGIONAL CHAMBER OF COMMERCE FOR THE 4TH OF JULY PARADE AND FIREWORKS SHOW; AND A CONTRACT WITH VERDIS FOR A GENERATOR UPGRADE AT FIRE STATION NO. 1.

**MOTION:** Motion by McEvers, seconded by Miller to approve the Consent Calendar as presented, including **Resolution 24-041**.

**ROLL CALL:** Evans Aye; Miller Aye; McEvers Aye; Gookin Aye; English Aye; Wood Aye.  
**Motion carried.**

**A QUASI-JUDICIAL PUBLIC HEARING FOR ITEM ZC-1-24; GS4 PROPERTY, LLC, IS REQUESTING A ZONE CHANGE FROM A NC (NEIGHBORHOOD COMMERCIAL) TO C-17; FOR PROPERTY LOCATED AT: SE CORNER OF THE INTERSECTION OF 15TH STREET AND BEST AVENUE.**

**STAFF REPORT:** Associate Planner Mike Behary noted the applicant and owner, GS4 Property LLC, is requesting approval of a zone change from NC (Neighborhood Commercial) to C-17. He said that the legal notice was published in Coeur d'Alene Pres on May 4, 2024 and all the required notifications have been made. He stated that the subject property is .93 acres, vacant, and located

at the southeast corner of the intersection of 15<sup>th</sup> Street and Best Avenue. He added that the site is adjacent to two duplexes and one single family dwelling that are located in the County to the east while to the south is a multi-family apartment complex that is located within the city limits. He stated that there is a gas station on the northwest corner of intersection of 15<sup>th</sup> Street and Best Avenue that is zoned C-17. Mr. Behary explained that the applicant has indicated that if this zone change request is approved, then he intends to build a four-pump gas station with a mini mart and a 2,500 sq. ft. quick serve restaurant. In order to address neighborhood compatibility, he said that the applicant proposed the following items: low profile signage, no LED reader boards, fuel filling will be limited to four pumps, electric car charging potential, and fuel canopy lights will be turned off at 11:00 p.m. Mr. Behary stated the following findings: the subject property is located within the existing city limits and it is designated in the mixed-use low place type; the application was routed to the City Departments for Engineering, Streets, Water, Fire, Police, Parks, and Wastewater for review and each department has indicated that there are adequate public facilities and public utilities available to serve the proposed zone change request; there are no topographical or physical constraints that would make the subject property unsuitable to change the zoning from NC to C-17; and the City Engineer has indicated that the four pump gas station and market will generate 133 morning peak hour trips and 153 evening peak hour trips; however, many of these trips would be bypass rather than diverted trips and unlikely to affect the traffic on 15<sup>th</sup> Street. Mr. Behary pointed out that the Planning and Zoning Commission considered this zone change request at their regularly scheduled hearing on January 9, 2024 and in a unanimous vote of 7-0, they recommended that City Council deny the zone change request from NC to C-17 because the proposed development would negatively affect the surrounding neighborhood in terms of increased traffic, non-residential noise, and light which would be greater than the uses permitted in the NC zone. He mentioned that the Planning and Zoning Commission found that the zoning is incompatible with the surrounding zoning and uses. He pointed out that other than the commercial use to the northwest, everything else to the north, south, east and west is residential.

**DISCUSSION:** Councilmember Gookin asked if the proposed project did not include a gas pump, would it still be considered under NC, and Mr. Behary replied that the applicant will be allowed to build those uses that are allowed under NC such as retail sales and market. Councilmember Wood asked about height restrictions on anything that will be built in the property, with Mr. Behary stated that it will be 32 feet. Councilmember Evans asked if a restaurant is allowed in a neighborhood commercial, and Mr. Behary explained that it would be allowed but not the gasoline services. Councilmember McEvers pointed out that in 1982 there was a zone change from R-12 to C-17. He asked for details on the R-12, to which Mr. Behary explained that R-12 is single family and duplex housing. Councilmember Evans inquired about the hours of operation and what is allowed in NC versus C-17. Mr. Behary stated that for C-17, there is no restriction on the hours of operation while NC is restricted from 6:00 a.m. to 10:00 p.m.

**APPLICANT TESTIMONY:** Rex Anderson, the Architect for the project, stated that when the owner, Andy Singh, purchased the property he approached Planning Director Hilary Patterson and Mike Behary to discuss possible options for zone change and proactive measures on how to be successful in building a gas station on the said property. He added that he and Mr. Singh spent a lot of time in creating a design that is responsive to the neighborhood and illustrated their intended design under C-17 as well as the NC zoning. He stressed that they even invited the neighbors from the surrounding area for a meeting to receive their inputs but no one showed up. Mr. Anderson



shared that Mr. Singh also owns a store at Atlas and Seltice which emphasizes the neighborhood market. He stated the following reasons why the NC zoning will not work at the signalized intersection of 15<sup>th</sup> and Best Avenue: NC has only been assigned to mostly single family homes that have converted to office space; no other NC lots in Coeur d'Alene are on signalized corners or intersections; all other developed NC lots are less than 1/3 of an acre; all other NC zoning is abutting single family residential; and NC zoning requires parking in rear which puts lights and vehicle noise adjacent to duplexes abutting residential. He said that they concluded that 15<sup>th</sup> and Best does not conform to any existing NC zoning. Mr. Anderson explained that the property at 15<sup>th</sup> and Best is still vacant today because there are too many restrictions in NC zoning, the size of the lot at 15<sup>th</sup> and Best is larger than any other NC zoned property, and the lack of feasibility with NC zoning at this location. According to Mr. Anderson, their NC zone design would be subdividing the property into two lots: they would build four 1,100 sq.ft. retail suites on one lot and then on the second lot will be two 4,000 sq.ft. markets. For the potential C-17 zoning lot development, Mr. Anderson showed a 4,600 sq.ft. market and a small fuel island, with amenities such as expanded grocery at supermarket prices, smoothies and fresh squeezed juices, produce, small hardware selection, coffee stand inside but no drive thru, and upscale landscaping and building that fits into the neighborhood. He added that Mr. Singh has been adamant about incorporating the bike lane, landscaping buffer, trees, and low-profile design. He said that in order to resolve the neighbors' complaints during the planning and zoning meeting as well as the recommendations of the city staff, their proposed resolution include: the store will be blocked by extra trees and landscaping buffers on all four sides; low visibility fuel island and trees on all sides; opening hours will be from 6:00 a.m. to 10:00 p.m.; all exterior lighting will be turned off after closing; building height will be kept to a maximum of 20 feet; any further stipulations or conditions of a zone change will be agreed to; and they are willing to roll these conditions into a development agreement with the City. Mr. Anderson pointed out that they applied for a C-17 zoning because of the following reasons: there are many C-17 zoned lots already in the vicinity; this has been endorsed by the Comprehensive Plan; it is located at a high traffic intersection (15<sup>th</sup> St. and Best/Appleway) less than 1 mile from I-90 on a major arterial and is a highly walkable area which classifies the lot as "mixed use low"; and this 1 acre lot is compatible with mixed use low in the Comprehensive Plan-mixed use low designation is zoned C-17 and C-17L, NC and CC zoning districts. He said that there are several additional C-17 lots that are also in the area along Best Avenue.

**DISCUSSION:** Councilmember Gookin inquired on the difference between the NC and C-17 options that he presented, with Mr. Anderson replying that it is the gas and that in the NC zoning, parking has to be at the back. Councilmember Wood inquired on the details of their efforts to meet with the neighborhood, to which Mr. Anderson narrated that Mr. Singh went door-to-door as well as mailed out options in order to get feedback from the community. He stated that Mr. Singh rented a conference room at the Coeur d'Alene Inn, but nobody showed up in the meeting. He added that Mr. Singh is open to conditional zoning. Councilmember McEvers asked if they are willing to commit to a development agreement, and Mr. Anderson confirmed, noting that is what was presented which is also in response to the feedback they got from the community. Mr. Anderson distributed supplement materials to the Council composed of their presentation and petition letters from residents in support of the zone change.

**PUBLIC TESTIMONY:** Mayor Hammond read the rules for the Quasi-Judicial Hearing and the Clerk sworn-in those who will testify. The Mayor opened the public testimony portion of the meeting.

Tod Hornby, Coeur d'Alene, said that he is not in favor of having a gas station because there is already a gas station in that corner, and they don't need another one. He stated that they were not notified of the meeting.

John Thomas Wilson, Coeur d'Alene, said that he felt threatened with the two renderings that were presented. He stressed that he did not get the notice of Mr. Singh's meeting as well as several others in the neighborhood. He asked the Council to put no gas pump as a condition on the C-17 zoning.

Anne Wilson, Coeur d'Alene, said that the gas station is a bad idea and there will be a lot of traffic and safety concerns.

Mark Carlton, Coeur d'Alene, stated that he is against the zone change because there will be increase in traffic. He pointed out that they are 5 minutes away from Costco where they can get gas and food so there is no community need that the proposed zone change is trying to address.

Jenny Schonhardt, Coeur d'Alene, said that she received an invitation to the meeting at Best Western on May 7 at 6:00 p.m. but when they went there, they were told that the meeting was from 4:00 – 6:00 p.m. She also mentioned that there is already so much traffic at 15<sup>th</sup> Street.

Jeff Jager, Coeur d'Alene, stated that he is concerned with the light and noise pollution that will have a negative impact on the wildlife in the lake that is near the proposed zone change. He added that it will also affect the value of his property. He asked the Council to postpone rendering a decision until they could consider an environment impact study.

Joe Archamdao, Coeur d'Alene, suggested to leave the zoning as it is as NC. He said that Mr. Singh can build his business without a gas station.

DL Mesbah, Coeur d'Alene, stated that he is neutral. He said that he agrees on the environment impact study but believes that the traffic is not really the issue because traffic increase will still happen no matter what is developed in that area. He added that the problem is that there was no proper communication with the neighborhood.

Jim Myers, Coeur d'Alene, said that he is against the gas station which will cause gas tankers and delivery trucks to be increased in the neighborhood.

Cathy Moehling, Coeur d'Alene, explained that she objects to the proposed zone change because what has been proposed is not in response to the needs and wants of the community as they have already existing plenty of options for market and fueling in the area. She said there is no need for additional gas station which will cause additional traffic and gas odor.

James Giraud, Coeur d'Alene, said he has no objection with the restaurant or market but the issue for him is the gas station. He asked the City Council to take the recommendation of the Planning Commission that voted 7-0 to deny the zone change.

Tonyak Hebner, Coeur d'Alene, said having a gas station that is close to her residence, off of Atlas, is a benefit. She said that she doesn't smell any gas, they keep it clean, and she did not notice that there's higher traffic.

Jay Iannacito, Coeur d'Alene, said that the zone change will be a benefit to the community. He said that the proposal of Mr. Singh are truly in the best interest of the community providing an avenue to get their staples that is walking distance. He added that the zone change to C-17 will allow him to build what he wants to build and also to offset some costs.

Steven Listman, Coeur d'Alene, stated concerns such as light pollution, the fence should be tall enough, gas odor and fuel trucks coming in, the smell of coffee and the slope that may have water running to his yard.

Rex Goatcher, Coeur d'Alene, stated that his issues are the traffic, noise, smell of diesel and having a restaurant will emit odors such as of fried chicken.

KC Reese, Coeur d'Alene, said that he is in favor with the zone change. He stated that he lives near Mr. Singh's store at Atlas, and they love that it is walking distance for them. He shared that it is a possibility that if the zone change will not be approved then Mr. Singh could sell the property to someone who will develop it without the constraints Mr. Singh has agreed too. He added that Mr. Singh is presenting the best use of the property and that can be controlled to the benefit of the neighborhood.

Amy Bartoo, Coeur d'Alene, said that she is neutral and appreciates Mr. Singh's efforts to do the best he can and still be able to make profit which would include the gas station. She said that she is hoping that the NC will allow elimination of the parking at the back. She mentioned that 15<sup>th</sup> Street in the past has always been a floodplain which is a concern that should be considered in the development.

**RECESS:** Mayor Hammond called for a recess at 8:17 p.m. The meeting resumed at 8:23 p.m.

**APPLICANT REBUTTAL:** Mr. Andy Singh stated that he has three stores in the area, two on Northwest Boulevard and one on Atlas. He said that he bought the property at 15<sup>th</sup> and Best Avenue a year ago and he was aware that it is under NC zoning. He mentioned that after they had their hearing for the zone change through the Planning Commission, he took into consideration all the complaints about lighting and traffic which they considered in coming up with a rendering. However, he pointed out that if they do a strip mall, it will cost them twice as much money. He added that he needs the gas station to be able to pay bills and pay his employees better. Mr. Rex Anderson reiterated that it is their intention to be responsive to the community. He emphasized the following points: an environmental report is a requirement for the gas station; it is part of the building code that storm water should stay on site and be treated; the traffic engineer has no

objections as stated in the staff report; and the Fire Department has no objections as well. Mr. Anderson mentioned that Mr. Singh puts so much passion in the way he runs his business.

With no other comments received, Mayor Hammond closed public testimony.

**DISCUSSION:** Councilmember Gookin inquired if there is a development agreement already and how this property became a NC, and Mr. Behary replied that there is no development agreement and that when it was annexed in 2011, NC was the zoning assigned to it and it is a mixed-use low. Councilmember English inquired if the parking has to be at the back, to which Mr. Behary explained that the development standard for the NC is surface parking which should be located to the rear or to the side of the principal building. He added that it is intended to have the buildings front loaded along the sidewalk for pedestrian walkable communities and the parking is supposed to be located in the rear. Councilmember Miller stated that most of the objections that came out of the planning and zoning meeting have been addressed in the new plan and tied to a development agreement. She inquired if the development agreements stay with the developer or the property, and Mr. Adams stated that the development agreement stays with the property. Councilmember McEvers asked for the difference between conditional zoning and development agreement, to which Mr. Adams explained that conditional zoning will just tie with the land while development agreement gets into more specifics about what the developer will be tied into. Councilmember Wood shared that she understands the neighborhood's concerns on how the rezone; however, Mr. Singh also needs to make a profit to have business. She suggested to table it to give both sides to continue the dialogue. Councilmember English stated that there is compromise presented such as reducing the gas pumps from four to three as well as the lights will be turned off at 10:00 p.m. Councilmember Evans said that she is comfortable making a decision based on the amount of compromise that has been presented. She appreciated the neighborhood that came to voice their concerns and assured them that the Council can make conditions if they decide to move forward. Councilmember McEvers explained that the major issue for the neighborhood is the gas; however, Mr. Singh needs it to have a business. Councilmember Miller pointed out that Mr. Singh listened to the concerns of the community, but he is also building his business brand which is a grocery store with fuel. She added that he is coming forward and agreed to put conditions on the approval into a development agreement. Councilmember Gookin stated that he believes that changing the NC to C-17 would adversely impact the established neighborhood.

**MOTION:** Motion by Evans, seconded by English, to approve with conditions ZC-1-24, changing the zoning of property located SE corner of the intersection of 15<sup>th</sup> Street and Best Avenue; Applicant: GS4 Property, LLC, based on the attached findings and conclusions, which are established by the evidence set forth in the staff report, during the staff presentation, and the testimony of the applicant.

**DISCUSSION:** Councilmember Gookin explained that he is voting no because he believes that it does adversely impact the surrounding neighborhood. He noted that the development agreement would have to come back to the Council.

**ROLL CALL:** Gookin No; English Aye; Wood Aye; Evans Aye; Miller Aye; McEvers Aye.  
**Motion carried.**

COUNCIL BILL NO. 24-1007

AN ORDINANCE AMENDING THE ZONING ACT OF THE CITY OF COEUR D'ALENE, KOOTENAI COUNTY, IDAHO, KNOWN AS ORDINANCE NO. 1691, ORDINANCES OF THE CITY OF COEUR D'ALENE, BY CHANGING THE FOLLOWING DESCRIBED PROPERTY FROM A NC (NEIGHBORHOOD COMMERCIAL) TO C-17 (COMMERCIAL AT 17 UNITS/ACRE), SAID PROPERTY BEING DESCRIBED AS FOLLOWS, TO WIT: THE SOUTHEAST CORNER OF THE INTERSECTION OF 15TH STREET AND BEST AVENUE; REPEALING ALL ORDINANCES AND PARTS OF ORDINANCES IN CONFLICT HEREWITH; PROVIDING A SEVERABILITY CLAUSE; PROVIDE FOR THE PUBLICATION OF A SUMMARY OF THIS ORDINANCE AND AN EFFECTIVE DATE HEREOF

**MOTION:** Motion by Evans, seconded by McEvers, to dispense with the rule and read **Council Bill No. 24-1007** once by title only.

**ROLL CALL:** Evans Aye; Miller Aye; McEvers Aye; Gookin Aye; English Aye; Wood Aye.  
**Motion carried.**

**MOTION:** Motion by McEvers, seconded by Evans, to adopt **Council Bill No. 24-1007**.

**ROLL CALL:** Evans Aye; Miller Aye; McEvers Aye; Gookin No; English Aye; Wood Aye.  
**Motion carried.**

RESOLUTION NO. 24-042

A RESOLUTION OF THE CITY OF COEUR D'ALENE, KOOTENAI COUNTY, IDAHO, APPROVING AN AGREEMENT WITH TRI UTILITY COST REDUCTIONS, INC., FOR SERVICES RELATED TO A UTILITY BILL AUDIT.

**STAFF REPORT:** City Administrator Troy Tymesen noted that in addition to the City's own wastewater and water services, the City receives electric, gas, and telecommunication services from third party vendors. He explained that TRI Utility Cost Reductions Inc. has offered to conduct an audit of the City's utility bills with the objective to obtain refunds, credits, and reductions for those bills if justified. He added that there will be no cost for this service, but TRI will receive a fee equal to 50% of any refunds or credits that will be obtained for the City. He stressed that if there is a reduction in a utility's billings because of TRI's efforts, TRI will receive a fee of 50% of the savings for the first 36 months; however, this fee will not apply if the City moves or upgrades account services with the utility. He added that they have 35 years of experience in the business with a success ration of 87%.

**DISCUSSION:** Councilmember Wood said that she is completely in favor of this agreement with TRI Utility and it will be a great opportunity for the City with no risk considering that there are a lot of fees that shows up in the bills that doesn't make sense.



**MOTION:** Motion by Wood, seconded by Gookin, to approve Resolution No. 24-042 – Approval of an Agreement with TRI Utility Cost Reductions, Inc., for services related to a utility bill audit.

**ROLL CALL:** Miller Aye; McEvers Aye; Gookin Aye; English Aye; Wood Aye; Evans Aye.  
**Motion carried.**

#### RESOLUTION NO. 24-043

A RESOLUTION OF THE CITY OF COEUR D’ALENE, IDAHO, DECLARING THAT THERE IS ONLY ONE (1) VENDOR REASONABLY AVAILABLE FOR THE PURCHASE AND INSTALLATION OF POLICE VEHICLE EMERGENCY EQUIPMENT; AUTHORIZING THE PROCUREMENT AND INSTALLATION OF POLICE VEHICLE EMERGENCY EQUIPMENT FROM PREMIER VEHICLE INSTALLATIONS (“PVI”) IN THE AMOUNT OF \$130,534.77; AND DIRECTING THE CITY CLERK TO PUBLISH THE NOTICE OF A SOLE SOURCE EXPENDITURE REQUIRED BY IDAHO CODE § 67-2808(2).

**STAFF REPORT:** Police Captain Jeff Walther noted that the Police Department purchased seven new patrol vehicles to replace high mileage, worn patrol vehicles currently in use by the department. He said that these vehicles are included in the FY2023-24 budget. He stated that the proposed sole source expenditure is for approximately 50 individual components which would otherwise have to be purchased from various other vendors; however, the Police Department proposes purchasing all the needed equipment from one vendor, Premier Vehicle Installations (PVI), for police vehicle emergency equipment purchase and installation which provides uniformity in patrol vehicle operation. Captain Walther explained that PVI is also an authorized installer for the Sound Off Signal, Blue Print, emergency equipment management system that is currently controlling all the installed equipment in the Police Patrol vehicle fleet, and it is also the authorized dealer for other specific equipment brands that make up a Police vehicle package. He added that the “sole source purchase” assists the department during service and warranty issues. He pointed out that the City Attorney has reviewed this purchase and is of the opinion that the proposed purchase complies with the statutory sole source expenditure requirements.

**MOTION:** Motion by Wood, seconded by English, to approve **Resolution No. 24-043** – Declaring that there is only one (1) vendor reasonably available for the purchase and installation of Police vehicle emergency equipment, authorizing a contract with Premier Vehicle Installations for the same in the amount of \$130,534.77, and directing the City Clerk to publish notice of the sole source expenditure.

**DISCUSSION:** Councilmember McEvers asked if labor was included in the price, and Captain Walther replied that the installation is the labor part.

**ROLL CALL:** McEvers Aye; Gookin Aye; English Aye; Wood Aye; Evans Aye; Miller Aye.  
**Motion carried.**

## COUNCIL BILL NO. 24-1004

AN ORDINANCE PROVIDING FOR THE AMENDMENT OF COEUR D'ALENE MUNICIPAL CODE § 17.09.340 RELATING TO APPEALS OF A DECISION OF THE DESIGN REVIEW COMMISSION; PROVIDING FOR THE REPEAL OF CONFLICTING ORDINANCES; PROVIDING FOR SEVERABILITY; PROVIDING FOR THE PUBLICATION OF A SUMMARY OF THE ORDINANCE; AND PROVIDING FOR AN EFFECTIVE DATE THEREOF.

**STAFF REPORT:** City Attorney Randy Adams recalled that in the April 16, 2024 meeting, the Council requested an amendment to M.C. § 17.09.340 to allow testimony from the public in an appeal hearing of a Design Review Commission decision. He explained that the Code limits participation in the appeal hearing to the appellant and applicant, and their representatives, and city staff. Considering that M.C. § 17.09.340 falls within the Zoning Ordinance, he pointed that the Planning and Zoning Commission is required to consider an amendment and make a recommendation to Council. Mr. Adams noted that he drafted a Code amendment for consideration, and the Commission held a public hearing on May 14, 2024 with Commissioner Ingalls being absent. He narrated that the Commission voted on three motions, rejecting each, before finally achieving a majority vote to make a recommendation: the first failed motion was to recommend approval of the amendments, but to substitute language from M.C. §17.09.715(b) for administrative appeals so the process would mirror an appeal of a Commission decision and allow the Council to determine who could testify in an appeal hearing on a case-by-case basis, and also to recommend that the amendments to the appeal hearing procedures not apply to any pending appeals; the second failed motion was to approve of the amendments as presented by the City Attorney; the third failed motion was to recommend no changes to the Code; and the fourth motion, which passed, was to recommend approval of the amendments as presented, but to not have them apply to any pending appeals.

**DISCUSSION:** Councilmember McEvers asked if this is similar to the public hearing this evening, and Mr. Adams replied that it is similar in a way that the Council can consider public testimony in making its decision. Councilmember Evans inquired on what was mentioned during the public comment about the design review process, to which Mr. Adams explained that the current municipal ordinance allows the public to address the Design Review Commission and they were able to hear the comments and take those into consideration. Councilmember Gookin said that this would basically turn into a hearing where the appellant give their testimony and the public would be allowed to make comments. Mr. Adams said that the parameters of the appeal are the same and the Council are just hearing additional information, hence, it may not be a testimony but the public could make an argument based on the record and try to convince the Council that the Design Review Commission did the right thing or did not do the right thing. Councilmember Gookin asked about the appeal of Joan Woodard, and Mr. Adams said that the status is still in limbo and there are no time requirements for the City Council to make a decision on an appeal and since it was tabled, the public hearing is still open. Councilmember Wood stated that the Council can practice discernment on whether it falls within public comments or within the design review restrictions or not. She added that she would like to err on the side of always allowing for public comment. Councilmember Evans raised her concern over changing direction while the process is

happening. Mr. Adams explained that the Planning Commission has the same comments, but it is his legal opinion that this procedural change is constitutional and does not violate any rights.

**MOTION:** Motion by Gookin, seconded by Wood, to dispense with the rule and read **Council Bill No. 24-1004** once by title only.

**DISCUSSION:** Councilmember Gookin recommended to remove the addition on Section 4 and stated that he wants it applied to the pending hearing. Councilmember McEvers raised his concern if Mariott can sue the City, and Mr. Adams said that it would not be a valid lawsuit. He added that he spoke with the lawyer of Mariott who said that they really want to get the project moving and not delay it any further.

**ROLL CALL:** Gookin Aye; English Aye; Wood Aye; Evans Aye; Miller Aye; McEvers Aye.  
**Motion carried.**

**MOTION:** Motion by Gookin, seconded by Wood, to adopt **Council Bill No. 24-1004** and remove the provided clause at the end of Section 4.

**DISCUSSION:** Councilmember Miller said that she hopes that as time allows in the future, that they can look at some uniformity in the appeals process and public involvement.

**ROLL CALL:** Gookin Aye; English Aye; Wood Aye; Evans Aye; Miller Aye; McEvers Aye.  
**Motion carried.**

#### COUNCIL BILL NO. 24-1005

AN ORDINANCE PROVIDING FOR THE AMENDMENT OF COEUR D'ALENE MUNICIPAL CODE SECTION 4.15.130 REGARDING ENFORCEMENT AND PENALTIES FOR COMMERCIAL LAUNCH OF WATERCRAFT; PROVIDING FOR THE REPEAL OF CONFLICTING ORDINANCES; PROVIDING FOR SEVERABILITY; PROVIDING FOR THE PUBLICATION OF A SUMMARY OF THE ORDINANCE; AND PROVIDING FOR AN EFFECTIVE DATE THEREOF.

**STAFF REPORT:** City Attorney Randy Adams noted that the City has established a Watercraft Launching Fee for the 3<sup>rd</sup> Street Public Docks: \$5.00 for Idaho residents per launch or \$45.00 per season, and \$10.00 for Out-of-State Residents per launch or \$85.00 per season. He said that the Docks are intended for the private launching of watercraft, but commercial vendors have used the Docks to launch watercraft for their customers, and often, the commercial vendors do not pay the fee. He recalled that during the May 7, 2024 Council Meeting, Council voted to impose a \$1,000.00 civil penalty on commercial vendors who launch watercraft without paying the fee. He explained that the intention is to discourage illegal launching at the 3<sup>rd</sup> Street Public Docks.

**MOTION:** Motion by Gookin, seconded by Evans, to dispense with the rule and read **Council Bill No. 24-1005** once by title only.

**ROLL CALL:** English Aye; Wood Aye; Evans Aye; Miller Aye; McEvers Aye; Gookin Aye.  
**Motion carried.**

**MOTION:** Motion by Gookin, seconded by Miller, to adopt **Council Bill No. 24-1005.**

**ROLL CALL:** English Aye; Wood Aye; Evans Aye; Miller Aye; McEvers Aye; Gookin Aye.  
**Motion carried.**

#### COUNCIL BILL NO. 24-1006

AN ORDINANCE PROVIDING FOR THE AMENDMENT OF SECTIONS 12.28.180(G) AND 12.28.210(C) OF THE COEUR D'ALENE MUNICIPAL CODE REGARDING CURB AND SIDEWALK CONSTRUCTION AND IMPROVEMENTS; PROVIDING FOR THE REPEAL OF CONFLICTING ORDINANCES; PROVIDING FOR SEVERABILITY; PROVIDING FOR THE PUBLICATION OF A SUMMARY OF THE ORDINANCE; AND PROVIDING FOR AN EFFECTIVE DATE THEREOF.

**STAFF REPORT:** City Engineer Chris Bosley noted that the Municipal Code requires that, when construction requiring a building permit has a valuation of \$30,000, the property owner must install curbs and sidewalks, with a few exceptions. He said that the \$30,000 threshold was codified in 2009 and, since then, the cost of construction has risen substantially, making the cost of the requirement disproportionate to the valuation of the building permit. Adjusting for inflation, He said that an equivalent threshold value in 2024 would be slightly less than \$45,000. Mr. Bosley asked the Council to approve amendments to Municipal Code §§ 12.28.180 (G) and 12.28.210 (C) to increase the building permit threshold for requiring curb and sidewalk installation from \$30,000 to \$45,000.

**DISCUSSION:** Councilmember McEvers asked if this is tear it all out and putting all new, and Mr. Bosley explained that this is if they don't have a sidewalk at all, but their neighbors do, then they would have to put theirs in also which can be substantial depending on how many feet. Councilmember Gookin recalled that he brought this forward because when Safeway wanted to do a major remodel and then they found out that they would have to redo all their sidewalks which came out to be more expensive than the remodel, so they decided to cancel doing the remodel. He suggested to just peg the value at a percentage of the assessed property value. Councilmember English said that it should be a higher threshold to at least \$50,000 or \$60,000. Mayor Hammond said that going with the percentage value of the home makes more sense. Councilmember Evans suggested for staff to study the feedback presented and come back to Council. Councilmember Miller suggested coming up with a formula and Councilmember Wood asked for examples of assessments.

**MOTION:** Motion by Gookin, seconded by Wood, to table **Council Bill No. 24-1006** for next meeting.

**ROLL CALL:** Wood Aye; Evans Aye; Miller Aye; McEvers Aye; Gookin Aye; English Aye.  
**Motion carried.**

**ADJOURNMENT:** Motion by McEvers, seconded by Evans that there being no other business this meeting be adjourned. **Motion carried.**

The meeting adjourned at 9:33 p.m.

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James Hammond, Mayor

ATTEST:

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Jo Anne Mateski  
Executive Assistant



**COEUR D'ALENE CITY COUNCIL  
FINDINGS AND ORDER  
ZC-1-24**

**A. INTRODUCTION**

This matter having come before the City Council on, May 21, 2024, to consider ZC-1-24, a request for a zone change from NC (Neighborhood Commercial) to C-17 zoning district.

APPLICANT: GS4 Property LLC

LOCATION: A parcel of land in the Northwest Quarter of Section 7, Township 50 North, Range 3 West, Boise Meridian, Kootenai County, Idaho, as adjusted on Record of Survey, Book 21, page 85, records of Kootenai County, and located on the southeast corner of the intersection of 15<sup>th</sup> Street and Best Avenue, being Tax # 20177.

**A. FINDINGS OF FACT:**

**The City Council finds that the following facts, A1 through A19, have been established on a more probable than not basis, as shown on the record before it and on the testimony presented at the public hearing.**

A1. All public hearing notice requirements have been met for item ZC-1-24.

- Notice of the public hearing must be published in the official newspaper of the City at least fifteen (15) days prior to the hearing. Idaho Code § 67-6509(a). The notice was published in the Coeur d'Alene Press on May 4, 2024, seventeen days prior to the hearing.
- Notice of the public hearing must be posted on the premises no less than one (1) week prior to the hearing. Idaho Code § 67-6511(2)(b). The Notice was posted on the property on May 12, 2024, nine days prior to the hearing.
- Notice of the public hearing must be provided by mail to property owners or purchasers of record within the land being considered, and within three hundred (300) feet of the external boundaries of the land being considered. Idaho Code § 67-6511(2)(b). Fifty-four (54) notices were mailed to all property owners of record within three hundred feet (300') of the subject property on May 3, 2024.
- Notice of the public hearing must be sent to all political subdivisions providing services within the planning jurisdiction, including school districts and the manager or person in charge of the local public airport, at least fifteen (15) days prior to the public hearing. Idaho Code § 67-6509(a). The Notice was sent to all political subdivisions providing services within the planning jurisdiction, including school districts on May 3, 2024, seventeen days prior to the hearing.

- Notice of the public hearing must be given to a pipeline company operating any existing interstate natural gas transmission pipeline or interstate petroleum products pipeline, as recognized by the pipeline and hazardous materials safety administration, with a center point within one thousand (1,000) feet of the external boundaries of the land being considered, provided that the pipeline company is in compliance with section 62-1104, Idaho Code. Idaho Code § 67-6511(2)(b). The Notice was sent to pipeline companies providing services within 1,000 feet of the subject property on May 3, 2024.
- A2. Public testimony was received at a public hearing on May 21, 2024.
  - A3. The subject property is vacant and is located on the southeast corner of the intersection of 15<sup>th</sup> Street and Best Avenue. The subject site is .93 acres in area and is relatively flat.
  - A4. The subject site is currently zoned Neighborhood Commercial (NC) and was annexed into the City in 2011 in item A-1-11.
  - A5. The neighborhood is a mix of commercial and residential uses. The site is adjacent to two duplexes and one single family dwelling that are located in the county to the east. To the south is a multi-family apartment complex that is located within the city limits. There is a gas station on the northwest corner of intersection of 15<sup>th</sup> Street and Best Avenue that is zoned C-17.
  - A6. The Comprehensive Plan Future Land Use Map designation is the Mixed-Use Low Place Type. The Comprehensive Plan states that the compatible zoning for such Place Type are C17, C17L, NC, and CC. The Comprehensive Plan Future Land Use Map designation is the Mixed-Use Low Place Type.
  - A7. The Place Types in the Comprehensive Plan represent the form of future development, as envisioned by the residents of Coeur d'Alene. These Place Types will, in turn, provide the policy level guidance that will inform the City's Development Ordinance. Each Place Type corresponds to multiple zoning districts that will provide a high-level of detail and regulatory guidance on items such as height, lot size, setbacks, adjacencies, and allowed uses.
  - A8. According to the Comprehensive Plan, Mixed-Use Low places are highly walkable areas typically up to four-stories. Development types are primarily mixed-use buildings, with retail, restaurants on corners or along the entire ground floor frontage but could also include townhomes and multifamily housing. Floors above are residential, office, or a combination of those uses. Multifamily residential development provides additional housing options adjacent to mixed-use buildings. This place type is typically developed along a street grid that has excellent pedestrian and bike facilities, with mid-block crossings, as needed, to provide pedestrian access.

- A9. The Comprehensive Plan Goals, Objectives and Policies that are applicable to this matter are as follows:

**Community & Identity**

**Goal CI 1:**

Coeur d’Alene citizens are well informed, responsive, and involved in community discussions.

**Objective CI 1.1:**

Foster broad-based and inclusive community involvement for actions affecting businesses and residents to promote community unity and involvement.

**Goal CI 3**

Coeur d’Alene will strive to be livable for median and below income levels, including young families, working class, low income, and fixed income households.

**OBJECTIVE CI 3.1**

Support efforts to preserve existing housing stock and provide opportunities for new affordable and workforce housing.

**Growth & Development**

**Goal GD 1**

Develop a mix of land uses throughout the city that balance housing and employment while preserving the qualities that make Coeur d’Alene a great place to live.

**OBJECTIVE GD 1.1**

Achieve a balance of housing product types and price points, including affordable housing, to meet city needs.

**OBJECTIVE GD 1.5**

Recognize neighborhood and district identities.

**Goal GD 2**

Ensure appropriate, high-quality infrastructure to accommodate community needs and future growth.

**OBJECTIVE GD 2.1**

Ensure appropriate, high-quality infrastructure to accommodate growth and redevelopment.

- A10. There is adequate capacity in the public water system to support commercial, residential, irrigation, and fire flow for the proposed zone change. There is an existing 12” main on the west side of the property paralleling 15<sup>th</sup> Street approximately 5’ behind the curb and a 12” main in Best Avenue. There are two 2” services currently stubbed in to the property as well as a 6” Fireline stub. Any additional main extensions and/or fire hydrants and services will be the responsibility of the developer at their expense. Any additional service will have cap fees due at building permitting.
- A11. The nearest public sanitary sewer is located in 15<sup>th</sup> Street to the west of subject property. The Subject Property is within the City of Coeur d'Alene and is in accordance with the 2023 Sewer Master Plan. The City's Wastewater Utility presently has the wastewater system capacity and willingness to serve this zone change request as proposed.
- A12. Fire department access to the site (road widths, surfacing, maximum grade and turning radiuses), and fire protection (size of water main, fire hydrant amount and placement, and any fire line(s) for buildings requiring a fire sprinkler system) will be reviewed prior to final plat recordation or during the Site Development and Building Permit, utilizing the currently adopted International Fire Code (IFC) for compliance.
- A13. The Police Department does not have concerns with the proposed zone change.
- A14. The site is generally flat and has a slight slope to the east. The site is vacant, and is in a natural state with grass and trees located on it.
- A15. The proposed zone change itself would not adversely affect the surrounding area with regard to traffic, as no traffic is generated from a zone change alone. However, the application shows eight fueling positions (four pumps) in the site plan, as well as a market. Using Land Use Code 853 – Convenience Market with Gasoline Pumps from the ITE Trip Generation Manual, it can be estimated that this use will generate 133 a.m. peak hour trips and 153 p.m. peak hour trips. It is assumed that many of these trips would be pass-by trips rather than diverted trips. It is unlikely that this use will adversely affect traffic on 15<sup>th</sup> Street.
- A16. 15<sup>th</sup> Street is a Major Collector that experiences over 1000 trips per day. The Kootenai Metropolitan Planning Organization’s traffic model predicts a potential maximum of 1200 vehicles per hour, but capacity would largely be controlled by the traffic signal, which can theoretically move over 1700 vehicles/hour. Future 15<sup>th</sup> Street improvements will upgrade the traffic signal to better accommodate traffic. Access to 15<sup>th</sup> Street will be limited to approximately the south ½ of the parcel to ensure approaches are not within the functional area of the Best Ave intersection.
- A17. Both the NC and C17 zoning districts have design guidelines, performance standards, and parking requirements to minimize impacts to neighboring properties.

- A18. The applicant has indicated that, if this zone change request is approved, he intends to build a gas station with a mini mart and a quick serve restaurant on the subject site. Per the applicant's narrative and testimony, the owner intends to develop a project with design aspects to be a good neighbor, such as using a low-profile signs, no LED reader boards, limiting the number of fuel stations, and turning off fuel canopy lights after 11:00pm. However, unless approved as a conditional zoning with conditions, it should be noted that if the zone change is approved all uses within the C-17 zoning district would be allowed and C-17 performance standards would apply. (See the C-17 permitted uses on page 21)
- A19. The Planning and Zoning Commission considered this zone change request at their regularly scheduled hearing on January 9, 2024. In a unanimous vote of 7-0 they recommended that City Council deny the zone change request from NC to C-17. The Planning and Zoning Commission found that the proposed development would still negatively affect the surrounding neighborhood in terms of increased traffic, non-residential noise, and light which would be greater than uses permitted in the NC zone. In this case, the Planning and Zoning Commission found that the zoning is incompatible with the surrounding zoning and uses. Other than the commercial use to the northwest, everything else to the north, south, east and west is residential.

**B. CONCLUSIONS OF LAW:**

**Based on the foregoing Findings of Fact, the City Council makes the following Conclusions of Law.**

- B1. This proposal is in conformance with the Comprehensive Plan.
- B2. Public facilities and utilities are available and adequate for the proposed use.
- B3. The physical characteristics of the site do make it suitable for the request.
- B4. The proposal would not adversely affect the surrounding neighborhood with regard to traffic, neighborhood character, and or existing land uses.

**C. DECISION**

The City Council, pursuant to the foregoing Findings of Fact and Conclusions of Law, has determined that the requested zone change does comply with the required evaluation criteria and the zone change request is approved with conditions:

1. Low profile signage
2. No LED reader boards
3. No more than three (3) fuel pumps
4. Canopy lights will be turned off after 10:00 p.m.
5. Signage for traffic directions for deliveries in and out of the property
6. The City will enter into a development agreement with the applicant to include rendering as proposed

Motion by Amy Evans, seconded by Dan English, to adopt the foregoing Findings and Order and approve with conditions the request.

**ROLL CALL:**

<b>COUNCIL MEMBER ENGLISH</b>	<b>Voted</b>	<b>Yes</b>
<b>COUNCIL MEMBER MILLER</b>	<b>Voted</b>	<b>Yes</b>
<b>COUNCIL MEMBER GOOKIN</b>	<b>Voted</b>	<b>No</b>
<b>COUNCIL MEMBER EVANS</b>	<b>Voted</b>	<b>Yes</b>
<b>COUNCIL MEMBER MCEVERS</b>	<b>Voted</b>	<b>Yes</b>
<b>COUNCIL MEMBER WOOD</b>	<b>Voted</b>	<b>Yes</b>

Motion to approve with conditions carried by a 5 to 1 vote.



May 28, 2024  
**GENERAL SERVICES/PUBLIC WORKS COMMITTEE**  
**MINUTES**  
**12:00 p.m., Library Community Room**

**COMMITTEE MEMBERS**

Council Member Woody McEvers, Chairperson  
Council Member Kiki Miller  
Council Member Dan Gookin

**CITIZENS**

Neal Schreiberis, Lease Manager, Lamar Outdoor Advertising  
Jeff Connaway  
James Fillmore

**STAFF**

Juanita Knight, Senior Legal Assistant  
Randy Adams, City Attorney  
Troy Tymesen, City Administrator  
Renata McLeod, Municipal Services Director  
Kelley Setters, Deputy City Clerk  
Hilary Patterson, Community Planning Director

**Item 1.           Amendments to Municipal Code §§ 15.50.210 and 15.50.400(C) pertaining to billboards, and § 15.50.400(D) pertaining to electronic message displays.**

**(INFORMATION ONLY)**

Renata McLeod, Municipal Services Director, requests the Council consider amendments to Municipal Code §§ 15.50.210 and 15.50.400(C) pertaining to billboards, and § 15.50.400(D) pertaining to electronic message displays. Mrs. McLeod explained in her staff report that on March 1, 2022, the City adopted a new sign code to simplify sections and to ensure it meets the constitutionality required by case law. The code amendment allowed the code to shrink from 31 pages to 19 pages. At the December 19, 2023, Council meeting, Council requested staff provide an opportunity for Council to revisit the Sign Code and provide a way to allow the relocation of existing billboards within the City limits. Currently, no new billboards are allowed and existing billboards cannot be moved to a different location. Mrs. McLeod's further explained in her staff report that staff has reviewed codes from several cities including Post Falls, Hayden, Meridian, Idaho Falls, Pocatello, Spokane, Spokane Valley, Twin Falls, Bend OR, Missoula MT, Boise, Nampa, Caldwell, Eagle, the Montana Department of Transportation, and Idaho Transportation Department. In reviewing those codes, most cities did not allow for new billboards, and a few provided for their movement with certain parameters. Staff has prepared amendments to the Sign Code to allow existing billboards to be moved within reasonable parameters. It should be noted that there were no size allowances for billboards in the existing code as new billboards were specifically excluded and existing billboards could be maintained as they existed. When the current Sign Code was adopted, Council directed the removal of certain sections pertaining to existing billboards and the ability to modify them, the requirement to remove existing billboards upon annexation of property, and the ability to rebuild billboards if damaged. The proposed amendments provide for the relocation of an existing billboard to a C-17, M or LM zoning district with certain standards for setbacks, height, underground electrical, and separation between signs. It is recommended that the relocation of a billboard be done through a public hearing before the City Council for the purpose of transparency. Additionally, after review of codes from other cities and state departments, and review of the City's existing electronic sign codes, amendments to better clarify the illumination standards for all signs, with some additions to better protect the night sky, are recommended.

Community Planning Director, Hilary Patterson, explained that Sergeant Reneau of the Police Department conducted a light meter study on February 11, 2024, at 2:00 A.M., to evaluate current conditions and compliance with the existing illumination standards for various electronic signs within the City limits. The request was made due to past accident reports and complaints about a few electronic signs being a distraction for drivers at night and during the early morning hours. The study was done using foot candles and converted to nits for the current code metrics of 500 nits at night and 5,000 nits during the day. The signs met the nit requirement in the current code with varying degrees of illumination. However, both Sergeant Reneau and Chief White indicated that the 500-nit threshold is likely too high. The proposed amendments change the measurement from nits to foot candles, which is more standard, and implements the industry standard of 0.3 foot candles above ambient light.

Mrs. McLeod said that staff has provided the proposed code amendments to the two outdoor sign companies Lamar and Yesco, as well as to sign companies that have pulled permits in the City over the past year.

Kelley Setters, Deputy City Clerk, provided an explanation of how the City allocates sign allowances and provided an example of the formula as multiplying the street frontage measurement (X) by the driving lane factors (X) then by the density factor and then adding (+) in the sign area factor.

Mrs. Patterson explained the proposed amendments to the electronic signs & reader boards. Mrs. Patterson provided several reasons for amending the code related to electronic signs: complaints about night brightness; research from other cities netted better ways to measure light, moving from nits to foot candles; required technology for auto dimming; hold displays for 8 seconds versus 2 seconds; and restrict video messaging on electronic message displays. Mrs. Patterson said there are currently 83 electronic signs in the city.

Neal Schreibeis, the Lease Manager at Lamar Outdoor Advertising, has expressed concerns about the proposed amendments, deeming them overly complex. He believes that City Council oversight and a public hearing are unnecessary for these matters. According to Schreibeis, the City staff is fully capable of making these decisions. Additionally, he questioned the need of the City Attorney's review of the agreement between the billboard owner and the property owner. Mr. Schreibeis suggests that further clarifications are needed regarding billboards that may remain in place, the 500-foot requirement, structural setbacks, and underground power requirements.

Councilmember Gookin said he also reviewed the proposed amendments and shares the view that they impose excessive burdens. Councilmember Gookin specifically questioned Mr. Schreibeis about certain aspects of the proposed amendments. Councilmember Gookin sought clarification on whether these provisions are already standard practices and thus might not require inclusion in the code.

Councilmember Miller said it appears that advertisers are largely adhering to the proposed regulations. As for the requirement that billboards should not be located within 500 feet of a residential zoning district, historic district, park, school, etc., she deems it appropriate as we cannot directly regulate the content displayed on billboards themselves. Randy Adams, City Attorney, confirmed that the State of Idaho does not regulate the content of a billboard.

Councilmember Gookin said he shares the view of Mr. Schreibeis that a public hearing is not needed in these matters.

Jeff Connoway, a former member of the Coeur d'Alene Sign Board Committee for over 20 years, recalls a time when there was a prohibition against new billboard construction dating back to the 1960's. Even then, billboards

were considered proliferating and not particularly desirable. Large signs in a low-density community made sense in the past, but Mr. Connaway believes they are no longer desirable. He advocates for changing the code to disallow billboards or off-premises signage altogether. According to him, if put to a public vote, most people would favor removing all billboards. During his tenure on the Sign Board Committee, they consulted with Legal Counsel multiple times and were informed that the City lacks the authority to condemn existing leases for billboards. He additionally spoke in opposition to electronic signs.

Councilmember Miller asked Mr. Connaway if he concurred with the formula the City uses for regular commercial onsite signs, Mr. Connaway expressed approval. He said he and Kathy Lewis, former Deputy City Clerk, played a role in creating that formula during their time on the Sign Board Committee.

James Fillmore, a resident of Coeur d'Alene, expressed his gratitude to the Council Members for allowing public comments on the proposed code amendments related to billboards. While acknowledging that most citizens would prefer not to see billboards, he recognizes the existence of current billboards and views bringing them into compliance with up-to-date goals as a step forward. He appreciates the idea of not allowing additional billboards.

Mr. Fillmore highlighted the following points regarding electronic signage:

1. **Electronic Display Duration:**
  - He supports the proposal for electronic displays to hold a message for a minimum of 8 seconds before transitioning to the next message.
2. **Transition Time:**
  - Mr. Fillmore suggests that the transition time between images should be under half a second.
3. **Downward Facing Lighting Requirements:**
  - He raises a concern about the lack of mention regarding downward-facing lighting requirements in the proposed code.
  - His belief is that including such requirements would prevent light trespass and skyglow caused by billboards.

Councilmember Gookin expressed support for the proposed half-second transition time and requested that staff incorporate this into the proposed amendments.

Councilmember McEvers explained that this item will move forward to the next General Services / Public Works to ensure that all Councilmembers have an opportunity to hear the presentation and provide their input before the proposed amendments are present to the full City Council.

Mrs. McLeod, facilitated a discussion with the Councilmembers regarding their suggestions for the upcoming General Services/Public Works (GS/PW) meeting. Here's a summary of their input:

1. **City Attorney Lease Review:**
  - Some Councilmembers believe that requiring the City Attorney to review leases is overly restrictive.
2. **Public Hearing and Council Oversight:**
  - There's a question about whether there should be a public hearing or Council oversight related to certain matters.

**3. Underground Power Requirement:**

- Councilmember Gookin proposed that properties without existing underground power should not be required to have it.

**4. Ambient Light Level (Nits):**

- Councilmember Gookin also suggested specifying that “nits” (a unit of brightness) should not exceed the current ambient light level.

**5. Transitions:**

- A transition time of half a second was discussed.

**6. Physically Rotating Signs:**

- Councilmember Gookin raised the idea of clarifying physically rotating signs vs. signs with changing messages.

Mrs. McLeod said these suggestions will be provided for considered during the next GS/PW meeting.

Councilmember Miller expressed the view that incorporating existing practices into the code is acceptable, and therefore, does not consider the proposed amendments to be excessive.

Recording of the meeting can be found at: <https://youtu.be/2d828r6oOa4?si=Wpm6wUNOUCtOlbs6>

The meeting adjourned at 1:10 p.m.

Respectfully submitted,

*Juanita Knight*

*Senior Legal Assistant*

Recording Secretary



# CEMETERY LOT TRANSFER / SALE / REPURCHASE ROUTING FORM

**REQUEST RECEIVED BY:**

Municipal Services Department Name      Kelley Setters Employee      5.17.24 Date

**REQUESTED BY:**

Leslie Bening Name

\_\_\_\_\_  
Address      Phone

Request is for:  Repurchase of Lot(s)  
 Transfer of Lots(s) from Leslie Bening to Curtis Gerald Kilian  
Section: B Block: 40 Niche(s): \_\_\_\_\_, \_\_\_\_\_, \_\_\_\_\_, \_\_\_\_\_, \_\_\_\_\_, \_\_\_\_\_, \_\_\_\_\_, \_\_\_\_\_  
Lots(s): 11

Lot(s) are located in:  Forest Cemetery       Forest Cemetery Annex. (Riverview)  
Copy must be attached:  Deed       Certificate of Sale  
Requester is:  owner  executor  other      \*Note: If "executor" or "other", affidavits of authorization must be attached.  
Title Transfer Fee: \$ 40      Receipt No: \_\_\_\_\_

**ACCOUNTING DEPARTMENT** completes the following:       Attach original contract.  
Accountant Signature Vonnie Jensen      Date: 5/17/2024

**CEMETERY SUPERVISOR** completes the following:  
The above-referenced Lot(s) is/are certified to be vacant:  Yes       No  
The owner(s) of record of the Lot(s) in the Cemetery Book of Deeds is listed as: Leslie Bening  
The purchase price of the Lot(s) when sold to the owner of record was \$ \_\_\_\_\_ per lot.  
Supervisor's Signature M Brundt      Date: 5/17/2024

**LEGAL/RECORDS** completes the following:  
Certificate of Conveyance/Transfer received:  Yes       No  
Requester is authorized to execute certificate:  Yes       No  
I certify that all requirements for the transfer/sale/repurchase of cemetery lot(s) have been met and recommend that the transaction be completed.  
City Clerk's Signature \_\_\_\_\_      Date: \_\_\_\_\_  
Council approved transfer/sale/repurchase of above-referenced Lots(s) in regular session on. Date: \_\_\_\_\_

**CEMETERY SUPERVISOR** completes the following:  
Change of ownership noted in Book of Deeds:  Yes       No  
Cemetery copy filed original and supporting documents returned to City Clerk:  Yes       No  
Cemetery Supervisor's Signature \_\_\_\_\_      Date: \_\_\_\_\_





**OUTDOOR EATING FACILITY ENCROACHMENT APPLICATION**  
**Valid March 17 – Nov 1 Annually**

New applications or renewals with changes will be submitted to City Council for approval.  
The application must be received in the Customer Service Center a minimum of seven (7) days prior to a City Council meeting  
(first and third Tuesday of each month). Payments are due with the application.

Please mark the appropriate seating location below:

Seating on Private Property

Seating on Public Right of Way

*\*Encroachment Permit and additional insurance required*

Name of Eating Establishment: TEN/LE, LLC  
Applicant's Name: TAYLOR TAYLOR Phone Number: (208) 626-6106  
Contact Person: TAYLOR TAYLOR Phone Number: ( ) ( ) ( ) ( ) ( ) ( )  
Cell Phone: ( ) ( ) ( ) ( ) ( ) ( ) Email: tenlocda@gmail.com  
Mailing Address: 1118 N 2ND ST City/State/Zip: COEUR D'ALENE, ID  
Physical Address: " " City/State/Zip: " 83814 "

Completed Application  New  Renewal  
Change in ownership or type of use?  No  Yes \_\_\_\_\_  
Please specify

Do you hold a current State of Idaho Kootenai County and City of CDA alcohol license?  No  Yes

If yes, on your State of Idaho alcohol license do you have a restaurant designation?  No  Yes

Is anyone under the age of 21 allowed in the area inside your establishment where alcohol is served?  No  Yes

What hours/days is the full menu available? Start 8AM End 1:30PM Days 5

**Please supply a proposed site/seating plan, which is subject to approval and includes the following:**

- Show table sizes and chair placement, distance from building (side street 24" tables maximum).
- Show distance to any tree, grate, bench, light post, bicycle rack, news rack, etc.
- What is width of sidewalk from property line to curb?
- Please show location of refuse receptacle and disposal of cigarette remains.
- If within the City sidewalk or City property, provide a Certificate of Liability Insurance naming the City as additional insured (\$1,000,000).
- If within the City sidewalk or City property, complete a signed encroachment application.

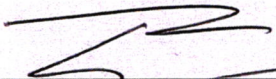


65.12	<b>FEES</b>		781.44	Pd. #
Number of Seats x \$12.13 per seat (Sewer Cap Fee)	12	=	\$ <del>245.57</del>	107
<i>*Fee required if not previously included in your original sewer rate seat count.</i>				
If located on sidewalk or City property, the encroachment fee is \$125.00.		+	\$ _____	
	<b>TOTAL DUE</b>		\$ _____	

If this is new or a renewal of permit with any changes to site plan or ownership, submit documentation. Please include the following, if within City sidewalk or City-owned property :

- \_\_\_\_\_ If serving alcohol, submit a site plan indicating proposed location of posts, type of barrier between posts, measurements from posts, and barriers to any obstacles including curbs, trees, grates, benches, etc.
- \_\_\_\_\_ Mark sidewalk for placement of posts and have the City team inspect and approve markings prior to installation
- \_\_\_\_\_ Have sidewalk cored and posts installed with caps for winter, at owners expense, after obtaining City Council approval (*see attached policy*)
- \_\_\_\_\_ Signs installed at exits

I have read the outdoor eating policy, and agree to abide by the regulations of the City. Further, I understand that no alcohol may be served at outdoor eating tables placed on City property after 10 p.m.

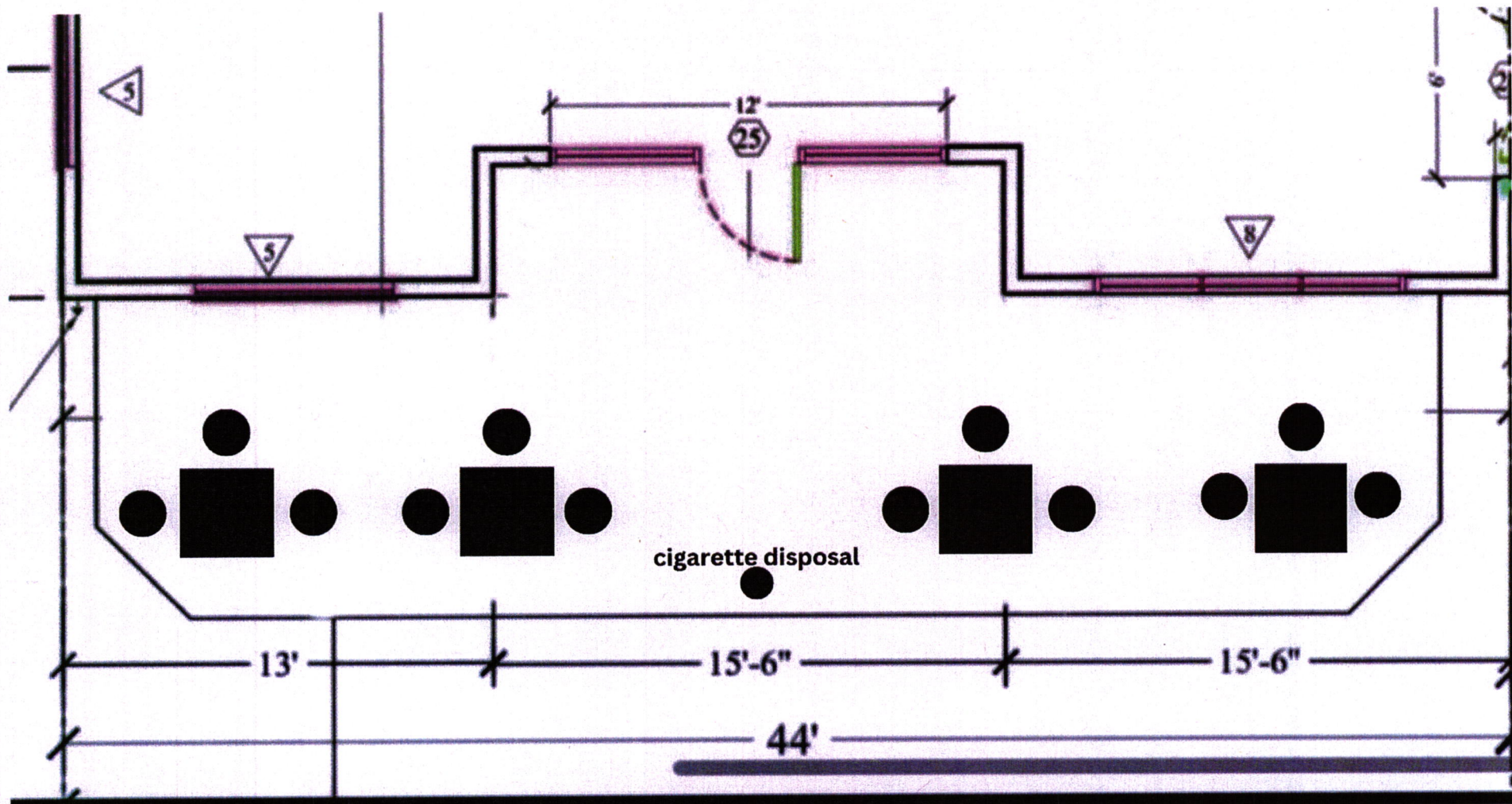
  
 \_\_\_\_\_  
 Applicant Signature

\_\_\_\_\_  
 Date 05/21/24

Internal Use Only

Reviewed and approved on: \_\_\_\_\_ By: \_\_\_\_\_  
 Issued By: \_\_\_\_\_ Date: \_\_\_\_\_  
 Conditions: \_\_\_\_\_  
 Denied due to: \_\_\_\_\_  
 Date: \_\_\_\_\_



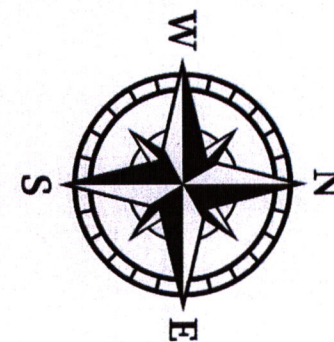


20ft from a tree on the south side

20ft from a light pole on the north side

Tables are 30x30 and square

7.5 ft from the building





Re: Fireworks Stands 2024  
 From: Kelley Setters, Deputy City Clerk

	<b>Location</b>	<b>Operated by</b>	<b>Distributor</b>	
	Corner of Ramsey and Prairie Rd	Eda Darwood 8505 Peach Lane Missoula, MT 59808	Bee-Rad Fireworks 8505 Peach Lane Missoula, MT 59808	X
	Albertsons 220 Ironwood Dr	Andy Flournoy 23310 E Inlet Dr #9 Liberty Lake WA 99019	TNT Fireworks S 104 Freya White Bldg #120B Spokane WA 99202	X
	Safeway 101 W Neider	Pamela Kyes 104 S Freya White Bldg Suite 120B Spokane WA 99202	TNT Fireworks S 104 Freya White Bldg #120B Spokane WA 99202	X
	Super 1 Foods 305 W Kathleen	Eric Campbell S 104 Freya White Bldg #120B Spokane WA 99202	TNT Fireworks S 104 Freya White Bldg #120B Spokane WA 99202	X
	Walgreens 225 W Appleway	Kristin Liberty 59 E Queen Ste 200 Spokane WA 97202	TNT Fireworks 104 S Freya White Bldg #120B Spokane WA 99202	X
	Ramsey & 1600 Appleway	Kimberly Craig 1600 W Appleway Ave Coeur d Alene, ID 83814	TNT Fireworks 104 S Freya White Bldg #120B Spokane WA 99202	X
	Fred Meyer 560 W. Kathleen	Wendi Cox 59 E. Queen Suite 200 Spokane, WA 99207	TNT Fireworks 59 E. Queen Suite 200 Spokane, WA 99207	
	Big 5 Parking Lot 101 E. Appleway Ave	Deborah Wright 3320 N 15 <sup>th</sup> Street Coeur d' Alene, ID 83814	TNT Fireworks PO Box 1318 Florence, AL 35631	X

## CITY COUNCIL STAFF REPORT

**DATE:** June 4, 2024  
**FROM:** Dennis J. Grant, Engineering Project Manager  
**SUBJECT:** **The Trails 6<sup>th</sup> Addition: Final Plat Approval, Acceptance of Improvements, Maintenance/Warranty Agreement and Security Approval**

---

### DECISION POINT

Staff is requesting the following:

1. Approval of the final plat document, a forty-eight (48) lot residential development.
2. Acceptance of the installed public infrastructure improvements.
3. Approval of the Maintenance/Warranty Agreement and Security.

### HISTORY

- a. Applicant: Melissa Wells, Manager  
Coeur Development, LLC  
1859 N. Lakewood Drive, Suite 201  
Coeur d'Alene, ID 83814
- b. Location: East of the Centennial Trail and west of Downing Lane on Homeward Bound Blvd.
- c. Previous Action:
  1. Final Plat Approval, The Trails (initial phase) – December 2015.
  2. Final Plat Approval, The Trails 1<sup>st</sup> Addition – May 2016.
  3. Final Plat Approval, The Trails 2<sup>nd</sup> Addition – December 2017.
  4. Final Plat Approval, The Trails 3<sup>rd</sup> Addition – June 2019.
  5. Final Plat Approval, The Trails 4<sup>th</sup> Addition – November 2019.
  6. Final Plat Approval, The Trails 5<sup>th</sup> Addition – October 2022.

### FINANCIAL ANALYSIS

The developer is installing the required warranty bond (10%) to cover any maintenance issues that may arise during the one (1) year warranty period that will commence upon this approval, and terminate, on June 4, 2025. The amount of the security provided is \$325,026.56.

### PERFORMANCE ANALYSIS

The developer has installed all required public infrastructure. The responsible City departments have approved the installations and found them ready to accept. Acceptance of the installed improvements will allow the issuance of all available building permits for this phase of the development, and, Certificate of Occupancy issuance upon completion. The City maintenance would be required to start after the one (1) year warranty period expires on June 4, 2025.

### DECISION POINT RECOMMENDATION

1. Approve the final plat document.
2. Accept the installed public infrastructure improvements.
3. Approve the Maintenance/Warranty Agreement and accompanying Security.

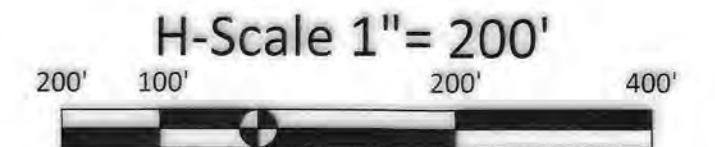


# THE TRAILS 6TH ADDITION

A REPLAT OF LOT 1, BLOCK 11 OF THE TRAILS

LOCATED IN THE SW 1/4 OF SECTION 28, TOWNSHIP 51 N., RANGE 4 W., BOISE MERIDIAN, CITY OF COEUR D'ALENE, KOOTENAI COUNTY, IDAHO

BOOK PAGE  
INST #



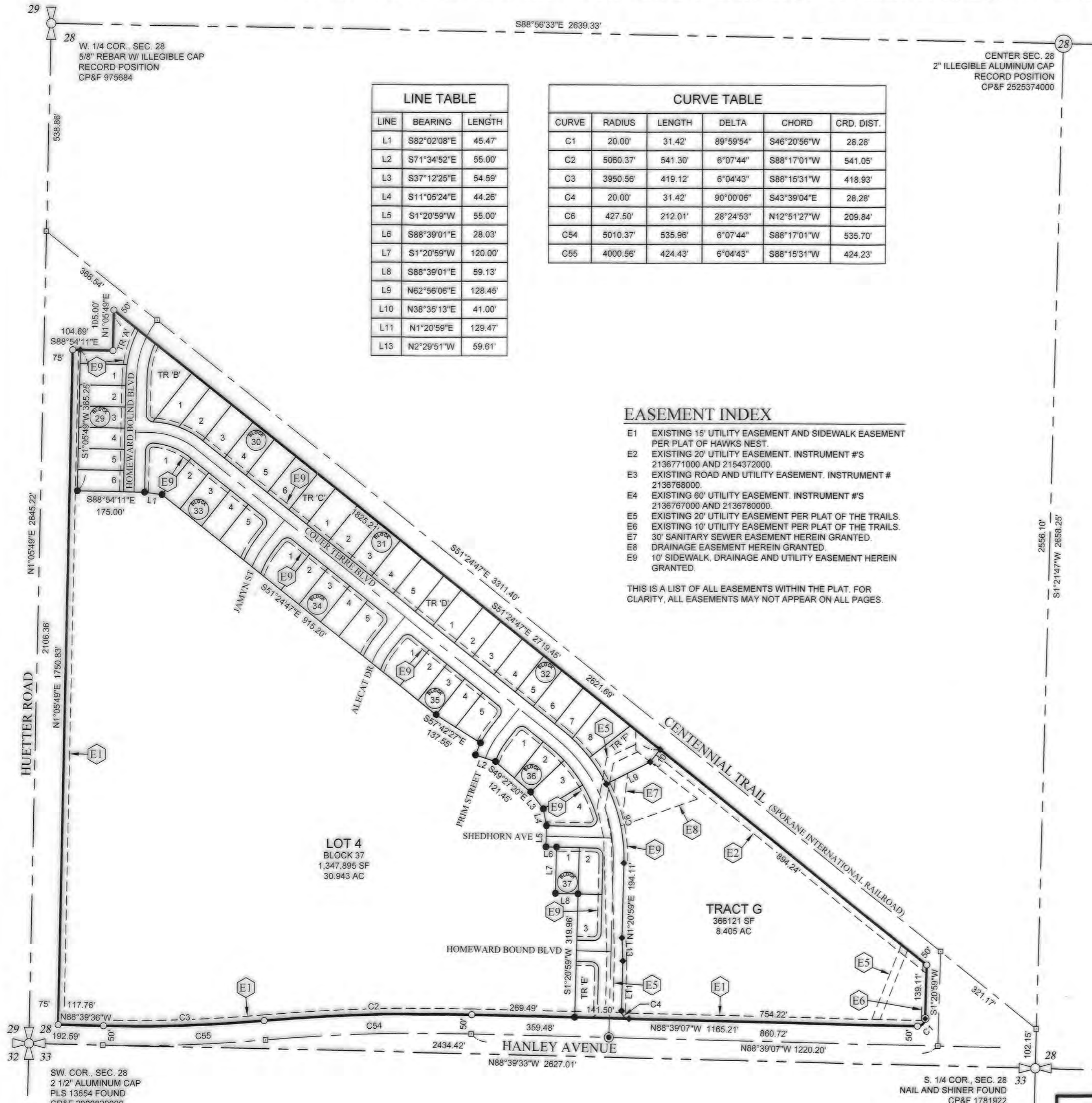
LINE	BEARING	LENGTH
L1	S82°02'08"E	45.47'
L2	S71°34'52"E	55.00'
L3	S37°12'25"E	54.59'
L4	S11°05'24"E	44.26'
L5	S1°20'59"W	55.00'
L6	S88°39'01"E	28.03'
L7	S1°20'59"W	120.00'
L8	S88°39'01"E	59.13'
L9	N62°56'06"E	128.45'
L10	N38°35'13"E	41.00'
L11	N1°20'59"E	129.47'
L13	N2°29'51"W	59.61'

CURVE	RADIUS	LENGTH	DELTA	CHORD	CRD. DIST.
C1	20.00'	31.42'	89°59'54"	S46°20'56"W	28.28'
C2	5060.37'	541.30'	6°07'44"	S88°17'01"W	541.05'
C3	3950.56'	419.12'	6°04'43"	S88°15'31"W	418.93'
C4	20.00'	31.42'	90°00'06"	S43°39'04"E	28.28'
C6	427.50'	212.01'	28°24'53"	N12°51'27"W	209.84'
C54	5010.37'	535.96'	6°07'44"	S88°17'01"W	535.70'
C55	4000.56'	424.43'	6°04'43"	S88°15'31"W	424.23'

### EASEMENT INDEX

- E1 EXISTING 15' UTILITY EASEMENT AND SIDEWALK EASEMENT PER PLAT OF HAWKS NEST.
- E2 EXISTING 20' UTILITY EASEMENT. INSTRUMENT #S 2136771000 AND 2154372000.
- E3 EXISTING ROAD AND UTILITY EASEMENT. INSTRUMENT # 2136768000.
- E4 EXISTING 60' UTILITY EASEMENT. INSTRUMENT #S 2136767000 AND 2136780000.
- E5 EXISTING 20' UTILITY EASEMENT PER PLAT OF THE TRAILS.
- E6 EXISTING 10' UTILITY EASEMENT PER PLAT OF THE TRAILS.
- E7 30' SANITARY SEWER EASEMENT HEREIN GRANTED.
- E8 DRAINAGE EASEMENT HEREIN GRANTED.
- E9 10' SIDEWALK, DRAINAGE AND UTILITY EASEMENT HEREIN GRANTED.

THIS IS A LIST OF ALL EASEMENTS WITHIN THE PLAT. FOR CLARITY, ALL EASEMENTS MAY NOT APPEAR ON ALL PAGES.



### LEGEND

- FOUND 5/8" x 24" REBAR WITH PLASTIC CAP, P.L.S. 9367
- SET 5/8" x 24" REBAR WITH PLASTIC CAP, P.L.S. 9367
- ◆ SET 1/2" x 24" REBAR WITH PLASTIC CAP, P.L.S. 9367
- ⊙ SET 5/8" x 24" REBAR WITH 2" ALUMINUM CAP, P.L.S. 9367
- ⊕ SET BRASS CAP MARKED P.L.S. 9367 IN CONCRETE
- CALCULATED POSITION, NOTHING FOUND OR SET
- ⊕ CENTER SECTION CORNER AS NOTED
- ⊕ E-W 1/4 SECTION CORNER AS NOTED
- ⊕ N-S 1/4 SECTION CORNER AS NOTED
- ⊕ SECTION CORNER AS NOTED
- E1 EASEMENT IDENTIFIER
- CP&F CORNER PERPETUATION & FILING RECORD (WITH INSTRUMENT NO.)

### TITLE DOCUMENTS

THESE ARE DOCUMENTS IN TITLE REPORT COMMITMENT KT-502074, ISSUED BY KOOTENAI COUNTY TITLE COMPANY, DATED: 12/29/2023 THAT FOR THE REASON INDICATED, CAN NOT BE DISPLAYED ON THE MAP.

1. BK 69 OF DEEDS, PG 353 AND BK 115 OF DEEDS, PG187 - HAVE NO EFFECT ON THE SUBJECT PROPERTY.
2. BK 115 OF DEEDS, PAGE 188, IS A BLANKET EASEMENT OVER THE SOUTHWEST QUARTER OF SECTION 28 FOR AN ELECTRICAL TRANSMISSION LINE AND TELEPHONE SYSTEM BENEFITING WASHINGTON WATER AND POWER.

### REFERENCES

IN RECORDS OF KOOTENAI COUNTY, IDAHO:

- R-1 PLAT OF HAWKS NEST 1ST ADDITION BY CHAD J. JOHNSON, PLS 9367, RECORDED IN BOOK K OF PLATS, PAGE 94, UNDER INSTRUMENT NUMBER 2139756000.
- R-2 PLAT OF THE TRAILS BY CHAD J. JOHNSON, PLS 9367, RECORDED IN BOOK L OF PLATS, PAGE 54, UNDER INSTRUMENT NUMBER 2527217000.
- R-3 PLAT OF THE TRAILS 4TH ADDITION BY CHAD J. JOHNSON, PLS 9367, RECORDED IN BOOK L OF PLATS, PAGE 405, UNDER INSTRUMENT NUMBER 2724783000.
- R-4 PLAT OF ENCLAVE AT THE TRAILS BY CHAD J. JOHNSON, PLS 9367, RECORDED IN BOOK L OF PLATS, PAGE 639, UNDER INSTRUMENT NUMBER 2872327000.
- R-5 PLAT OF THE TRAILS 5TH ADDITION BY CHAD J. JOHNSON, PLS 9367, RECORDED IN BOOK L OF PLATS, PAGE 764, UNDER INSTRUMENT NUMBER 2920350000.

### BASIS OF BEARING

AS SHOWN HEREON, BASIS OF BEARING FOR THIS SURVEY IS IDAHO STATE PLANE COORDINATE SYSTEM, WEST ZONE, (1103). PROJECT COORDINATES WERE TRANSLATED TO WEST ZONE AT A JOHNSON SURVEYING BASE POINT LOCATED AT (N: 2,211,208.49; E: 2,348,419.65) AND WERE POST PROCESSED USING NGS OPUS WITH A HORIZONTAL REFERENCE FRAME OF NAD83 (2011)(EPOCH: 2010.0000) AND A VERTICAL DATUM OF NAVD83 (GEOID 18). ALL BEARINGS SHOWN ARE GRID. ALL DISTANCES SHOWN ARE GROUND (US SURVEY FEET), WITH A COMBINED SCALE FACTOR OF 1.00008293 APPLIED AT THE BASE POINT. GEODETIC NORTH IS AN ANGULAR ROTATION OF -00°49'50"

### SURVEYOR'S NARRATIVE

THE PURPOSE OF THIS SURVEY IS TO REPLAT LOT 1, BLOCK 11, OF THE TRAILS. SURVEY MONUMENTS WHERE FOUND/HOLD AS DEPICTED HEREON.

### NOTE

1. EXCEPT AS SHOWN HEREON, THERE WAS NO ATTEMPT MADE TO SHOW THE PHYSICAL FEATURES OF THE PROPERTY, OR TO SHOW ANY UNRECORDED EASEMENTS.



<b>THE TRAILS 6TH ADDITION</b>			
A REPLAT OF LOT 1, BLOCK 11 OF THE TRAILS SW 1/4 OF SECTION 28, TOWNSHIP 51 N., RANGE 4 W., BOISE MERIDIAN CITY OF COEUR D'ALENE, KOOTENAI COUNTY, IDAHO			
DATE SURVEYED: DEC. 2023	DRAFTED BY: DTL	PLOT DATE: 04/22/2024	SHEET
FILE NAME: 23-062 PLAT	CHECKED BY: CJJ	PROJECT No.: 23-062	1 5

**Johnson**  
Surveying

P.O. Box 2544 Post Falls, ID 83877  
208-660-2351  
johnsonsurveyingnw.com



# THE TRAILS 6TH ADDITION

A REPLAT OF LOT 1, BLOCK 11 OF THE TRAILS LOCATED IN THE SW 1/4 OF SECTION 28, TOWNSHIP 51 N., RANGE 4 W., BOISE MERIDIAN, CITY OF COEUR D'ALENE, KOOTENAI COUNTY, IDAHO



H-Scale 1" = 60'

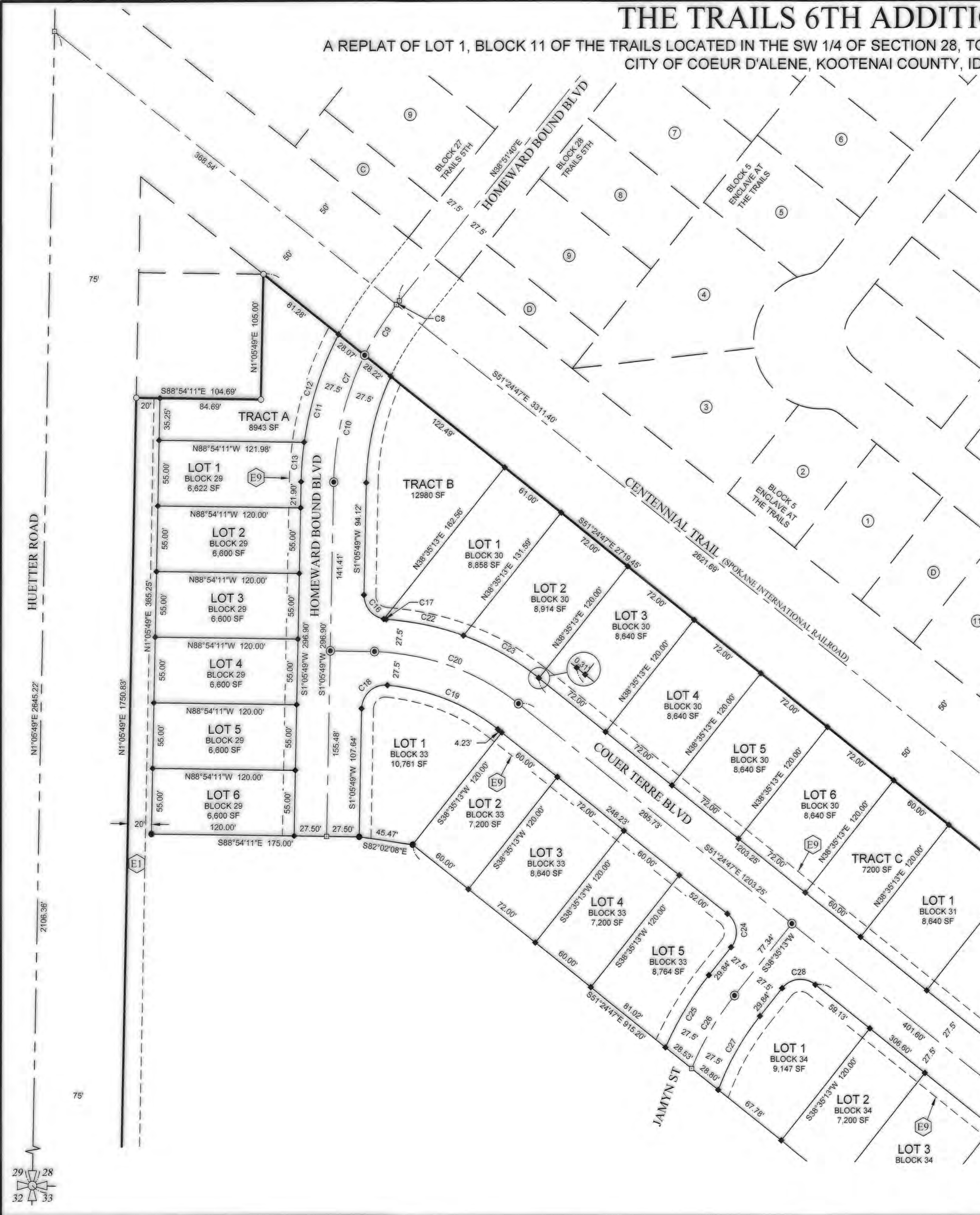


CURVE TABLE					
CURVE	RADIUS	LENGTH	DELTA	CHORD	CRD. DIST.
C7	250.00'	164.78'	37°45'51"	S19°58'45"W	161.81'
C8	250.00'	4.03'	0°55'25"	S38°23'58"W	4.03'
C9	250.00'	50.40'	11°33'04"	S32°09'44"W	50.32'
C10	250.00'	110.35'	25°17'23"	S13°44'30"W	109.45'
C11	277.50'	128.42'	26°30'52"	S14°21'15"W	127.27'
C12	277.50'	95.23'	19°39'47"	S17°46'48"W	94.77'
C13	277.50'	33.18'	6°51'05"	S4°31'21"W	33.16'
C16	20.00'	28.51'	81°40'25"	S39°44'24"E	26.16'
C17	20.00'	2.08'	5°58'19"	S83°33'45"E	2.08'
C18	20.00'	32.75'	93°49'23"	S48°00'30"W	29.21'
C19	172.50'	101.36'	33°40'02"	N68°14'48"W	99.91'
C20	200.00'	130.87'	37°29'24"	N70°09'29"W	128.54'
C22	227.50'	66.57'	16°46'00"	N78°09'55"W	66.34'
C23	227.50'	72.94'	18°22'08"	N60°35'51"W	72.62'
C24	20.00'	31.42'	90°00'00"	N6°24'47"W	28.28'
C25	277.50'	70.93'	14°38'41"	S31°15'53"W	70.74'
C26	250.00'	71.11'	16°17'53"	N30°26'17"E	70.87'
C27	222.50'	71.38'	18°22'49"	S29°23'49"W	71.07'
C28	20.00'	31.42'	90°00'00"	S83°35'13"W	28.28'

### EASEMENT INDEX

- E1 EXISTING 15' UTILITY EASEMENT AND SIDEWALK EASEMENT PER PLAT OF HAWKS NEST.
- E2 EXISTING 20' UTILITY EASEMENT. INSTRUMENT #S 2136771000 AND 2154372000.
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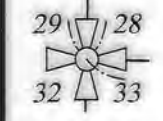
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THE TRAILS 6TH ADDITION			
A REPLAT OF LOT 1, BLOCK 11 OF THE TRAILS SW 1/4 OF SECTION 28, TOWNSHIP 51 N., RANGE 4 W., BOISE MERIDIAN CITY OF COEUR D'ALENE, KOOTENAI COUNTY, IDAHO			
DATE SURVEYED: DEC. 2023	DRAFTED BY: DTL	PLOT DATE: 04/22/2024	SHEET 2
FILE NAME: 23-062 PLAT	CHECKED BY: CJJ	PROJECT No.: 23-062	5

**Johnson**  
Surveying

P.O. Box 2544 Post Falls, ID 83877  
208-660-2351  
johnsonsurveyingnw.com





# THE TRAILS 6TH ADDITION

A REPLAT OF LOT 1, BLOCK 11 OF THE TRAILS

LOCATED IN THE SW 1/4 OF SECTION 28, TOWNSHIP 51 N., RANGE 4 W., BOISE MERIDIAN, CITY OF COEUR D'ALENE, KOOTENAI COUNTY, IDAHO

BOOK PAGE  
INST.#



H-Scale 1" = 60'



CURVE TABLE					
CURVE	RADIUS	LENGTH	DELTA	CHORD	CRD. DIST.
C29	20.00'	31.42'	90°00'00"	N6°24'47"W	28.28'
C30	227.50'	58.16'	14°38'47"	S31°15'50"W	58.00'
C31	200.00'	58.35'	16°42'58"	N30°13'45"E	58.14'
C32	172.50'	58.65'	19°28'47"	S28°50'50"W	58.37'
C33	20.00'	31.42'	90°00'00"	S83°35'13"W	28.28'
C34	20.00'	31.42'	90°00'00"	N6°24'47"W	28.28'
C35	227.50'	80.08'	20°10'05"	S28°30'11"W	79.67'
C36	227.50'	46.41'	11°41'14"	S32°44'38"W	46.33'
C37	227.50'	33.67'	8°28'51"	S22°39'34"W	33.64'
C38	200.00'	70.40'	20°10'05"	S28°30'11"W	70.04'
C39	172.50'	60.72'	20°10'05"	S28°30'11"W	60.41'
C40	20.00'	31.42'	90°00'00"	S83°35'13"W	28.28'
C42	372.50'	52.01'	7°59'59"	N47°24'47"W	51.97'

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DATE SURVEYED: DEC. 2023	DRAFTED BY: DTL	PLOT DATE: 04/22/2024	SHEET 3
FILE NAME: 23-062 PLAT	CHECKED BY: CJJ	PROJECT No.: 23-062	5

**Johnson**  
Surveying

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johnsonsurveyingnw.com



# THE TRAILS 6TH ADDITION

A REPLAT OF LOT 1, BLOCK 11 OF THE TRAILS

LOCATED IN THE SW 1/4 OF SECTION 28, TOWNSHIP 51 N., RANGE 4 W., BOISE MERIDIAN, CITY OF COEUR D'ALENE, KOOTENAI COUNTY, IDAHO

BOOK PAGE

INST.#



H-Scale 1" = 60'

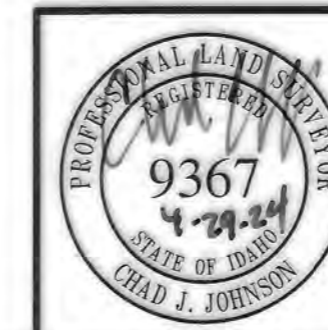
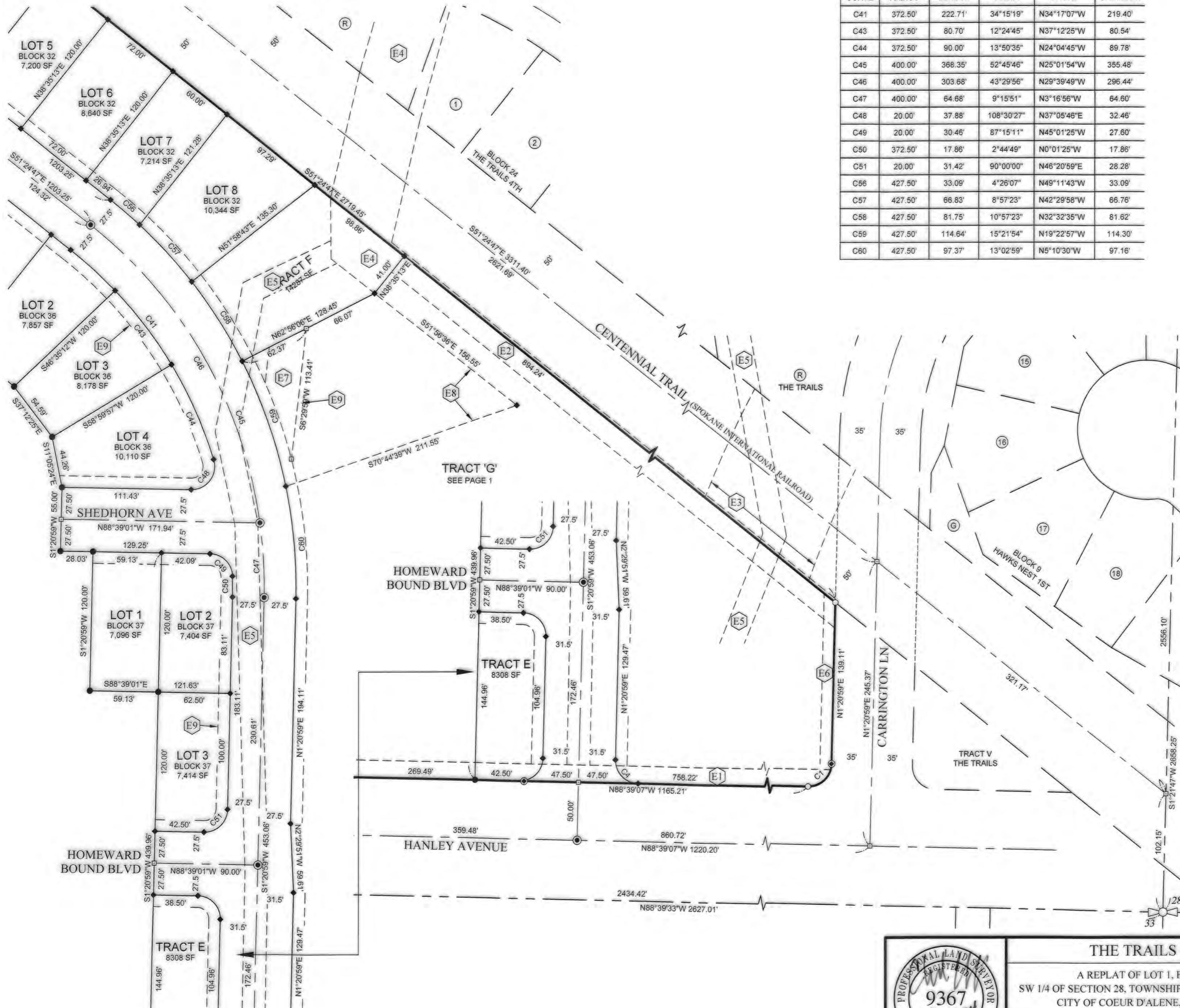


CURVE TABLE					
CURVE	RADIUS	LENGTH	DELTA	CHORD	CRD. DIST.
C41	372.50'	222.71'	34°15'19"	N34°17'07"W	219.40'
C43	372.50'	80.70'	12°24'45"	N37°12'25"W	80.54'
C44	372.50'	90.00'	13°50'35"	N24°04'45"W	89.78'
C45	400.00'	368.35'	52°45'46"	N25°01'54"W	355.48'
C46	400.00'	303.68'	43°29'56"	N29°39'49"W	296.44'
C47	400.00'	64.68'	9°15'51"	N3°16'56"W	64.60'
C48	20.00'	37.88'	108°30'27"	N37°05'46"E	32.46'
C49	20.00'	30.46'	87°15'11"	N45°01'25"W	27.60'
C50	372.50'	17.86'	2°44'49"	N0°01'25"W	17.86'
C51	20.00'	31.42'	90°00'00"	N46°20'59"E	28.28'
C56	427.50'	33.09'	4°26'07"	N49°11'43"W	33.09'
C57	427.50'	66.83'	8°57'23"	N42°29'58"W	66.76'
C58	427.50'	81.75'	10°57'23"	N32°32'35"W	81.62'
C59	427.50'	114.64'	15°21'54"	N19°22'57"W	114.30'
C60	427.50'	97.37'	13°02'59"	N5°10'30"W	97.16'

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# THE TRAILS 6TH ADDITION

A REPLAT OF LOT 1, BLOCK 11 OF THE TRAILS

LOCATED IN THE SW 1/4 OF SECTION 28, TOWNSHIP 51 N., RANGE 4 W., BOISE MERIDIAN, CITY OF COEUR D'ALENE, KOOTENAI COUNTY, IDAHO

BOOK PAGE

INST.#

## OWNER'S CERTIFICATE

KNOW ALL MEN BY THESE PRESENTS, THAT COEUR DEVELOPMENT LLC, AN IDAHO LIMITED LIABILITY COMPANY, HEREBY CERTIFIES THAT THEY OWN THE PROPERTY HEREINAFTER DESCRIBED AND INCLUDES SAID PROPERTY IN A PLAT TO BE KNOWN AS "THE TRAILS 6TH ADDITION".

LOT 1, BLOCK 11 OF THE PLAT OF THE TRAILS, ACCORDING TO THE PLAT THEREOF FILED FOR RECORD IN BOOK L OF PLATS, PAGES 54-54F, RECORDS OF KOOTENAI COUNTY, IDAHO.

CONTAINING 53.403 ACRES, MORE OR LESS, AND SUBJECT TO ALL EASEMENTS, RIGHTS-OF-WAY, COVENANTS, CONDITIONS, RESTRICTIONS AND AGREEMENTS OF RECORD OR APPEARING ON SAID PLAT.

ALL LOTS ARE SUBJECT TO THE COVENANTS, CONDITIONS AND RESTRICTIONS RECORDED UNDER INSTRUMENT NO. 2524354000, RECORDS OF KOOTENAI COUNTY, IDAHO.

THE OWNER HEREBY DEDICATES, TO THE PUBLIC IN THE NAME OF THE CITY OF COEUR D'ALENE, THE INTERIOR RIGHTS-OF-WAY TO BE KNOWN AS HOMEWARD BOUND BOULEVARD, COEUR TERRE BOULEVARD, JAMYN STREET, ALECAT DRIVE, PRIM STREET AND SHEDHORN LANE AS SHOWN ON THE FACE OF THE PLAT.

THE OWNER(S) HEREBY DEDICATE TO THE PUBLIC IN THE NAME OF THE CITY OF COEUR D'ALENE TRACT F AS DEPICTED HEREON FOR A SANITARY SEWER LIFT STATION.

THE OWNER(S) HEREBY DEDICATE TO THE PUBLIC IN THE NAME OF THE CITY OF COEUR D'ALENE TRACT G AS DEPICTED HEREON FOR USE AS A PARK.

THE OWNER(S) HEREBY GRANT TO THE PUBLIC IN THE NAME OF THE CITY OF COEUR D'ALENE THE THIRTY (30) FOOT WIDE SEWER EASEMENT IDENTIFIED HEREON AS EASEMENT E7.

THE OWNER(S) HEREBY GRANT TO THE TRAILS HOME OWNER'S ASSOCIATION THE DRAINAGE EASEMENT IDENTIFIED HEREON AS EASEMENT E8.

THE OWNER(S) HEREBY GRANT TO THE PUBLIC IN THE NAME OF THE CITY OF COEUR D'ALENE THE TEN (10) FOOT WIDE SIDEWALK, DRAINAGE AND UTILITY EASEMENT, ADJACENT TO ALL ROADS, IDENTIFIED HEREON AS EASEMENT E9.

TRACTS A, B, C, D AND E SHALL BE DEEDED TO THE TRAILS HOME OWNER'S ASSOCIATION FOR INSTALLATION, OPERATION AND MAINTENANCE OF STORM WATER MANAGEMENT FACILITIES, PUBLIC PATHWAYS AND SIDEWALKS. SAID TRACTS CANNOT BE SOLD OR TRANSFERRED. IN ADDITION, THE STORM WATER FACILITIES CANNOT BE MODIFIED WITHOUT EXPRESS WRITTEN APPROVAL OF THE CITY OF COEUR D'ALENE ENGINEERING DEPARTMENT.

THE TRAILS HOME OWNER'S ASSOCIATION, INC. SHALL BE RESPONSIBLE FOR LANDSCAPING, IRRIGATION AND MAINTENANCE OF TRACTS A, B, C, D AND E.

THE OWNER HEREBY GRANTS A BLANKET INGRESS & EGRESS EASEMENT OVER THE ENTIRETY OF TRACTS A, B, C, D AND E TO THE CITY OF COEUR D'ALENE FOR INSPECTION AND EMERGENCY MAINTENANCE OF STORM WATER MANAGEMENT FACILITIES.

SANITARY SEWER TO BE PROVIDED BY THE CITY OF COEUR D'ALENE.

DOMESTIC WATER TO BE PROVIDED BY THE CITY OF COEUR D'ALENE.

Melissa Wells 5/23/24  
MELISSA WELLS, MANAGER DATE

## NOTARY PUBLIC

ACKNOWLEDGMENT

STATE OF IDAHO )  
COUNTY OF KOOTENAI ) S.S.

THIS RECORD WAS ACKNOWLEDGED BEFORE ME ON THIS 23<sup>rd</sup> DAY OF May, IN THE YEAR OF 2024, BY MELISSA WELLS AS MANAGER OF COEUR DEVELOPMENT LLC, IDAHO LIMITED LIABILITY COMPANY.

NOTARY PUBLIC: Cynthia L Thomas

MY COMMISSION EXPIRES: JUNE 7, 2028



## PANHANDLE HEALTH DISTRICT

SANITARY RESTRICTIONS AS REQUIRED BY IDAHO CODE, TITLE 50, CHAPTER 13 HAVE BEEN SATISFIED BASED ON A REVIEW BY THE QUALIFIED LICENSED PROFESSIONAL ENGINEER (QPE) REPRESENTING (CITY OF COEUR D'ALENE) AND THE QPE APPROVAL OF THE DESIGN PLANS AND SPECIFICATIONS AND THE CONDITIONS IMPOSED ON THE DEVELOPER FOR CONTINUED SATISFACTION OF THE SANITARY RESTRICTIONS. WATER AND SEWER LINE HAVE BEEN COMPLETED AND SERVICES CERTIFIED AS AVAILABLE. SANITARY RESTRICTIONS MAY BE RE-IMPOSED, IN ACCORDANCE WITH SECTION 50-1326, IDAHO CODE, BY THE ISSUANCE OF A CERTIFICATE OF DISAPPROVAL.

HEALTH DISTRICT SIGNATURE: Kristina Kauts DATE: 5-16-2024

## CITY COUNCIL APPROVAL

THIS PLAT IS HEREBY APPROVED AND ACCEPTED BY THE CITY COUNCIL OF COEUR D'ALENE, IDAHO, THIS \_\_\_\_\_ DAY OF \_\_\_\_\_, 20\_\_\_\_.

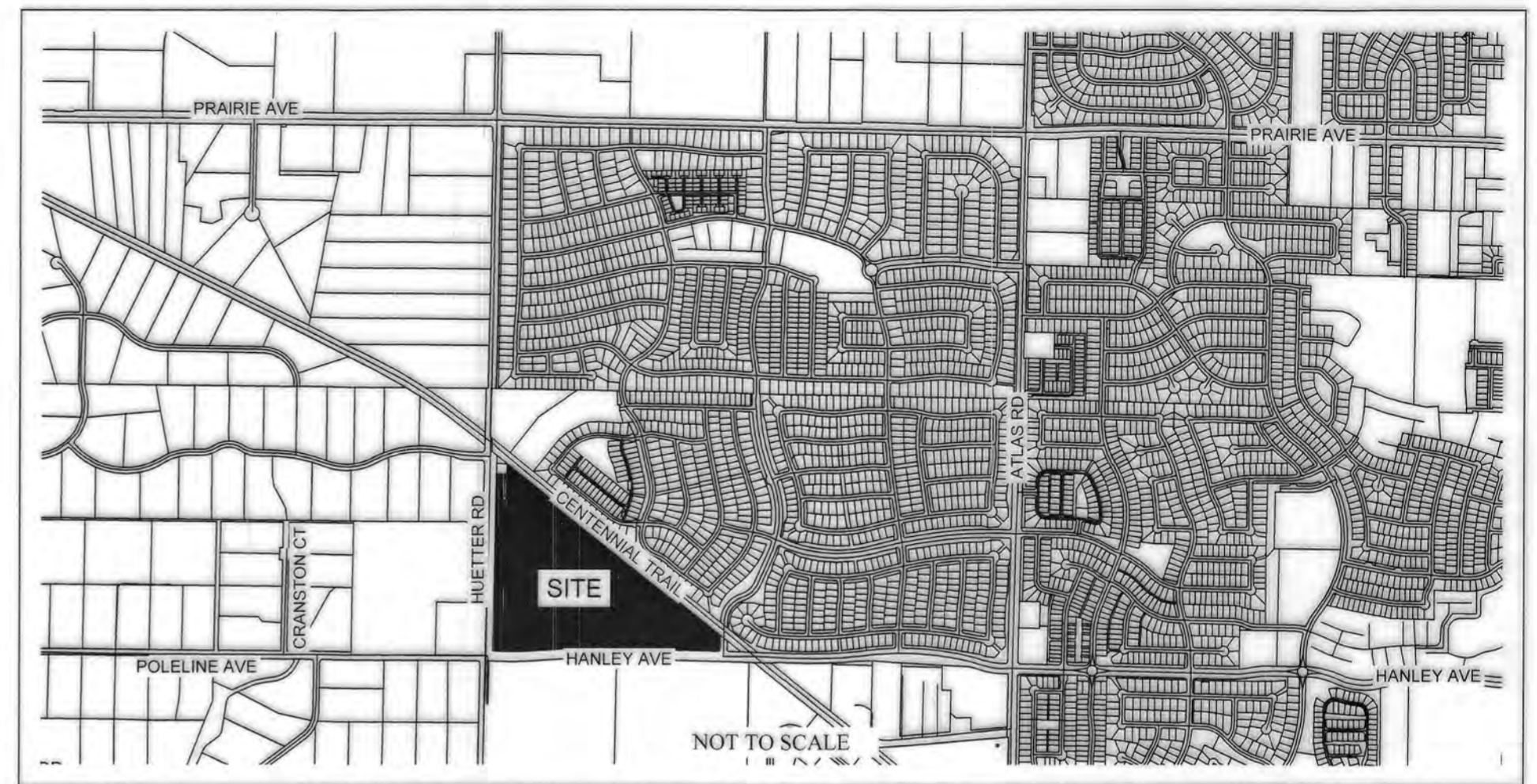
CITY OF COEUR D'ALENE, CLERK

## CITY ENGINEER APPROVAL

I HEREBY CERTIFY THAT I HAVE EXAMINED, ACCEPTED AND APPROVED THIS PLAT, THIS \_\_\_\_\_ DAY OF \_\_\_\_\_, 20\_\_\_\_.

CHRISTOPHER W. BOSLEY PE #10804  
CITY OF COEUR D'ALENE, ENGINEER

## VICINITY MAP



## TREASURER'S CERTIFICATE

I HEREBY CERTIFY THIS 17 DAY OF May, 2024, THAT THE REQUIRED TAXES ON THE HEREIN PLATTED LAND HAVE BEEN PAID THROUGH

December 31st 2023

Anna Engels (Deputy Treasurer)  
KOOTENAI COUNTY TREASURER

## RECORDER'S CERTIFICATE

I HEREBY CERTIFY THAT THIS PLAT WAS FILED FOR RECORD IN THE OFFICE OF THE RECORDER OF KOOTENAI COUNTY, IDAHO THIS \_\_\_\_\_ DAY OF \_\_\_\_\_, 20\_\_\_\_.

AT \_\_\_\_\_ M. AND DULY RECORDED IN BOOK \_\_\_\_\_ OF PLATS AT PAGE(S) \_\_\_\_\_ AND UNDER INSTRUMENT NO. \_\_\_\_\_ AT THE REQUEST OF \_\_\_\_\_.

KOOTENAI COUNTY CLERK

BY: \_\_\_\_\_ DEPUTY CLERK

FEE PAID

## COUNTY SURVEYOR'S CERTIFICATE

I HEREBY CERTIFY THAT I HAVE EXAMINED THIS PLAT AND APPROVE THE SAME FOR RECORDING THIS \_\_\_\_\_ DAY OF \_\_\_\_\_, 20\_\_\_\_.

RODNEY E. JONES PLS 12463  
KOOTENAI COUNTY SURVEYOR



## SURVEYOR'S CERTIFICATE

I, CHAD J. JOHNSON, PLS 9367, A PROFESSIONAL LAND SURVEYOR IN THE STATE OF IDAHO, DO HEREBY CERTIFY THAT THIS IS A TRUE SURVEY OF THE PLATTED LAND MADE BY ME, OR UNDER MY DIRECT SUPERVISION, AND THAT APPLICABLE CORNER RECORDS HAVE BEEN FILED, IN COMPLIANCE WITH THE LAWS OF THE STATE OF IDAHO.

Chad J. Johnson 4-29-24  
CHAD J. JOHNSON PLS 9367 DATE

	<b>THE TRAILS 6TH ADDITION</b> A REPLAT OF LOT 1, BLOCK 11 OF THE TRAILS SW 1/4 OF SECTION 28, TOWNSHIP 51 N., RANGE 4 W., BOISE MERIDIAN CITY OF COEUR D'ALENE, KOOTENAI COUNTY, IDAHO				
	DATE SURVEYED: DEC. 2023 FILE NAME: 23-062 PLAT	DRAFTED BY: DTL CHECKED BY: CJJ	PLOT DATE: 04/29/2024 PROJECT No.: 23-062	SHEET 5 5	

RESOLUTION NO. 24-044

A RESOLUTION OF THE CITY OF COEUR D'ALENE, KOOTENAI COUNTY, IDAHO, APPROVING THE FINAL PLAT, ACCEPTING INSTALLED PUBLIC INFRASTRUCTURE IMPROVEMENTS, AND APPROVING A MAINTENANCE/WARRANTY AGREEMENT AND SECURITY FOR THE TRAILS 6<sup>TH</sup> ADDITION (S-5-14).

WHEREAS, the City Engineer for the City of Coeur d'Alene has recommended that the City of Coeur d'Alene approve the Final Plat, accept the installed public infrastructure improvements, enter into an agreement for Maintenance/Warranty of Subdivision Work, and accept Security to complete certain work for The Trails 6<sup>th</sup> Addition, pursuant to terms and conditions set forth in an agreement, a copy of which are attached hereto as Exhibit "A" and by reference made a part hereof; and

WHEREAS, it is deemed to be in the best interests of the City of Coeur d'Alene and the citizens thereof to enter into such agreement.

NOW, THEREFORE,

BE IT RESOLVED by the Mayor and City Council of the City of Coeur d'Alene that the City approve the Final Plat, accept the installed infrastructure improvements, enter into an agreement for Maintenance/Warranty of Subdivision Work, and accept Security to complete certain work for The Trails 6<sup>th</sup> Addition, in substantially the form attached hereto as Exhibit "A" and incorporated herein by reference, with the provision that the Mayor, City Administrator, and City Attorney are hereby authorized to modify said agreement to the extent the substantive provisions of the agreement remain intact.

BE IT FURTHER RESOLVED that the Mayor and City Clerk be and they are hereby authorized to execute such agreement on behalf of the City.

DATED this 4<sup>th</sup> day of June, 2024.

---

James Hammond, Mayor

ATTEST:

---

Renata McLeod, City Clerk

Motion by \_\_\_\_\_, Seconded by \_\_\_\_\_, to adopt the foregoing resolution.

ROLL CALL:

COUNCIL MEMBER MILLER Voted

COUNCIL MEMBER ENGLISH Voted

COUNCIL MEMBER GOOKIN Voted

COUNCIL MEMBER EVANS Voted

COUNCIL MEMBER MCEVERS Voted

COUNCIL MEMBER WOOD Voted

\_\_\_\_\_ was absent. Motion \_\_\_\_\_.



**AGREEMENT FOR MAINTENANCE/WARRANTY OF SUBDIVISION WORK**  
**The Trails 6<sup>th</sup> Addition**

THIS AGREEMENT made this 4<sup>th</sup> day of June, 2024 between Coeur Development, LLC, whose address is 1859 N. Lakewood Drive, Suite 201, Coeur d'Alene, ID 83814, with Melissa Wells, Manager, hereinafter referred to as the "**Developer**," and the city of Coeur d'Alene, a municipal corporation and political subdivision of the state of Idaho, whose address is City Hall, 710 E. Mullan Avenue, Coeur d'Alene, ID 83814, hereinafter referred to as the "**City**";

WHEREAS, the City has approved the final subdivision plat of The Trails 6<sup>th</sup> Addition, a forty-eight (48) lot, residential development in Coeur d'Alene, lying within the Southwest Quarter of Section 28, Township 51 North, Range 4 West, B.M., Kootenai County, Idaho; and

WHEREAS, the Developer completed the installation of certain public improvements in the noted subdivision as required by Title 16 of the Coeur d'Alene Municipal Code and is required to warrant and maintain the improvements for one year; NOW, THEREFORE,

**IT IS AGREED AS FOLLOWS:**

The Developer agrees to maintain and warrant for a period of one year from the approval date of this agreement, the public improvements listed in the attached spreadsheet, attached as Exhibit "A", and, as shown on the construction plans entitled "The Trails 6<sup>th</sup> Addition", signed and stamped by Gabe R. Gallinger, PE, # 12184, dated June 13, 2023, incorporated herein by reference, including but not limited to: sanitary sewer system and appurtenances, potable water system and appurtenances, catch basins, stormwater drainage swales, drywells and appurtenances, concrete curb and sidewalk including ramps, asphalt paving, street luminaires, signing and monumentation as required under Title 16 of the Coeur d'Alene Municipal Code.

The Developer herewith delivers to the City, security in a form acceptable to the City, for the amount of Three hundred Twenty-five Thousand Twenty-six and 56/100 Dollars (\$325,026.56) securing the obligation of the Developer to maintain and warrant the public subdivision improvements referred to herein. The security shall not be released until the 4<sup>th</sup> day of June, 2025. The City Inspector will conduct a final inspection prior to the release of the security to verify that all installed improvements are undamaged and free from defect. In the event that the improvements made by the Developer were not maintained or became defective during the period set forth above, the City may demand the funds represented by the security and use the proceeds to complete maintenance or repair of the improvements thereof. The Developer further agrees to be responsible for all costs of warranting and maintaining said improvements above the amount of the security given.

IN WITNESS WHEREOF, the parties have set their hands and seal the day and year first above written.


**City of Coeur d'Alene**

\_\_\_\_\_  
James Hammond, Mayor

**ATTEST:**

\_\_\_\_\_  
Renata McLeod, City Clerk

**Coeur Development, LLC**

  
\_\_\_\_\_  
Melissa Wells, Manager

# EXHIBIT 'A'

**Trails 6th Addition**  
**Maintenance-Warranty Bond Estimate**  
 May 8, 2024

Item No.	Description & Cost Code	Quantity	Units	Unit Price	Total Price
<b>6200 - Site Preparation/Grading</b>					
<b>6210 - Excavation &amp; Grading</b>					<b>\$64,420.00</b>
1	Road Subgrade Prep	24,300	SY	\$1.80	\$43,740.00
2	Finish Grading (replace topsoil openspace)	1,960	CY	\$8.00	\$15,680.00
3	Construction Entrance	1	EA	\$1,500.00	\$1,500.00
4	Inlet Protection	40	EA	\$75.00	\$3,000.00
5	Concrete Washout Area	1	EA	\$500.00	\$500.00
<b>6250 - Sewer</b>					
<b>6250 - Sewer</b>					<b>\$727,357.09</b>
1	15" PVC Sanitary Sewer (Deep > 25')	811	LF	\$218.65	\$177,325.15
2	12" PVC Sanitary Sewer (Deep > 25')	73	LF	\$175.50	\$12,811.50
3	8" PVC Sanitary Sewer (Deep > 25')	80	LF	\$154.71	\$12,376.80
4	8" PVC Sanitary Sewer	1,140	LF	\$44.15	\$50,331.00
5	8" PVC Sanitary Sewer (Deep > 20')	1,784	LF	\$154.71	\$276,002.64
6	48" Manholes	7	EA	\$4,800.00	\$33,600.00
7	48" Manholes (Deep Manhole > 25')	8	EA	\$6,200.00	\$49,600.00
8	8" Interior Manhole Drop	4	EA	\$1,950.00	\$7,800.00
9	Adjust Existing Manhole Rim	3	EA	\$600.00	\$1,800.00
10	8" Cleanout	2	EA	\$1,400.00	\$2,800.00
11	Sewer Services	50	EA	\$1,740.00	\$87,000.00
12	Seal exsiting 12" in wet well	1	LS	\$5,020.00	\$5,020.00

**Trails 6th Addition**  
**Maintenance-Warranty Bond Estimate**  
 May 8, 2024

Item No.	Description & Cost Code	Quantity	Units	Unit Price	Total Price
13	Core/Connect to Existing Lift Station (15" Diam.)	1	LS	\$10,890.00	\$10,890.00

**6300 - Stormwater**

<b>6300 - Stormwater</b>					<b>\$182,404.00</b>
1	Concrete Curb Inlet (4' Wide)	2	EA	\$400.00	\$800.00
2	Concrete Inlet (4' wide Concrete Sidewalk)	2	EA	\$2,235.00	\$4,470.00
3	Concrete Curb Cut with Apron	42	EA	\$225.00	\$9,450.00
3	Catch Basin	20	EA	\$2,535.00	\$50,700.00
4	Drywell Type A (Single)	3	EA	\$3,300.00	\$9,900.00
4	Drywell Type B (Double)	17	EA	\$3,800.00	\$64,600.00
5	12" PVC Storm Pipe	380	LF	\$63.00	\$23,940.00
6	12" Ductile Storm Pipe	0	LF	\$0.00	\$0.00
7	8" PVC Storm Pipe	401	LF	\$44.00	\$17,644.00
8	Rip-Rap Pad at Pipe Outlet	2	EA	\$450.00	\$900.00

**6350 - Water**

<b>6350 - Water System</b>					<b>\$737,995.00</b>
1	8" PVC Water Main	955	LF	\$52.00	\$49,660.00
2	12" PVC Water Main	4,120	LF	\$79.25	\$326,510.00
3	12" Water Connection	1	EA	\$1,295.00	\$1,295.00
4	8" Fittings	13	EA	\$675.00	\$8,775.00
5	12" Fittings	16	EA	\$1,495.00	\$23,920.00

**Trails 6th Addition**  
**Maintenance-Warranty Bond Estimate**  
 May 8, 2024

Item No.	Description & Cost Code	Quantity	Units	Unit Price	Total Price
6	8" Gate Valve w/ Box	7	EA	\$1,950.00	\$13,650.00
7	12" Gate Valve w/ Box	12	EA	\$3,215.00	\$38,580.00
8	1" Water Services	51	EA	\$3,525.00	\$179,775.00
9	2" Irrigation Service	2	EA	\$8,105.00	\$16,210.00
10	Fire Hydrant Assembly, Incl. Valve and Tee	8	EA	\$7,950.00	\$63,600.00
11	Temporary Blowoff	9	EA	\$1,780.00	\$16,020.00

**6400 - Streets & Walkways**

**6405 - Streets, Curbs, & Gutter**

**\$822,860.05**

1	Asphalt Roadway 4" AC over 6" Base	13,000	SY	\$28.75	\$373,750.00
2	Asphalt Roadway 2" AC over 6" Base	4,000	SY	\$19.00	\$76,000.00
3	Asphalt Roadway 3" AC over 6" Base	6,900	SY	\$23.45	\$161,805.00
4	Rolled Curb and Gutter	6,349	LF	\$18.45	\$117,139.05
5	Curb and Gutter	4,780	LF	\$19.70	\$94,166.00

**6410 - Sidewalks**

**\$303,650.00**

1	Concrete Sidewalk (Incl. ADA Ramp Conc.)	39,400	SF	\$5.75	\$226,550.00
2	Cement Concrete Driveway Approach (Lift Station)	200	SF	\$7.50	\$1,500.00
3	Pedestrian Ramps (Extra Work and Warning Surface)	48	EA	\$1,575.00	\$75,600.00

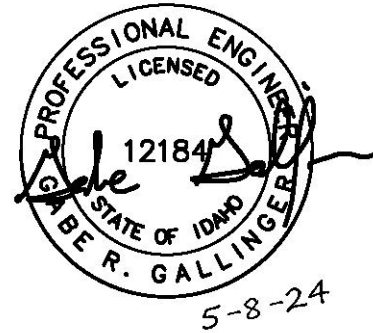
**Trails 6th Addition**  
**Maintenance-Warranty Bond Estimate**  
 May 8, 2024

<b>Item No.</b>	<b>Description &amp; Cost Code</b>	<b>Quantity</b>	<b>Units</b>	<b>Unit Price</b>	<b>Total Price</b>
<b><u>6415 - Pathways &amp; Trails</u></b>					<b>\$117,820.50</b>
1	Asphalt Paths (2" AC over 6" Base)	5,145	SY	\$20.90	\$107,530.50
2	Asphalt Path Subgrade Prep	5,145	SY	\$2.00	\$10,290.00
<b><u>6450 - Utilities - Dry</u></b>					<b>\$55,500.00</b>
<b><u>6450 - Dry Utilities</u></b>					<b>\$55,500.00</b>
1	Dry Utilities Trenching (Ex, Bed, and Backfill only)	7,400	LF	\$7.50	\$55,500.00
<b><u>6500 - Landscaping, Signage, Striping, Mailboxes</u></b>					<b>\$136,931.96</b>
<b><u>6505 - Landscaping</u></b>					<b>\$136,931.96</b>
1	Irrigation (Pipe, Heads, Valves, Wire, Controls)	38	Zone	\$2,500.00	\$95,000.00
2	Fine Grade Swales/Common Area	149,757	SF	\$0.10	\$14,975.70
3	Hydroseed Swales/Common Area	149,757	SF	\$0.18	\$26,956.26
<b><u>6520 - Signage and Striping</u></b>					<b>\$98,402.00</b>
1	Signage	12	EA	\$1,200.00	\$14,400.00
2	Type III Barricades	31	EA	\$1,345.00	\$41,695.00
3	Pavement Markings-Waterborne	4,950	LF	\$0.50	\$2,475.00
4	Pavement Markings- Thermoplastic Line	4,500	LF	\$6.60	\$29,700.00
5	Thermoplastic Crosswalk and Stop Bar	680	SF	\$9.90	\$6,732.00
6	Plastic Turn Arrow	6	EA	\$275.00	\$1,650.00



**Trails 6th Addition**  
**Maintenance-Warranty Bond Estimate**  
 May 8, 2024

Item No.	Description & Cost Code	Quantity	Units	Unit Price	Total Price
7	Plastic Bike Lane Symbol with Arrow	5	EA	\$350.00	\$1,750.00
<b>6525 - Mailboxes</b>					<b>\$2,925.00</b>
1	Concrete Mailbox Pad	3	EA	\$700.00	\$2,100.00
2	CBU Mailbox Installation	3	EA	\$275.00	\$825.00
				<b>TOTAL</b>	<b>\$3,250,265.60</b>
				10% Multiplier	10%
				<b>Warranty Bond Amount</b>	<b>\$325,026.56</b>



OTHER BUSINESS

**CITY COUNCIL  
STAFF REPORT**

**DATE:** 6-4-24  
**FROM:** Kyle Marine, Water Department Director  
**SUBJECT:** Approval for construction of a new 16” Water Transmission Main in Thomas Lane

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**DECISION POINT:** Should City Council accept the lowest responsive bid and approve a construction contract with Alpine Northwest LLC for installation of a new 16” water transmission main in Thomas Lane?

**HISTORY:** In the 2012 Water Comprehensive Plan Update, the need for additional water storage due to the City's growth highlighted deficiencies in system capacity and supply in the High Zone. This necessitated the construction of a new tank with 1 million gallons (MG) of storage in the northeast end. However, finding a suitable site within existing boundaries posed a challenge. Consequently, in 2016, JUB was selected to help identify potential tank locations and propose builds. They utilized topographical information and hydraulic studies to determine suitable sites. Through this process, several possible new tank locations were identified, establishing a basic timeline for planned improvements. These improvements were divided into two phases: phase one involving the design, bidding, and construction of the transmission line, and phase two entailing the design, bidding, and construction of the tank site.

**FINANCIAL ANALYSIS:** Funding for the proposed project is partly included in the 2023-24 FY budget at \$1,500,000.00 to be paid out of Capitalization Fees. Funding in this line-item may need to be carried over into the next FY budget. A Request for Statements of Qualifications was issued, the Water Department qualified contractors pursuant to the published criteria, and bids were solicited from the pre-qualified contractors. Bids received for were as follows: Alpine Northwest \$2,369,358.00, Northwest Grading Inc. \$2,613,435.36, Halme Construction Inc. \$2,971,076.00, Big Sky Corp \$3,019,115.50, DW Excavating Inc \$3,115,105.00, S&L underground \$3,497,286.00, Terra Underground LLC \$3,698,390.00, Apollo \$3,999,909.60, and J7 Contracting \$4,113,251.00. The engineer’s estimate was \$4,631,000.00. The consulting engineer reviewed all bids for accuracy and verified with the lowest bidder that they were comfortable with their numbers. Northwest Grading and Big Sky sent the City letters objecting to the pre-qualification of Alpine Northwest, to which letters the City Attorney responded on May 14. Nothing further has been received from Northwest Grading or Big Sky.

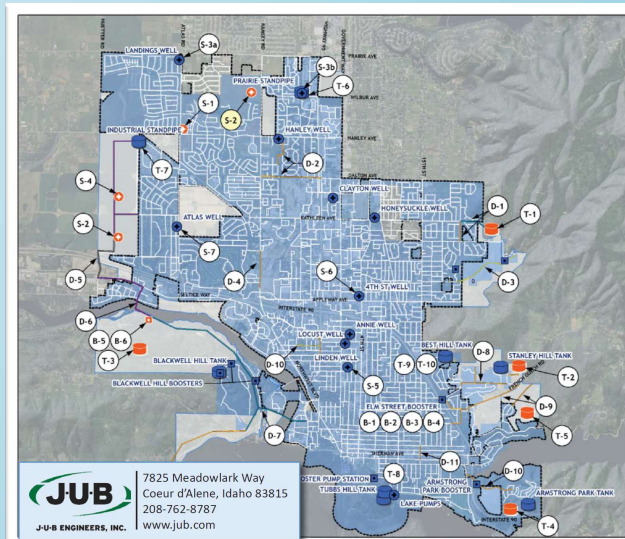
**PERFORMANCE ANALYSIS:** The Thomas Lane Transmission Main will move water from Margaret and 15<sup>th</sup> Street south to Thomas Lane, then East to the end of Thomas Lane, where we will be building the 1 MG water tank. This will help supply water to the northeast side of the High Zone to meet peak demand. Sections of 15th Street and Thomas Lane will have moving traffic detours and restrictions while construction takes place.

**DECISION POINT/RECOMMENDATION:** City Council should accept the lowest responsive bid of, and approve a construction contract with, Alpine Northwest LLC for installation of a new 16” transmission main in Thomas Lane in the amount of \$2,369,358.00.



1

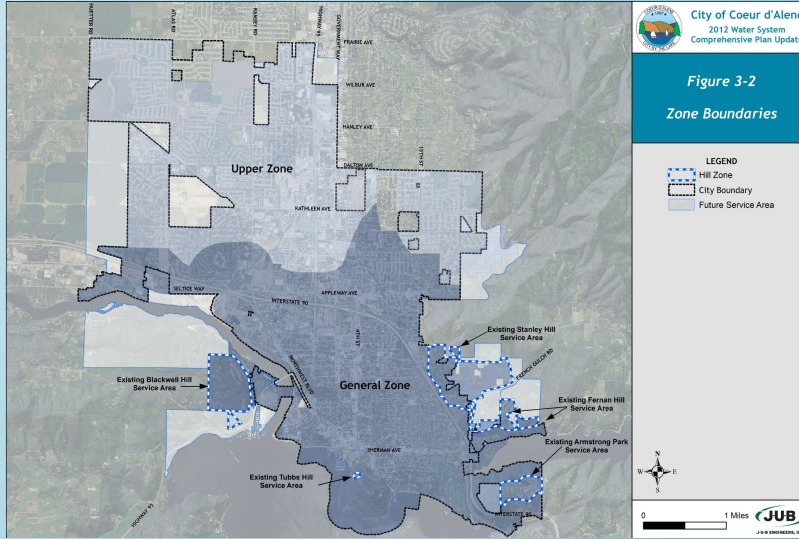
## 2012 Comp Plan



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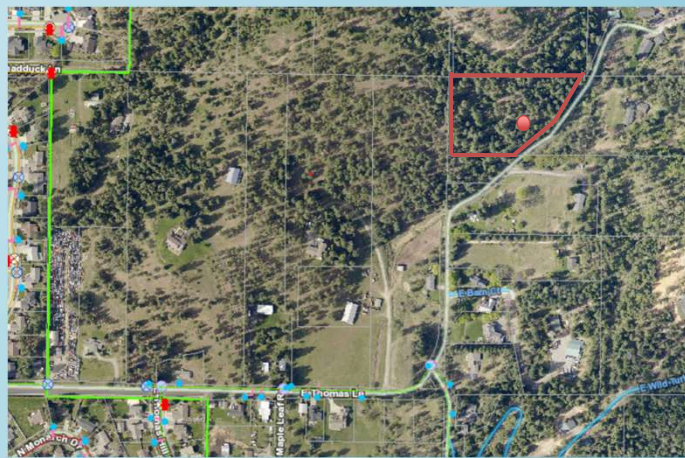


# 2012 Zone Boundaries



3

# Northeast Storage Facility



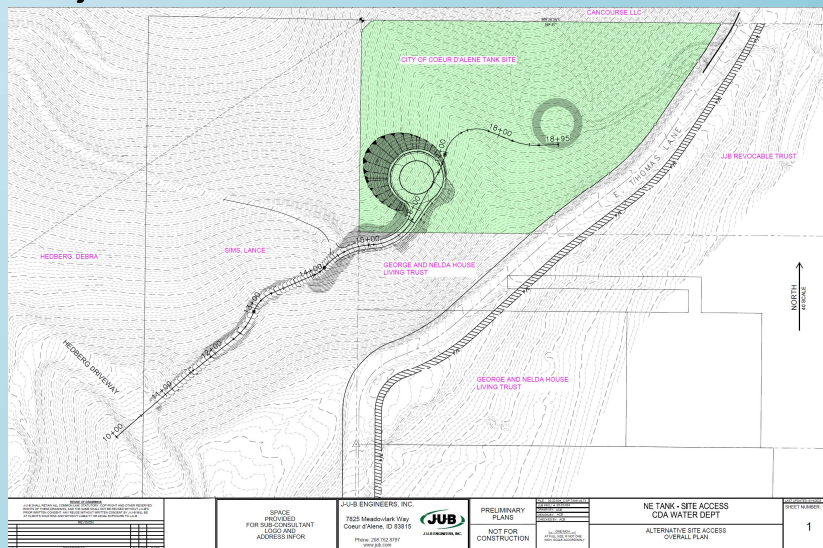
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# PLANNED TANK SITE



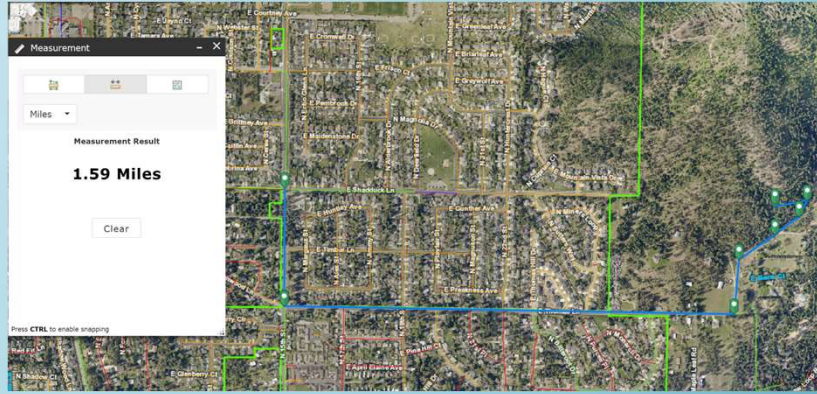
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# TRAIL, SITE DISTURBANCE & ACCESS



6

# PLANNED TRANSMISSION MAIN ROUTE



7

## Coeur d'Alene Water System Storage Improvements - Estimated Range of Cost

<i>Item Description</i>	<i>Opinion of Probable Cost (2024)</i>	<i>Opinion of Probable Cost (2024)</i>
Tank	\$ 4,860,000	\$ 5,290,000
Trail & Site	\$ 910,000	\$ 980,000
Transmission Main	\$ 4,631,000	\$ 5,436,000
<b>OPINION OF TOTAL PROJECT COST</b>	<b>\$ 10,401,000</b>	<b>\$ 11,706,000</b>



8



# Bid Totals

Contractor	Schedule A	Schedule B	Total
Alpine Northwest	\$ 2,227,069.00	\$ 142,289.00	\$ 2,369,358.00
Northwest Grading Inc	\$ 2,492,082.59	\$ 121,352.77	\$ 2,613,435.36
Halme Construction INC	\$ 2,813,424.00	\$ 157,652.00	\$ 2,971,076.00
Big Sky ID Corp	\$ 2,885,788.00	\$ 133,327.50	\$ 3,019,115.50
DW Excavating inc	\$ 3,000,634.00	\$ 114,471.00	\$ 3,115,105.00
S&L underground	\$ 3,339,468.00	\$ 157,818.00	\$ 3,497,286.00
Terra Underground LLC	\$ 3,575,748.00	\$ 122,642.00	\$ 3,698,390.00
Apollo	\$ 3,878,690.00	\$ 121,219.60	\$ 3,999,909.60
J7 Contracting	\$ 3,925,810.00	\$ 187,441.00	\$ 4,113,251.00



9

# Thank you!



10

RESOLUTION NO. 24-045

A RESOLUTION OF THE CITY OF COEUR D’ALENE, KOOTENAI COUNTY, IDAHO, ACCEPTING THE BID OF, AND AWARDING A CONTRACT TO, ALPINE NORTHWEST LLC FOR THE COEUR D’ALENE WATER DEPARTMENT TRANSMISSION LINE – NORTHEAST TANK/THOMAS LANE PROJECT IN AN AMOUNT NOT TO EXCEED \$2,369,358.00.

WHEREAS, the City duly solicited Statements of Qualifications, qualified contractors, and solicited bids for the CDA Water Department Transmission Line – Northeast Tank/Thomas Lane Project in Coeur d’Alene, Idaho; bids were received by the Water Department and evaluated on May 3, 2024; and the lowest responsive bid received was that of Alpine Northwest LLC, in the amount of Two Million Three Hundred Sixty-Nine Thousand Three Hundred Fifty-Eight and no/100 Dollars (\$2,369,358.00); and it is in the best interests of the City of Coeur d’Alene and the citizens thereof that said bid be accepted and a contract be entered into.

NOW, THEREFORE,

BE IT RESOLVED by the Mayor and City Council of the City of Coeur d’Alene that the bid of Alpine Northwest LLC, in an amount not to exceed Two Million Three Hundred Sixty-Nine Thousand Three Hundred Fifty-Eight and no/100 Dollars (\$2,369,358.00), for the CDA Water Department Transmission Line – Northeast Tank/Thomas Lane Project be and the same is hereby accepted.

BE IT FURTHER RESOLVED that the City enter into a contract with Alpine Northwest LLC in substantially the form attached hereto as Exhibit “A” and incorporated herein by reference, with the provision that the Mayor, City Administrator, and City Attorney are hereby authorized to modify said contract provided that the substantive provisions of the contract remain intact.

BE IT FURTHER RESOLVED that the Mayor and City Clerk be and they are hereby authorized to execute such contact on behalf of the City.

DATED this 4<sup>th</sup> day of June, 2024.

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James Hammond, Mayor

ATTEST:

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Renata McLeod, City Clerk



Motion by \_\_\_\_\_, Seconded by \_\_\_\_\_, to adopt the foregoing resolution.

ROLL CALL:

COUNCIL MEMBER ENGLISH Voted

COUNCIL MEMBER MILLER Voted

COUNCIL MEMBER GOOKIN Voted

COUNCIL MEMBER EVANS Voted

COUNCIL MEMBER MCEVERS Voted

COUNCIL MEMBER WOOD Voted

\_\_\_\_\_ was absent. Motion \_\_\_\_\_.

**CITY OF COEUR D'ALENE WATER DEPARTMENT  
NORTHEAST TANK – TRANSMISSION MAIN CONTRACT**

THIS CONTRACT is made and entered into this 4<sup>th</sup> day of June, 2024, between the **CITY OF COEUR D'ALENE (CITY)**, Kootenai County, Idaho, a municipal corporation duly organized and existing under and by virtue of the laws of the state of Idaho, hereinafter referred to as “CITY”, and **ALPINE NORTHWEST LLC**, a limited liability company duly organized and existing under and by virtue of the laws of the state of Idaho, with its principal place of business at 3155 Berta Jo Ct., Hayden Idaho, hereinafter referred to as the “CONTRACTOR.”

WITNESSETH:

WHEREAS the CONTRACTOR has been awarded the Contract for the Northeast Tank – Transmission Main in Coeur d’Alene, according to plans and specifications on file in the office of the City Clerk of the CITY, which plans and specifications are entitled:

**City of Coeur d’Alene - Water Department – Northeast Tank – Transmission Main**

NOW, THEREFORE,

IT IS AGREED that, for and in consideration of the covenants and agreements to be made and performed by the CITY, as hereinafter set forth, the CONTRACTOR shall perform the work as set forth in the said plans and specifications described above, in said City, furnishing all labor and materials therefor according to said plans and specifications and under the penalties expressed in the performance bond bearing even date herewith, and which bond with said plans and specifications are hereby declared and accepted as parts of this Contract. All material shall be of the high standard required by the said plans and specifications and approved by the Water Director or designee, and all labor performed shall be of first-class workmanship.

The CONTRACTOR shall employ appropriate means to prevent accidents and defend the CITY from all claims for injury to person or property resulting from the CONTRACTOR’s actions or omissions in performance of this Contract, and to that end shall maintain insurance of the type and in the amount specified in the Contract Documents, including the Standard General Conditions and Supplementary General Conditions applicable to this Project. Certificates of Insurance, providing at least thirty (30) days’ written notice to the City prior to cancellation of the policy, shall be filed in the office of the City Clerk.

The CONTRACTOR agrees to maintain Worker’s Compensation coverage on all employees, including employees of subcontractors, during the term of this Contract as required by Idaho Code §§ 72-101 through 72-806. Should the CONTRACTOR fail to maintain such insurance during the entire term hereof, the CONTRACTOR shall indemnify the CITY against any loss resulting to the CITY from such failure, either by way of compensation or additional premium liability. The CONTRACTOR shall furnish to the CITY, prior to commencement of the work, such evidence as the CITY may require guaranteeing contributions which will come due under the Idaho Worker’s Compensation Law including, at the option of the CITY, a surety bond in an amount sufficient to make such payments.

The CONTRACTOR shall furnish the CITY certificates of all insurance coverages required herein, which certificates must be approved by the City Attorney.

The CITY shall pay to the CONTRACTOR, for the work, services and materials herein provided to be done and furnished by it, a sum not to exceed **Two Million Three-hundred Sixty-nine Thousand Three Hundred Fifty-eight and no/100 Dollars (\$2,369,358.00)**, as provided in the Unit Price Schedule. Partial payment shall be made by the end of each calendar month on a duly certified estimate of the work completed in the previous calendar month less five percent (5%) provided that the estimate is submitted to the CITY by the first Tuesday of the month. Final payment shall be made within thirty (30) days after completion of all work and acceptance by the City Council.

The Work shall be substantially complete within the calendar days listed below (for the Contract Award, as applicable) after the date when the Contract Times commence to run, as provided in Paragraph 2.03 of the Standard General Conditions and completed and ready for final payment in accordance with Paragraph 14.07 of the Standard General Conditions within **thirty (30) calendar days** after the date of substantial completion.

<b>CONTRACT TIME</b>	<b>CONTRACT AWARD</b>	<b>CALENDAR TIME (DAYS)</b>
Substantial Completion	Base Bid – Schedule A	<b>120</b> calendar days
Substantial Completion	Add. Alt. – Schedule B	Additional <b>14</b> calendar days shall be added to Base Bid Contract Times
Final Completion	any	<b>30</b> calendar days

The CITY and the CONTRACTOR recognize that time is of the essence and failure of the CONTRACTOR to complete the work within the time allowed shall result in damages being sustained by the CITY. Such damages are and will continue to be impractical and extremely difficult to determine. Therefore, in the event the CONTRACTOR shall fail to complete the work within the above time limit, the CONTRACTOR shall pay to the CITY or have withheld from moneys due, liquidated damages at the rate of **\$500.00** per calendar day, which sums shall not be construed as a penalty.

IT IS AGREED that the CONTRACTOR, as required by Idaho law, must employ ninety-five percent (95%) bona fide Idaho residents as employees on any job under this Contract except where under this Contract fifty (50) or fewer persons are employed by the CONTRACTOR, in which case the CONTRACTOR may employ no more than ten percent (10%) nonresidents; PROVIDED, however, in all cases the CONTRACTOR must give preference to the employment of bona fide Idaho residents in the performance of said work pursuant to Idaho Code § 44-1002.

CONTRACTOR further agrees to comply will all the requirements of **Attachment 1**, which is incorporated herein by reference.

Pursuant to Idaho Code § 67-2359, the CONTRACTOR certifies that it is not currently owned or operated by the government of the People's Republic of China and will not for the duration of the contract be owned or operated by the government of People's Republic of China.

Pursuant to Idaho Code § 67-2346, the CONTRACTOR certifies that it is not currently engaged in, and will not for the duration of the contract engage in, a boycott of goods or services from Israel or territories under its control.

Pursuant to Idaho Code § 18-8703, the **CONTRACTOR** certifies that it is not, and will not for the duration of this Agreement become, an abortion provider or an affiliate of an abortion provider, as those terms are defined in the "No Public Funds for Abortion Act," Idaho Code §§ 18-8701 et seq.

The CONTRACTOR further agrees that, in consideration of securing the business of constructing the works to be constructed under this Contract, recognizing the business in which it is engaged is of a transitory character and that in the pursuit thereof, its property used therein may be outside the state of Idaho when taxes, excises or license fees to which it is liable become payable:

1. To pay promptly when due all taxes (other than on real property), excises and license fees due to the State of Idaho, its subdivisions, and municipal and quasi-municipal corporations therein, accrued or accruing during the term of this Contract, whether or not the same shall be payable at the end of such term; and

2. If the taxes, excises and license fees are not payable at the end of said term, but liability for said payment thereof exists, even though the same are or become liens upon its property, to secure the same to the satisfaction of the respective officers charged with the collection thereof; and

3. In the event of its default in the payment or securing of such taxes, excises and license fees, to consent that the department, officer, board or taxing unit entering into this Contract may withhold from any payment due it thereunder the estimated amount of such accrued and accruing taxes, excises and license fees for the benefit of all taxing units to which said CONTRACTOR is liable.

IT IS FURTHER AGREED that, for additions or deductions to the plans and specifications, the unit prices as set forth in the written proposal of the CONTRACTOR are hereby made a part of this Contract.

For the faithful performance of this Contract in accordance with the plans and specifications and payment for all labor and materials, the CONTRACTOR shall execute a good and sufficient performance bond and a payment bond, each in the amount of one hundred percent (100%) of the total amount of the bid as herein before stated, said bonds to be executed by a surety company authorized to do business in the state of Idaho.

The term "CONTRACT DOCUMENTS" are defined in "Standard General Conditions of the Construction Contract" ISPWC Division 100.

THIS CONTRACT, with all of its forms, specifications and stipulations, shall be binding upon the parties hereto, their successors and assigns.

IN WITNESS WHEREOF, the Mayor and City Clerk of the CITY OF COEUR D'ALENE have executed this Contract on behalf of said CITY, the City Clerk has affixed the seal of said city hereto, and the CONTRACTOR has caused the same to be signed by its President, and its seal to be affixed hereto, the day and year first above written.

**CITY OF COEUR D'ALENE**

**ALPINE NORTHWEST LLC**

By \_\_\_\_\_  
James Hammond, Mayor

By \_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
(printed name)  
(title)

ATTEST:

ATTEST:

\_\_\_\_\_  
Renata McLeod, City Clerk

\_\_\_\_\_  
Corporate Secretary



# ATTACHMENT 1

This attachment is to be inserted in every agreement/contract subject to Title VI of the Civil Rights Act of 1964 and associated Regulations.

During the performance of this contract, the contractor/consultant, for itself, its assignees and successors in interest (hereinafter referred to as the “contractor”) agrees as follows:

**1. Compliance with Regulations**

The contractor shall comply with the Regulations relative to non-discrimination in federally assisted programs of United States Department of Transportation (USDOT), Title 49, Code of Federal Regulations, part 21, as they may be amended from time to time, (hereinafter referred to as the Regulations), which are herein incorporated by reference and made a part of this contract.

**2. Non-discrimination**

The contractor, with regard to the work performed by it during the contract, shall not discriminate on the grounds of race, color, sex, or national origin in the selection and retention of sub-contractors, including procurement of materials and leases of equipment. The contractor shall not participate either directly or indirectly in the discrimination prohibited by Section 21.5 of the Regulations, including employment practices when the contract covers a program set forth in Appendix B of the Regulations.

**3. Solicitations for Sub-contracts, Including Procurement of Materials and Equipment**

In all solicitations either by competitive bidding or negotiations made by the contractor for work to be performed under a sub-contract, including procurement of materials or leases of equipment, each potential sub-contractor or supplier shall be notified by the contractor of the contractor’s obligations under this contract and the Regulations relative to non-discrimination on the grounds of race, color, sex, or national origin.

**4. Information and Reports**

The contractor shall provide all information and reports required by the Regulations or directives issued pursuant thereto, and shall permit access to its books, records, accounts, other sources of information, and its facilities as may be determined by the contracting agency or the appropriate federal agency to be pertinent to ascertain compliance with such Regulations, orders and instructions. Where any information required of a contractor is in the exclusive possession of another who fails or refuses to furnish this information, the contractor shall so certify to ITD or the USDOT as appropriate, and shall set forth what efforts it has made to obtain the information.

**5. Sanctions for Non-compliance**

In the event of the contractor’s non-compliance with the non-discrimination provisions of this contract, the contracting agency shall impose such contract sanctions as it or the USDOT may determine to be appropriate, including, but not limited to:

- Withholding of payments to the contractor under the contract until the contractor complies, and/or;
- Cancellation, termination, or suspension of the contract, in whole or in part.

**Incorporation of Provisions**

The contractor shall include the provisions of paragraphs (1) through (5) in every sub-contract, including procurement of materials and leases of equipment, unless exempt by the Regulations, or directives issued pursuant thereto. The contractor shall take such action with respect to any sub-contractor or procurement as the contracting agency or USDOT may direct as a means of enforcing such provisions including sanctions for non-compliance.

Provided, however, that in the event a contractor becomes involved in, or is threatened with, litigation with a sub-contractor or supplier as a result of such direction, the contractor may request ITD enter into such litigation to protect the interests of the state and, in addition, the contractor may request the USDOT enter into such litigation to protect the interests of the United States.

**CITY COUNCIL  
STAFF REPORT**

**DATE:** June 4<sup>th</sup>, 2024  
**FROM:** Glen Poelstra, Water Department Assistant Director  
**SUBJECT:** Approval for reallocation of part-time employee budget to purchase materials from Consolidated Supply Co. for upsizing the water main in Cda Place 38<sup>th</sup> Addition

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**DECISION POINT:** Should Council reallocate \$85,222.93 from the Water Department Part-Time Employee Budget and approve the purchase of parts and materials from the lowest responsive bidder, Consolidated Supply Co., in the amount of for the Cda Place 38<sup>th</sup> Addition water main upsizing?

**HISTORY:** On an annual basis the Water Dept. has budgeted for at least four part-time staff members to help with the workload in the busy summer season. Through the most recent Water Comprehensive Plan Update, deficiencies were identified regarding system capacity and supply in the north central part of Cda Place and would like to utilize funds from the part-time staff budget to help fund an upsize in pipe. Traditionally in years past, 12” water mains have been able to supply developments with enough water for domestic and irrigation use. However, engineered flow models indicate that an upsize of water main to 18” in this area and in the future connecting Prairie Well transmission main to Atlas Rd, would substantially help equalize the flows between Prairie Standpipe and Industrial Standpipe. This would also help solve pressure issues during high demand situations in the Landings development. The goal would be to install a production well in the north central part of town in the future as it would help supply this area with growth taking place to the southwest.

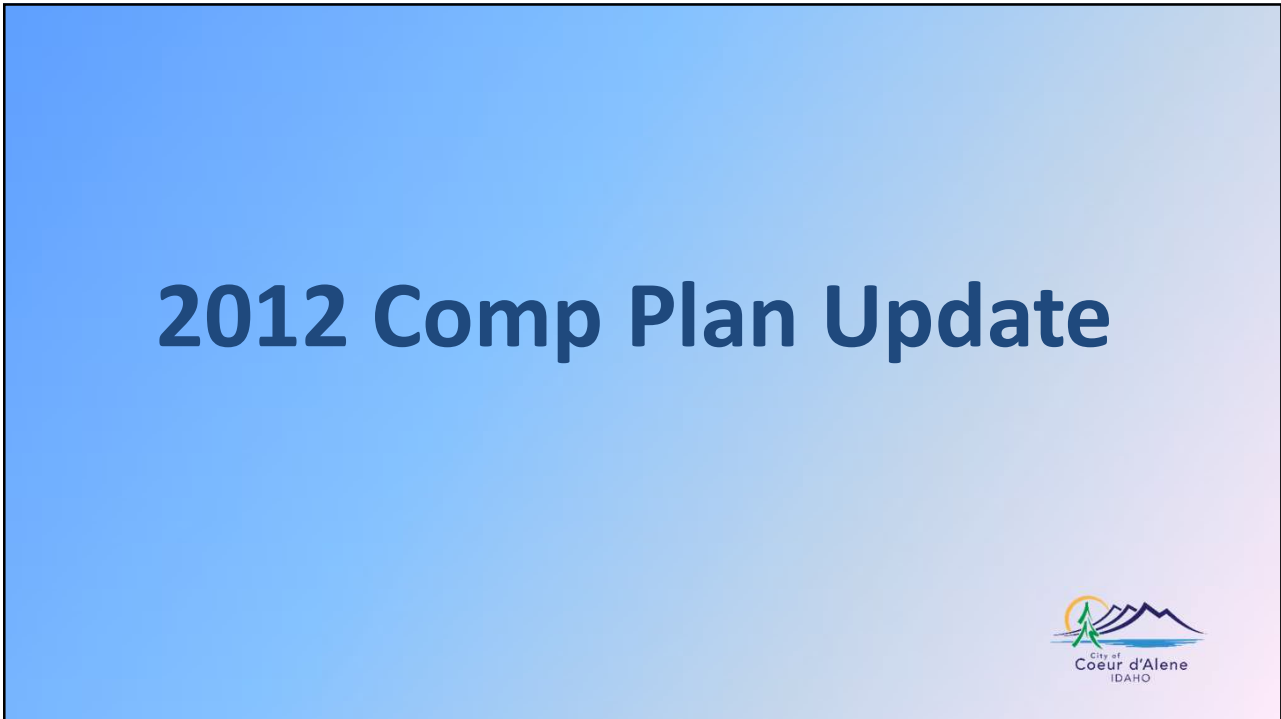
**FINANCIAL ANALYSIS:** Funding for the proposed purchase of these materials would need to be reallocated from the part-time staff budget in the amount \$87,000 and the additional funds needed to complete the purchase would be from the capitalization fee budget. Pursuant to the City’s purchasing policy, quotes were received from three vendors. The three quotes received were from Consolidated Supply Co. \$ 85,222.93, HD Fowler \$98,364.85, and Ferguson Waterworks \$99,714.70. The Water Dept. would incur no extra costs by reallocating the part-time staff funds to this project. Being able to upsize the water main and install it this year, the Water Dept would save a substantial amount of money and inconvenience to residents by not having to install it after the area has been developed. The Water Dept has determined that this is a more critical need for the City than the need for part-time staff this year.

**PERFORMANCE ANALYSIS:** According to the engineered flow model, the upsizing of this water main in Cda Place 38<sup>th</sup> addition from 12” to 18” is crucial to help set up this area for future projects to hydraulically equalize Prairie Standpipe and Industrial Standpipe and provide much better pressure and flow to customers during high demand. Installing 18” water main in this area would provide the necessary infrastructure for a future production well to be added.

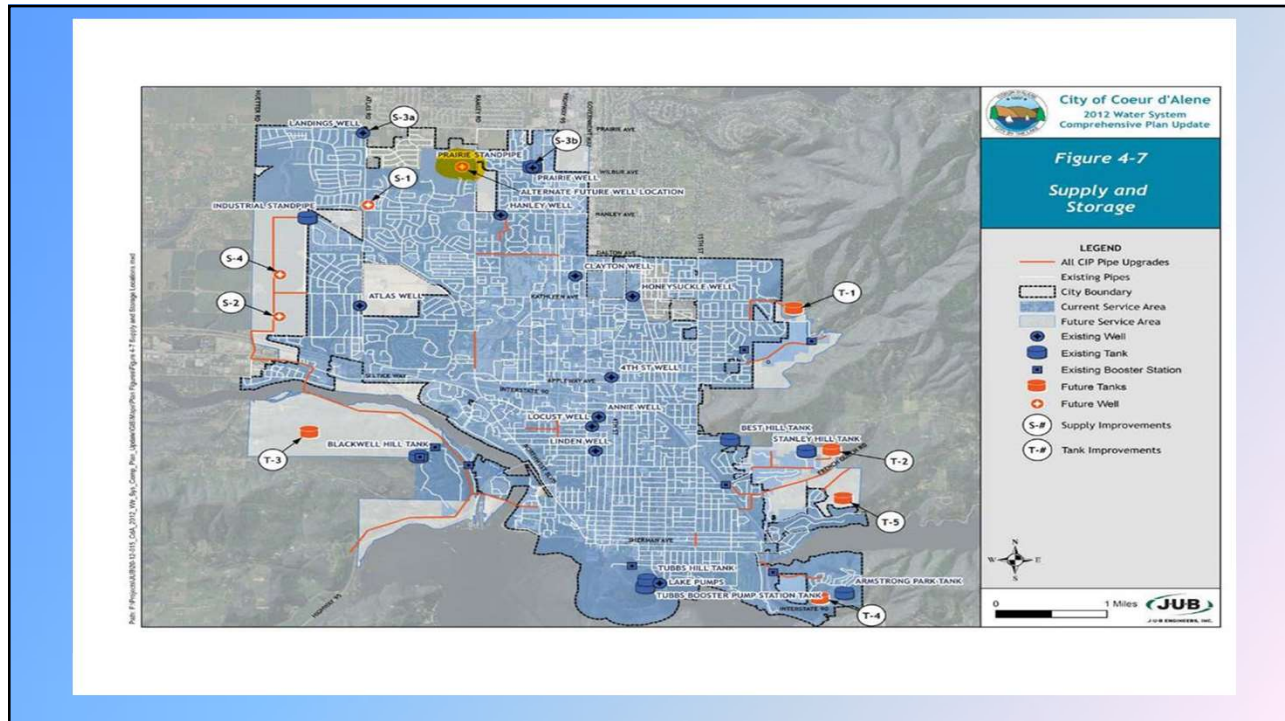
**DECISION POINT/RECOMMENDATION:** City Council should approve the reallocation of part-time employee budget funds of \$87,000, accept the lowest responsive quote, and approve the purchase of parts and materials from Consolidated Supply Co. in the amount of \$85,222.93 for upsizing the water main in Cda Place 38<sup>th</sup> Addition.



1



2



3

Chapter 4 – Water Supply Evaluation

**Fernan Hill Pressure Zone**  
 The Fernan Booster Station will be located on the same property as the Elm Street Station. Build out MDD for this zone is expected to be 760 gpm. A triplex station is the recommended layout. Specific options for the station configuration are included in **Appendix D**. This zone will require an additional station near the tank in order to provide higher elevation properties.

**Armstrong Park Pressure Zone**  
 The current demand for this zone is 380 gpm with a firm capacity of 220 gpm. When the demands near 200 gpm, it is recommended that the third pump be installed at the booster station. This third pump will provide a firm capacity of 440 gpm, which will supply the planned service area demand of 370 gpm.

**Blackwell Hill Pressure Zone**  
 Blackwell Hill has some significant areas within the planning boundary that can be developed over a range of elevations. The final build-out will require several booster stations or PRVs to service this area. A more detailed discussion of this area is included in **Appendix E**. The current demands of 55 gpm are met by the pumps. Replacement of the station will be required to meet future demands. Additional booster stations will be required to serve upper elevations of this zone.

**4.5.2 New Wells**  
 Water supply recommendations include installing one new well in 2013 to meet demand and storage requirements. The well should be installed in the Upper Zone to meet the system demands. Placement of future wells will need to take place on the western edge of the City where the aquifer is productive and water quality is best. Building and improving the pipe network to supply the water from the western side of town to the expected areas of growth in the areas east and south of the City will be critical in providing adequate pressures and flow. Figure 4-7 shows proposed locations of the new wells. A total of four new wells will be required to serve the system's projected build-out (two within the next ten years). Verification of site requirements with IDEQ should be completed prior to final selection of future well sites.

4-10

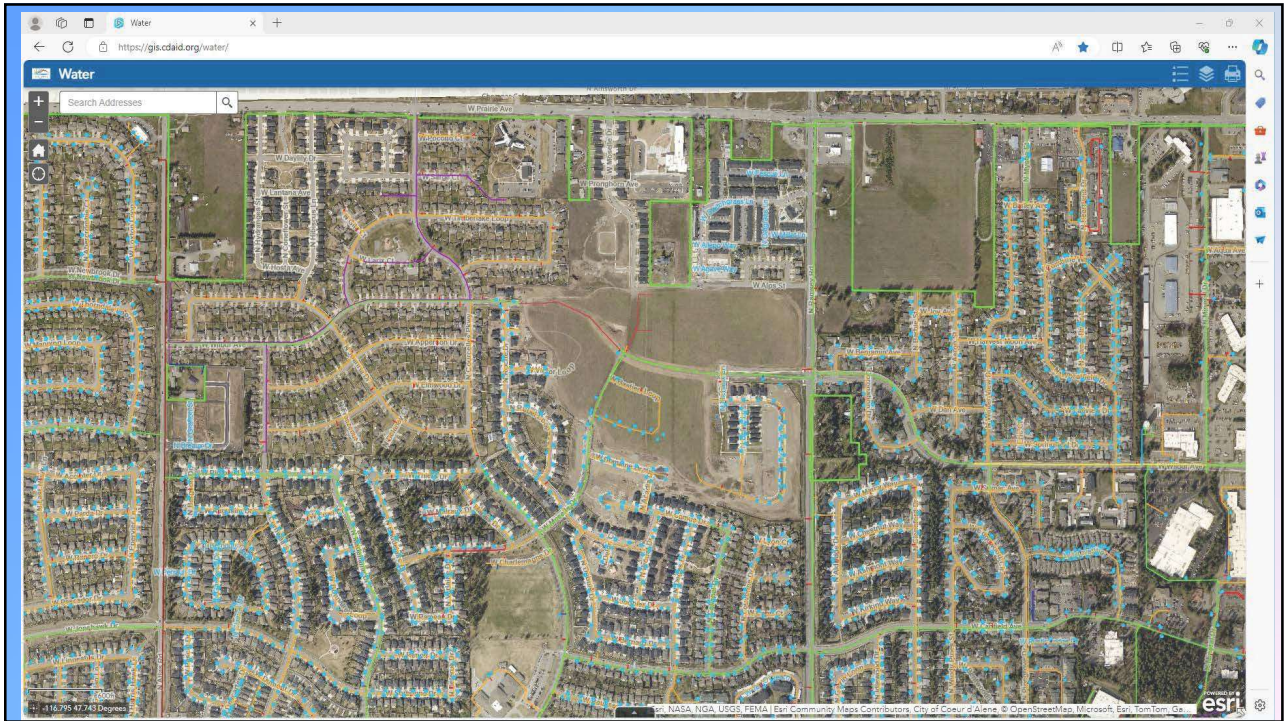
City of Coeur d'Alene  
 2012 Water System Comprehensive Plan Update  
Prepared by JUB, Inc. for the City of Coeur d'Alene, Idaho

4





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7

RESOLUTION NO. 24-046

A RESOLUTION OF THE CITY OF COEUR D'ALENE, KOOTENAI COUNTY, IDAHO, AUTHORIZING THE REALLOCATION OF FUNDS FOR WATER DEPARTMENT PART-TIME EMPLOYEES TO ASSIST WITH THE PURCHASE OF PARTS AND MATERIALS FROM CONSOLIDATED SUPPLY CO. FOR THE CDA PLACE WATER MAIN PROJECT IN AN AMOUNT NOT TO EXCEED \$85,222.93.

WHEREAS, the Water Department Assistant Director of the City of Coeur d'Alene has recommended that the Mayor and Council authorize the reallocation of \$85,222.93 for the Water Department Part-Time Employees to purchase of parts and materials from Consolidated Supply Co. for the CDA Place Water Main Project, a copy of which quote is attached hereto as Exhibit "1" and by reference made a part hereof; and

WHEREAS, it is deemed to be in the best interests of the City of Coeur d'Alene and the citizens thereof to make such expenditure.

NOW, THEREFORE,

BE IT RESOLVED by the Mayor and City Council of the City of Coeur d'Alene that the sum of \$85,222.93 be reallocated from the Water Department Part-Time Employee budget to allow the purchase parts and materials for the CDA Place Water Main Project.

DATED this 4<sup>th</sup> day of June, 2024.

\_\_\_\_\_  
James Hammond, Mayor

ATTEST:

\_\_\_\_\_  
Renata McLeod, City Clerk

Motion by \_\_\_\_\_, Seconded by \_\_\_\_\_, to adopt the foregoing resolution.

ROLL CALL:

COUNCIL MEMBER EVANS Voted \_\_\_\_\_

COUNCIL MEMBER MCEVERS Voted \_\_\_\_\_

COUNCIL MEMBER MILLER Voted \_\_\_\_\_

COUNCIL MEMBER WOOD Voted \_\_\_\_\_

COUNCIL MEMBER GOOKIN Voted \_\_\_\_\_

COUNCIL MEMBER ENGLISH Voted \_\_\_\_\_

\_\_\_\_\_ was absent. Motion \_\_\_\_\_.



**Consolidated  
Supply Co.**

**\*\* Quotation \*\***

Bid #: S011930877  
Page #: 1

Send P/O To:  
CONSOLIDATED SUPPLY CO - CDA  
155 EAST DALTON AVE  
DALTON GARDENS, ID 83815

Phone # : 208-762-2568

Bid To:  
CITY OF COEUR D'ALENE  
CITY HALL - ACCTS PAYABLE  
710 E MULLAN  
COEUR D ALENE, ID 83814

Ship To:  
CITY OF COEUR D ALENE WATER SHOP  
3145 N. HOWARD ST  
NORTH GATE  
COEUR D ALENE, ID 83814

Requested By: GLEN  
Phone # : 208-769-2233

JOB: CDA PLACE 38TH

Bid-Date-Expr-Date-Writer-----Salesperson-----Ship Via-----  
05/24/24 06/23/24 Cody Roberts - CDA Skyler Huffman -Corpo WCN WC NOW

Quantity	Description	Unit Price	Ext Price
	***** Shipping Instructions ***** * ****DRIVER MUST CALL GLENN 30 MINUTES * * BEFORE DELIVERY @ 208-755-9728 *** * *****		
1MC	18" WATER PARTS LIST	0.000ea	0.00
528'	18"X22' DR18 CL235 C905 PVC PIPE W/ RINGS *Special - Subject to Restock Fee*	68.525ft	36181.20
1ea	18" MJ CROSS C153 CL L/ACCS 505112 *Special - Subject to Restock Fee*	2376.181ea	2376.18
2ea	18" MJ TEE C153 CL L/ACCS 103318 *Special - Subject to Restock Fee*	1527.511EA	3055.02
6ea	18" CRISPIN BUTTERFLY VALVE, MJXMJ, 250PSI RATED, 506HP W/ TNA OPERATOR AND 2" NUT, DI BODY, 17-4 SHAFT, EPDM SEAT *Special - Subject to Restock Fee*	3489.861ea	20939.17
2ea	18"X8" MJXMJ REDUCER C153 CL L/ACCS 103103 *Special - Subject to Restock Fee*	579.852EA	1159.70
7ea	TYLER 16"X36" 564A STANDARD VALVE BOX SLIP TYPE ADJ (WATER)"COMBO" (CDA SPEC) ***** Kit Components ***** * 7 - 940 36" CI VALVE BOX SLIP BASE * * TYLER 112082 OLYMPIC 13-5270 * * Loc: YA06-27-1A Pn: 9618 * * 7 - TYLER 6855/7000 WATER PICK LID *	146.318ea	1024.23

\*\*\* Continued on Next Page \*\*\*  
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**\*\* Quotation \*\***

Bid #: S011930877  
Page #: 2

CITY OF COEUR D'ALENE

Quantity	Description	Unit Price	Ext Price
	* F/ CI VALVE BOX DOM 145325 *		
	* Loc: YA06-22-1A Pn: 10893 *		
	* 7 - TYLER 6855 16T 16" HEAVY DUTY *		
	* SLIP TYPE VLV BOX TOP ONLY L/L *		
	* 502289 *		
	* Loc: YA06-25-1A Pn: 131626 *		
	*****		
14ea	EBAA 2018PSC 18" C905 MEGALUG RESTRAINT PACK WITH BOLTS & NUTS (B&N INCLUDED BUT PACKED SEPARATELY)	318.885ea	4464.39
3ea	18"X6" MJXFLG TEE C153 CL L/ACCS 131281 *Special - Subject to Restock Fee*	922.911EA	2768.73
1ea	18"X8" MJXFLG TEE C153 CL L/ACCS 131304 *Special - Subject to Restock Fee*	1202.405EA	1202.41
3ea	18"X12" MJXMJ REDUCER C153 CL L/ACC 103073 *Special - Subject to Restock Fee*	533.270EA	1599.81
10ea	18" STAR MJXMJ ADAPTER W/ MJ ACCS MJA18FCB *Special - Subject to Restock Fee*	576.110ea	5761.10
2ea	12" MJ LONG SOLID SLEEVE C153 CL L/ACCS 137337	246.013EA	492.03
7ea	EBAA 2012PSC 12" C900 & IPS PVC MEGALUG ACCS PACK WITH CORTEN B&N (TRANS GSKT REQ'D F/IPS)	143.010ea	1001.07
3ea	EBAA 2008PSC 8" C900 & IPS PVC MEGALUG ACCS PACK W/ CORTEN B&N (TRANS GSKT REQ'D F/IPS)	79.995ea	239.99
9ea	ROMAC 202S-21.10X1" IPT 18"X1" DLB SS STRAP SADDLE 19.50-21.10 *Special - Subject to Restock Fee*	328.655ea	2957.90
			-----
	Bid Total		85222.93
			-----
	Bid Amount		85222.93

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Thank you for requesting a quotation from Consolidated Supply Co. ("Consolidated") for certain materials you need for the project identified in the attached or enclosed quotation document (the "Project"). The enclosed quotation to you is made subject to the following terms and conditions:

1. You must carefully review the quotation to confirm that it meets your requirements before using it for a bid. Unless you have provided Consolidated with a detailed bill of materials and specifications with your requirements (with any applicable addendums), this quotation is only a good-faith estimate of the material types and quantities that may be required for the Project. Building plans alone do not constitute a detailed bill of materials or specifications, particularly if more than one supplier or subcontractor may be involved in supplying plumbing and/or waterworks materials. You agree that all risk of loss arising from the use of this quotation for bidding purposes-including any loss relating to errors in scope, quantity, price, time, and place of delivery-is on you. Notwithstanding anything to the contrary in this paragraph, you are responsible to specify and select appropriate materials for your intended use. Consolidated provides no design, engineering, or other professional services and cannot recommend or warrant goods to be fit for your particular purposes.

2. If you place an order with Consolidated for work or materials for the Project, the resulting contract will be subject to Consolidated's General Terms and Conditions of Sale. If credit is provided by Consolidated, then that credit is provided on Consolidated's general credit terms and conditions. These terms and conditions are available to you upon request and can be viewed on our website at [www.consolidatedsupply.com](http://www.consolidatedsupply.com).

3. Delivery under this quotation is FOB Consolidated's OR manufacturer's facility. If the quotation includes delivery to a jobsite, Consolidated may use a method and carrier of Consolidated's choice, unless otherwise stated in the quotation, and Consolidated assumes that the location is legally and physically accessible to interstate freight carriers operating under ICC regulations. Unloading labor will be provided by purchaser. Additional charges may apply if these assumptions are incorrect or if multiple deliveries are required. Consolidated will make a good faith effort to meet delivery dates agreed to in writing, but cannot guaranty delivery dates for goods not in stock or for which the terms of delivery are outside our control.

4. Except as otherwise provided in Section 5 below, pricing in this quotation is based on unit amounts and is based on the expectation that goods will be ordered within 30 calendar days from the date of the quotation. Consolidated may extend quoted prices on a case-by-case basis beyond the 30-day period. Consolidated reserves the right to correct or withdraw this quotation in the case of clerical error. Any change in quantities ordered or time for delivery may result in a change of the quoted prices, including unit prices, unless otherwise agreed to by Consolidated in writing.

5. Due to the current unstable market and price conditions, Consolidated reserves the right, at any time after the date of this quotation and until the time of shipment, to adjust the delivery times and/or increase the prices set forth in this quotation to address price and availability factors, including but not limited to government regulations, tariffs, transportation, fuel and raw materials costs. Any material deliveries delayed beyond manufacturer lead times may be subject to applicable storage fees in effect at the time of shipment. This quotation is not a bid or a lump-sum quote, unless specifically stated in the quotation.